



PROPOSED TERMINAL STATION

1005 BOUNDARY ROAD, TARNEIT

TRAFFIC MANAGEMENT REPORT

PROPOSED TERMINAL STATION 1005 BOUNDARY ROAD, TARNEIT

Client: Mecone Group Pty Ltd

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1 INTRODUCTION

SALT has been engaged by Mecone Group Pty Ltd to prepare a Traffic Management Report for the proposed terminal station to be located at 1005 Boundary Road in Tarneit.

In the course of preparing this report, the following has been undertaken:

- The development plans and background information have been reviewed;
- Car parking and bicycle parking requirements have been reviewed;
- Traffic movements have been identified; and
- Traffic management measures have been recommended.

The following sets out SALT's findings with respect to the traffic management matters of the proposal.

2 EXISTING CONDITIONS

2.1 LOCATION AND LAND USE

The subject site is located at 1005 Boundary Road in Tarneit. It has frontage to Boundary Road in the north, Derrimut Road to the east, and Tarneit Road to the west.

The proposal relates to development toward the eastern portion of the site, at the south-western corner of Boundary Road / Derrimut Road / Hopkins Road. The western portion of the site is under construction for a new industrial/warehouse development (Tarneit Logistics Hub).

Surrounding land is largely undeveloped or industrial or residential in nature.

Figure 1 depicts the location of the site with respect to the surrounding road network and land uses. An aerial view of the subject portion of the site is provided in Figure 2. The location of the development site (Lot 4) in relation to the larger site is depicted in Figure 3.

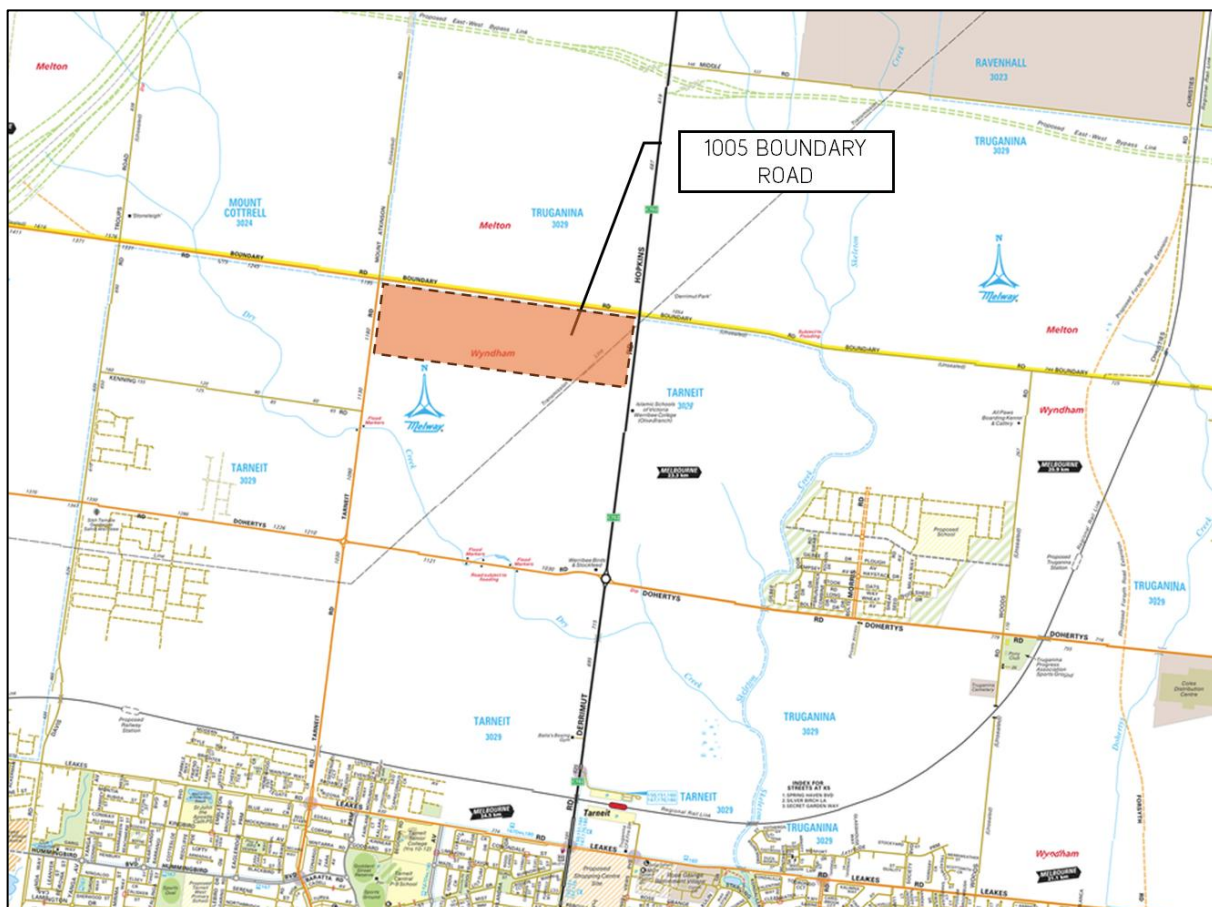


Figure 1 Subject site location



Figure 2 Aerial view of development site

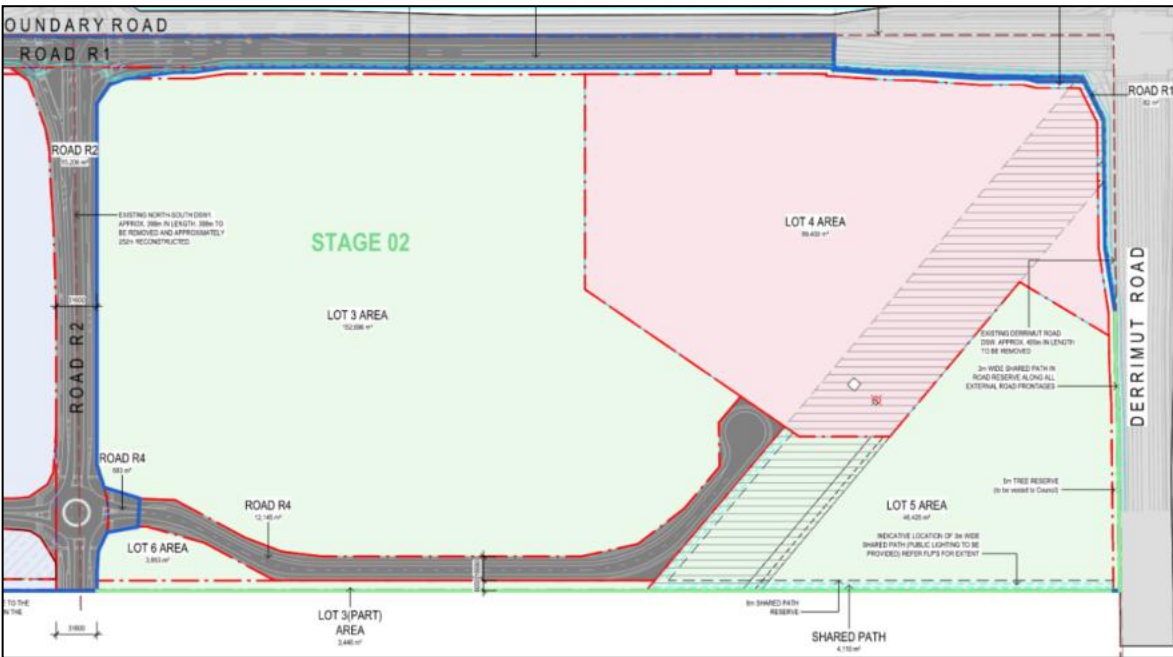


Figure 3 Development site context

2.2 ZONING AND PLANNING

The subject site lies within an Urban Growth Zone – Schedule 13 (UGZ), with sections of Special Use Zone – Schedule 7 (SUZ7) and Transport Network Zone 2 – Principal Road Network (TRZ2) under the Wyndham Planning Scheme, as depicted in Figure 4.

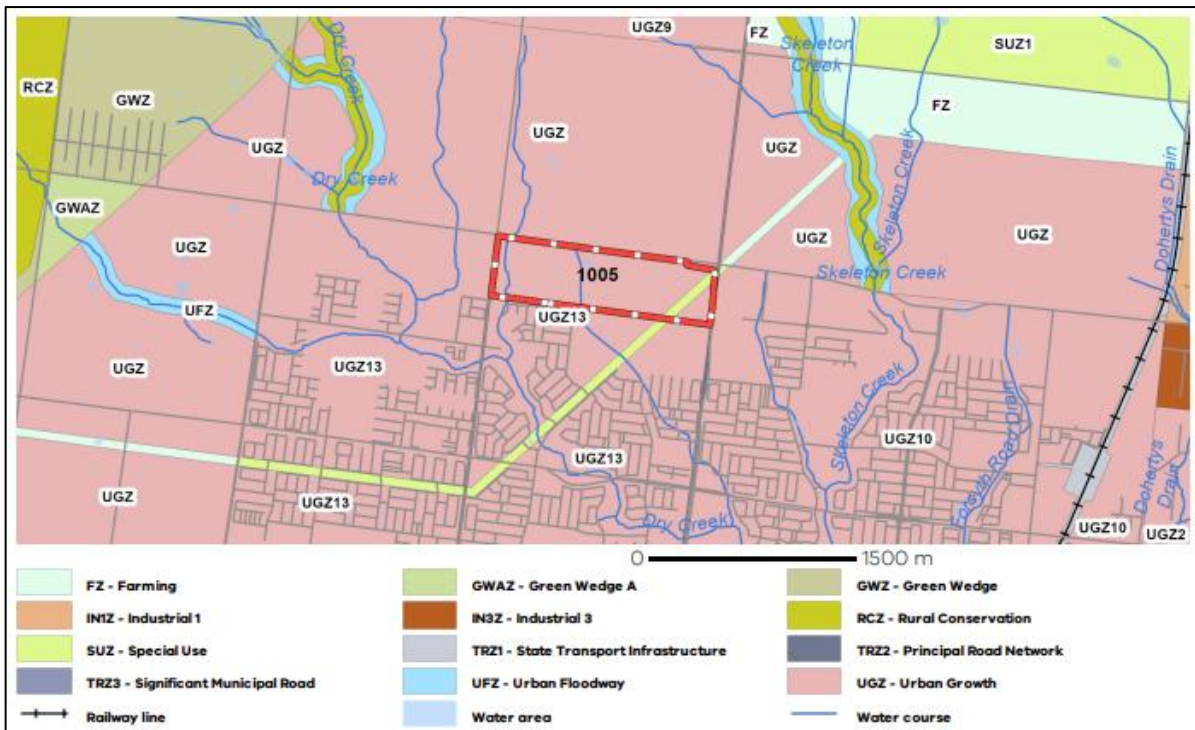


Figure 4 Local planning map

UGZ7 relates to the site being located within the Tarneit North Precinct Structure Plan, as depicted in Figure 5. Clause 3.3 of Schedule 13 to Clause 37.07 Urban Growth Zone requests the following:

An application that proposes to create or change access to a primary or secondary arterial road must be accompanied by a Traffic Impact Assessment Report (TIAR). The TIAR, including functional layout plans and a feasibility / concept road safety audit, must be to the satisfaction of VicRoads or Wyndham City Council, as required.

SALT has prepared a separate TIAR (refer 25661TREP01F01) including concept layout plans, and a Road Safety Audit will be undertaken separately.

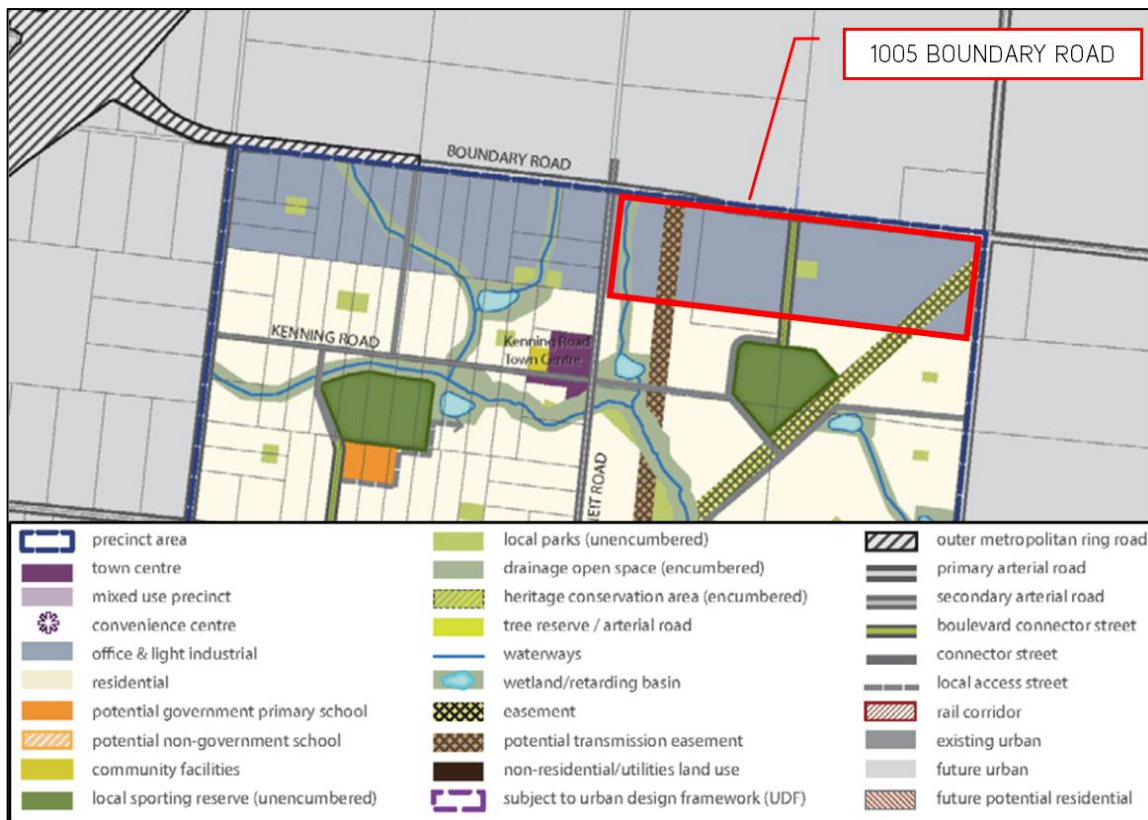


Figure 5 Tarneit North PSP land use diagram

It is also subject to the following overlays:

- Development Contributions Plan Overlay – Schedule 13 (DCP013)
- Design and Development Overlay – Schedule 11 (DDO11)
- Public Acquisition Overlay – PS Map Ref PA01 (PA01)
- Public Acquisition Overlay – PS Map Ref PA02 (PA02)

Schedule 11 to Clause 43.02 Design and Development Overlay specifies the following, relevant from a traffic engineering perspective.

Car parking, vehicle access and loading

A new development should be planned to achieve the following:

- Car parking and loading facilities to the side or rear of any buildings, with limited visitor parking incorporated in frontage areas.
- Encourage shared parking areas for large developments.
- Include trees spaced evenly throughout the car park at a ratio of 1 every 6 bays.
- Clear pedestrian paths that are separated from main vehicle access ways.
- Minimisation of vehicle crossovers.
- Loading and servicing should generally be located away/out of view from street, residential interface or creek/public realm interface.

Application requirements

The following application requirements apply to an application for a permit under Clause 43.02, in addition to those specified elsewhere in the scheme and must accompany an application, as appropriate, to the satisfaction of the responsible authority:

- A Traffic Management Report that addresses traffic movements, traffic management measures and parking requirements and provisions, bicycle parking provisions and pedestrian and vehicle circulation.

This report provides the Traffic Management Report.

2.3 ROAD NETWORK

2.3.1 BOUNDARY ROAD

At the site frontage, Boundary Road is under the jurisdiction of Wyndham City Council. It extends in a generally east-west alignment and provides one traffic lane in each direction. On-street parking is not facilitated. Pedestrian footpaths are not currently provided on either side of Boundary Road.

A posted speed limit of 60km/hr applies.

2.3.2 DERRIMUT ROAD

At the site frontage, Derrimut Road is under the jurisdiction of Department of Transport and Planning (DTP). North of Boundary Road, it continues as Hopkins Road.

Derrimut Road extends in a generally north-south alignment and generally provides one traffic lane in each direction. An additional lane is provided on the northern and southern approaches to the intersection with Boundary Road. On-street parking is not facilitated. Pedestrian footpaths are not currently provided on either side of Boundary Road.

A posted speed limit of 60km/hr applies.

2.3.3 FUTURE ROAD UPGRADES

Within the Tarneit North PSP, both Boundary Road and Derrimut Road are identified as 6 lane arterial roads, as depicted in Figure 6, with the cross-section provided in Figure 7.

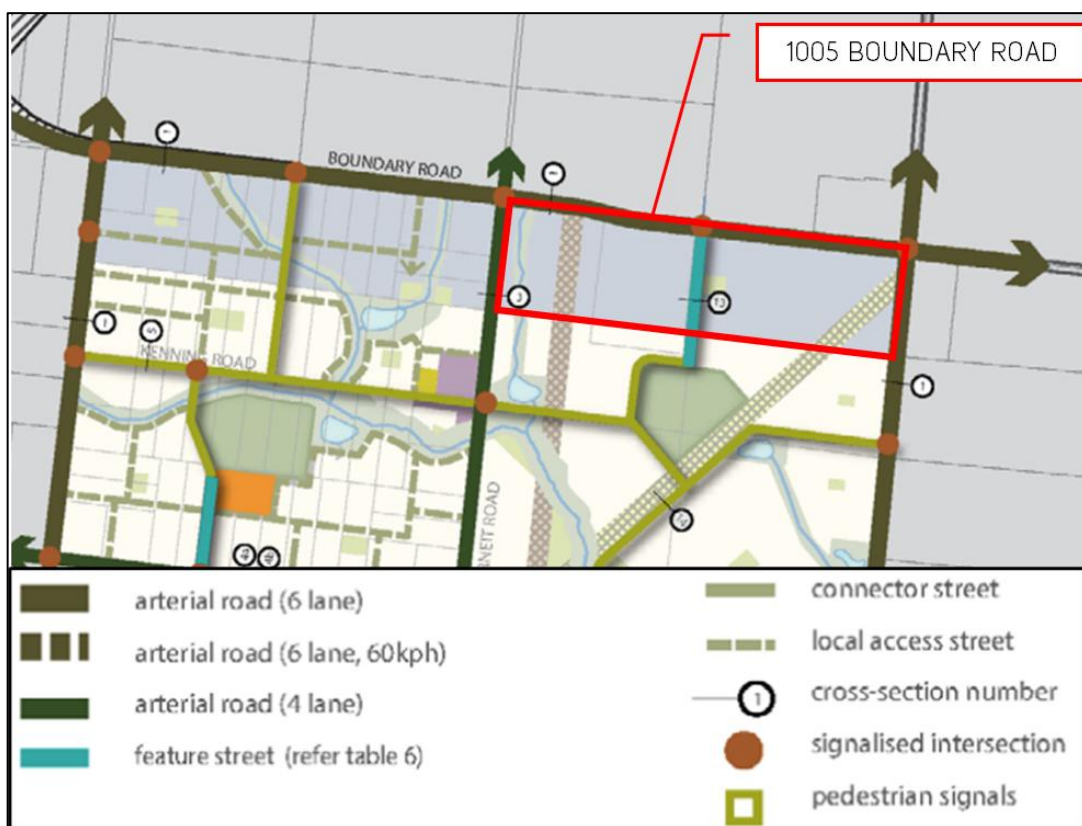


Figure 6 Tarneit North PSP road network

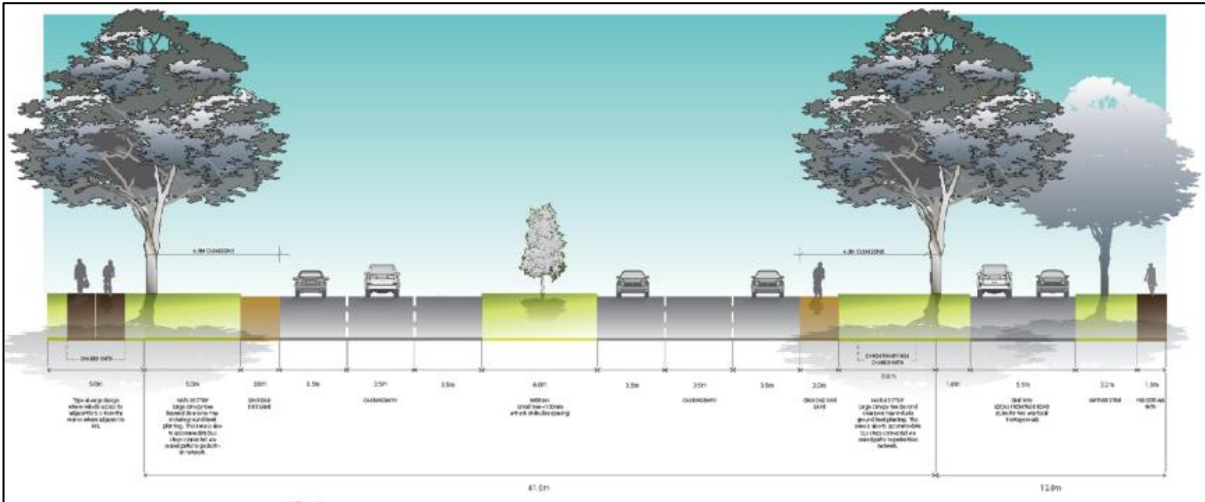


Figure 7 Tarnait North PSP arterial road cross section

The intersection of Boundary Road and Derrimut Road is to be signalised, as identified in the PSP in Figure 6.

Boundary Road is set to be upgraded in stages, in the vicinity of the site, including:

- A new signalised intersection with the Charter Hall estate road to the west of the subject site. This is currently under design development.
- A signalised intersection with Hopkins Road / Derrimut Road to be delivered by MRVP/DTP. At the time of writing, this is in planning (refer Figure 8), with site investigations underway. The intersection is currently provided with temporary traffic lights. Reduced speed limits of 40km/hr are provided on all approaches to the temporary signals.

Figure 9 depicts the future road upgrades in the vicinity of the subject site and the proposed access.



Figure 8 Boundary Road / Derrimut Road upgrade diagram (source: Transport Victoria)

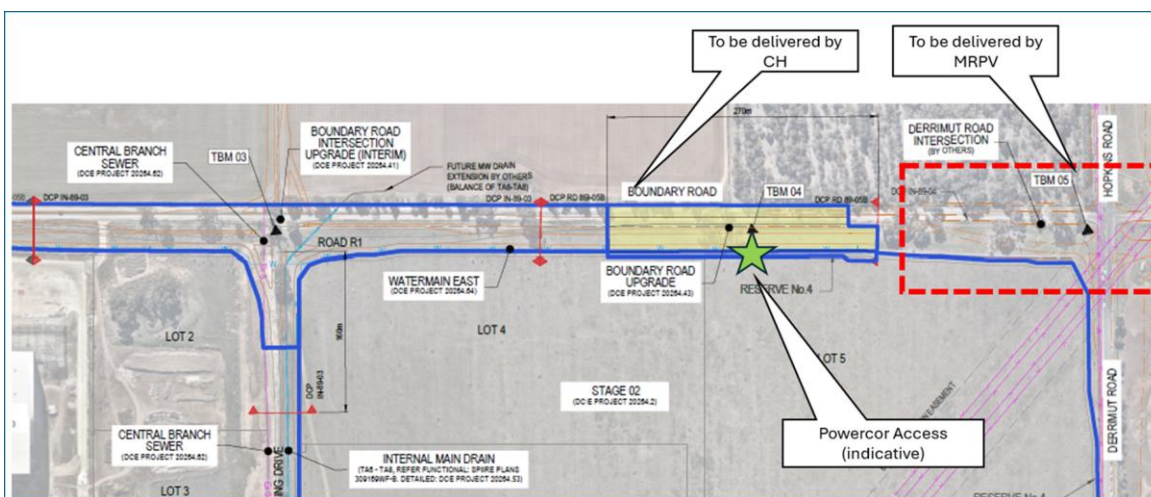


Figure 9 Road upgrade diagram

2.4 SUSTAINABLE TRANSPORT

The site currently has limited sustainable transport opportunities, with no train or bus routes operating within walking distance, and no formal pedestrian or cycling facilities.

Tarneit train station is situated approximately 3.5km to the south.

The Tarneit North PSP shows future sustainable transport improvements, as depicted in Figure 10, including:

- Boundary Road and Derrimut Road to be bus capable arterial roads
- On-road bike lanes and shared paths on Boundary Road and Derrimut
- New Tarneit West train station (set to open in 2026)

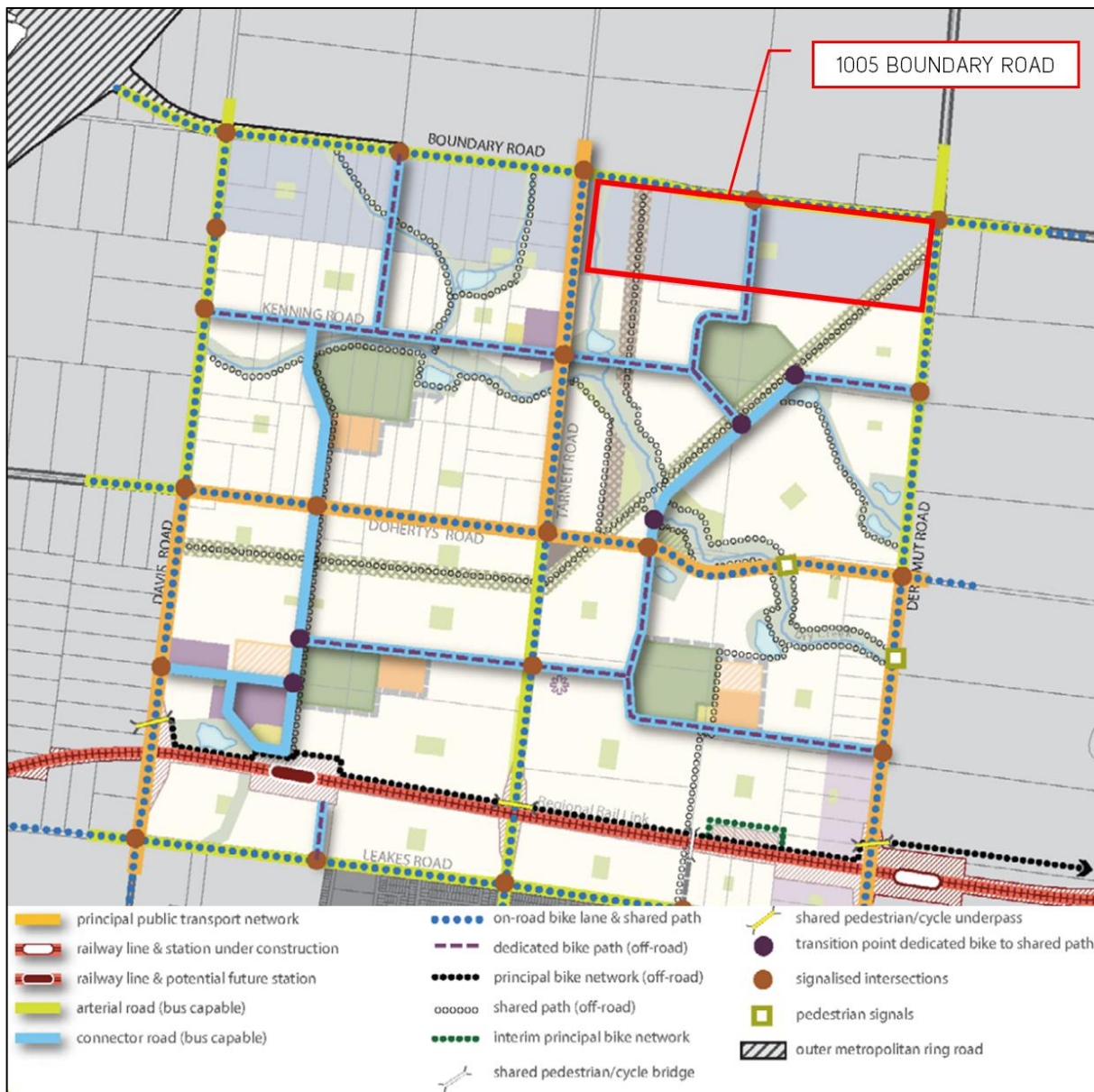


Figure 10 Tarneit North PSP sustainable transport

2.5 TRAFFIC VOLUMES

The Department of Transport (DTP) Open Data Hub provides traffic volume information for both Boundary Road and Derrimut Road.

The Annual Average Daily Traffic (AADT) volumes and percentage of heavy vehicles (% HV) on Boundary Road and Derrimut Road in both directions are depicted in Figure 11.



Figure 11 Traffic volumes (source: DTP Open Data Hub)

3 PROPOSAL

It is proposed to develop the site to provide a terminal station (utility installation). This is to be situated to the north-eastern corner of the property at 1005 Boundary Road.

Vehicular access is proposed from both Boundary Road and an internal road connecting to the western portion of the wider property (Road R4 in Figure 3). Access from Boundary Road is proposed to function as the main access in the short-term and during construction. In the long-term, the access from Road R4 is proposed to be the main access (following upgrade of Boundary Road).

The terminal station is expected to attract very little vehicular traffic on a day-to-day basis. There is expected to be two (2) attendances to the site each month for the purpose of operations and maintenance. Under usual circumstances, a maximum of two (2) staff may attend the site at any one time, and no visitors are expected. Vehicular traffic may be increased only in the circumstance that items need to be replaced or if there is a significant fault.

Typically, vehicle types attending the site will be light truck or utility vehicles.

A parking area is proposed adjacent the control room, which can accommodate trucks and staff cars. This would have capacity for five (5) staff cars parked at 90-degrees, with potential for another five (5) cars to park in tandem. Alternatively, it could store vehicles up to 15m in length without encroaching into the accessway.

The development plans assessed by this report are provided in APPENDIX 1.

SALT has prepared Concept Layout Plans for the access arrangement, as provided in APPENDIX 2.

4 CAR PARKING

Table 1 of Clause 52.06 of the Planning Scheme specifies the car parking requirements applicable for various development types. No parking rate is provided for a utility installation land use; rather parking is to be provided to the satisfaction of the responsible authority.

The proposed terminal station is expected to accommodate up to two (2) staff members on-site at any one time under most circumstances. A parking area is proposed adjacent the control room which has more than sufficient capacity to accommodate the two (2) staff vehicles, as well as a servicing/maintenance vehicle.

In the rare circumstance that items need to be replaced or there is a significant fault, more staff may be required on-site. In this case, up to 10 cars can park in tandem adjacent the control room, with additional capacity along the accessway in front of the control room.

5 BICYCLE PARKING

Table 1 of Clause 52.34 of the Planning Scheme specifies the bicycle parking requirements applicable to various developments. No bicycle parking rate is provided for a utility installation land use; rather bicycle parking is to be provided to the satisfaction of the responsible authority.

Considering the proposed land use and location with no cycling facilities within the direct vicinity, it is considered unlikely that any staff or visitors would cycle to the site. It is therefore not considered necessary to provide formal on-site bicycle parking facilities.

6 TRAFFIC MANAGEMENT

6.1 HEAVY VEHICLE MOVEMENTS

Heavy vehicles may be required on-site for the purposes of servicing and maintenance, including light trucks and utility vehicles.

Servicing and maintenance vehicles are to be permitted to traverse all internal accessways, as well as access points from Boundary Road and the southern internal road, as depicted in Figure 12.

There is a parking opportunity adjacent to the control room, should servicing / maintenance vehicles require parking on-site. This area is approximately 18.5m long, thus it is sufficient to accommodate key vehicles expected on-site.

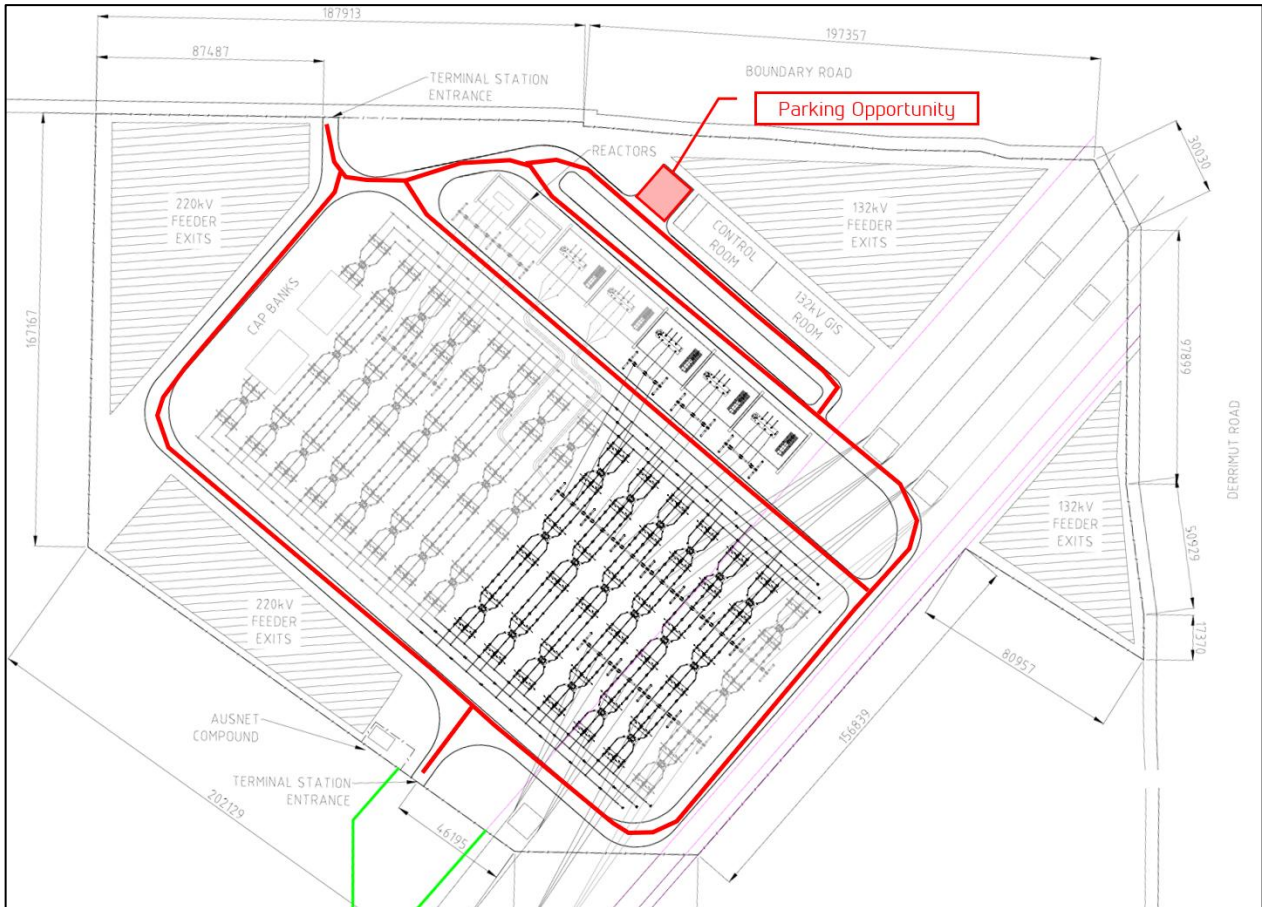


Figure 12 Heavy vehicle movement diagram

6.2 PASSENGER VEHICLE MOVEMENTS

Passenger vehicles (cars) may be expected on-site in the form of staff cars, including Australian Standard B85 and B99 vehicles.

Whilst the internal accessways will be sufficient to accommodate passenger vehicles, it is anticipated that staff vehicles will mainly be distributed between the site access points and the control room, as depicted in Figure 13. There is a parking opportunity adjacent to the control room, sufficient to accommodate staff cars.

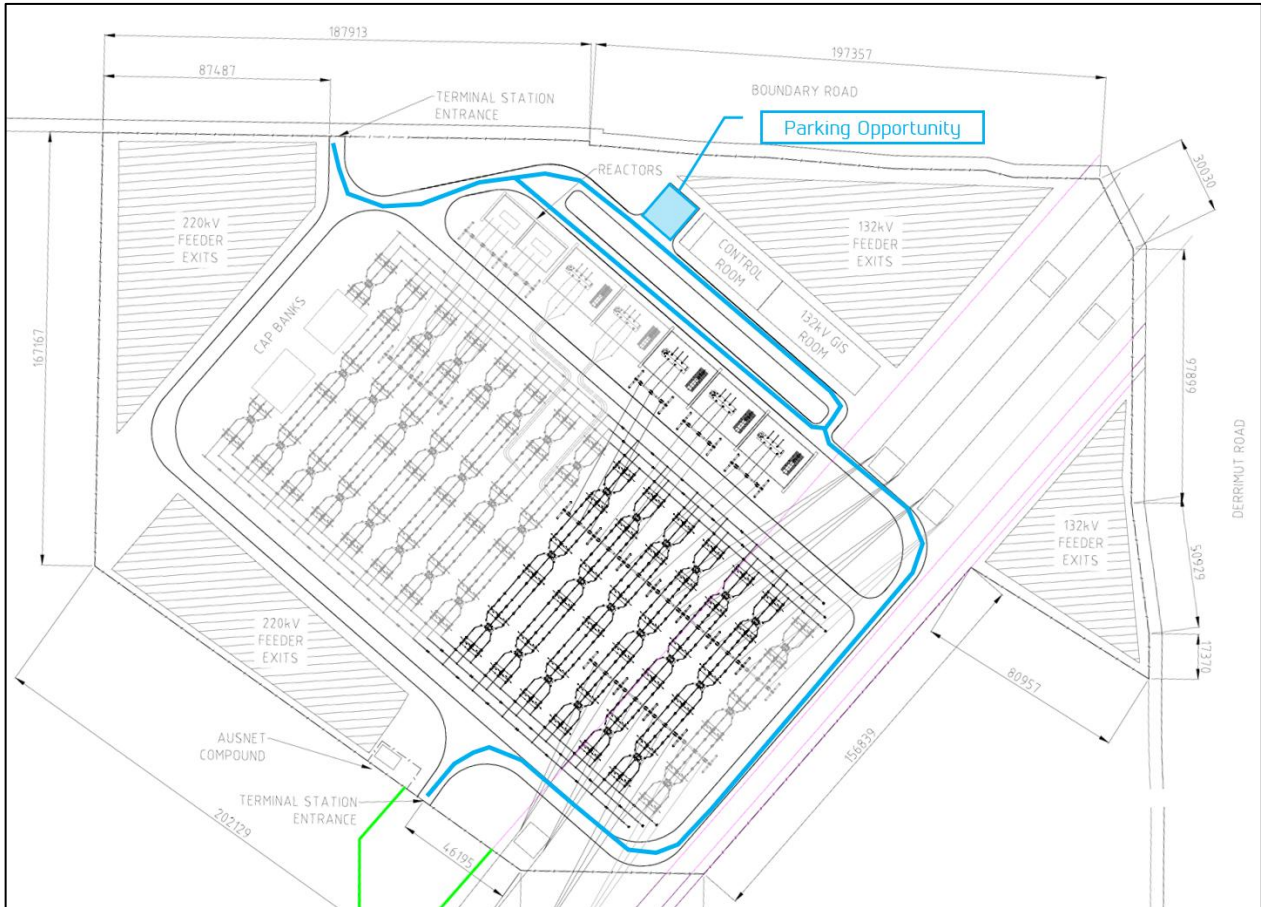


Figure 13 Main passenger vehicle movements

6.3 PEDESTRIAN MOVEMENTS

The site is not expected to generate any significant pedestrian movements, aside from those travelling between vehicles and the control room or other equipment on-site requiring servicing. It is highly unlikely that any staff would arrive/depart the site on foot, considering the site location and proposed operations.

The internal accessways will carry low traffic such that they will be suitable for sharing between pedestrians and vehicles.

7 TRAFFIC MANAGEMENT RECOMMENDATIONS

Based on the above, the following traffic management measures are recommended to ensure safety and functionality for all users. These are also depicted in Figure 14.

- Security gates should be provided at both access points. These should be set-back from the road frontages to allow vehicles to prop off the external road carriageways whilst waiting for gates to open.
- Wayfinding signage should be provided to allow drivers to locate key destinations on-site such as the control room.
- Advisory speed signage should be provided along the internal accessways to remind drivers to travel slowly, should pedestrians also be present.
- Additional speed control should be implemented on long, straight accessways such as truck-friendly speed humps.
- It should be considered to implement a one-way circulation arrangement, considering heavy vehicles may not be able to pass around bends in the accessway.
- Provide signage for pedestrians exiting the control room to "Look for vehicles" before stepping into the accessways.

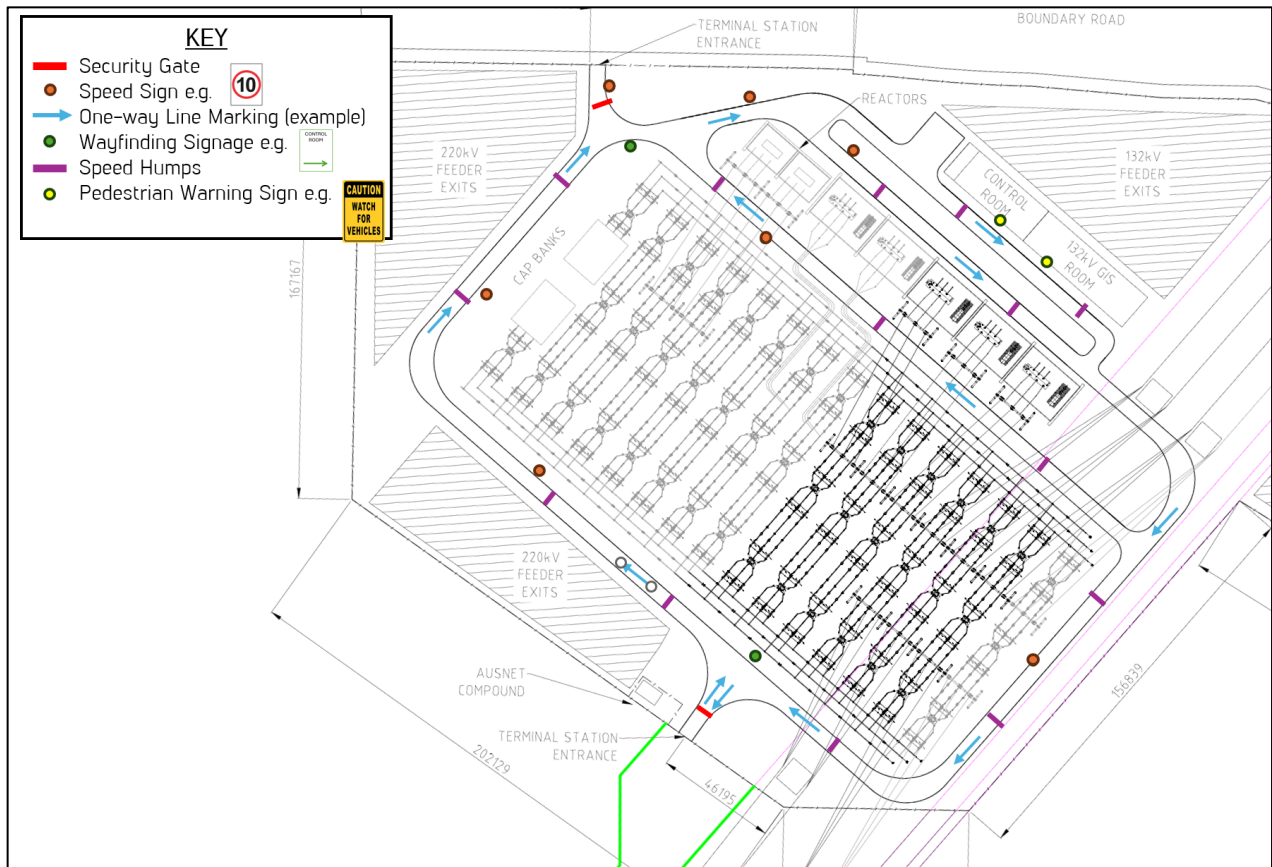


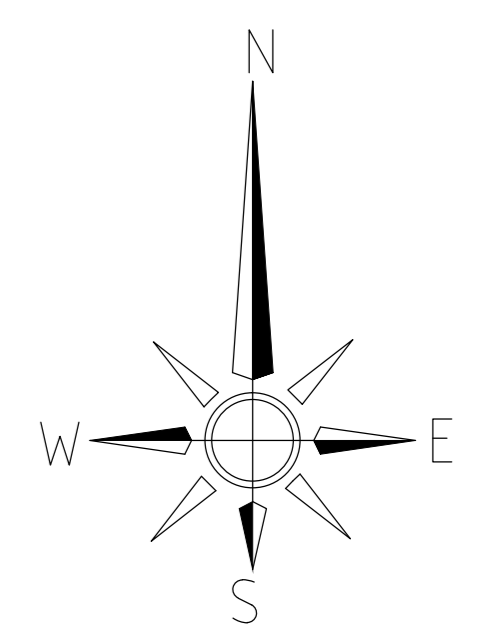
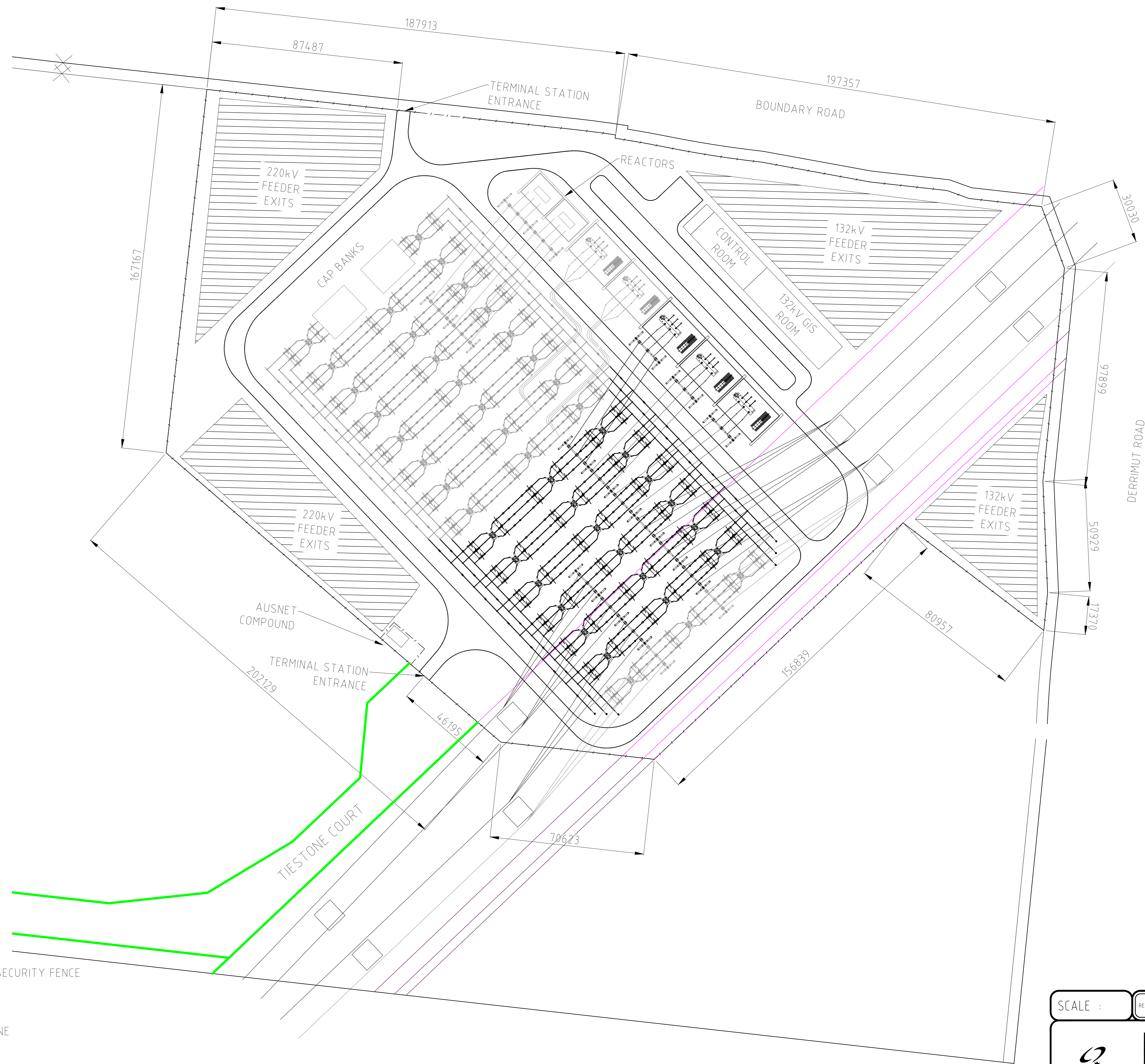
Figure 14 Traffic management recommendations

8 CONCLUSION

Based on the preceding assessment the following can be concluded:

- It is proposed to use the north-eastern portion of the site at 1005 Boundary Road, Tarneit for a new terminal station (utility installation).
- The proposal is expected to generate minimal traffic on a day-to-day basis, with up to four (4) staff members on-site under normal circumstances.
- There are no statutory car parking requirements applicable to the development, rather parking is to be provided to the satisfaction of the responsible authority. Adequate space is provided next to the control room to accommodate the rare occurrence of staff or maintenance vehicles on-site.
- There are no statutory bicycle parking requirements applicable to the development. No formal bicycle parking spaces are proposed, as is appropriate considering the site location and proposed land use.
- The internal accessways have been designed to accommodate heavy vehicles, which will be permitted to travel in all directions, entering and exiting from either access point.
- Passenger cars may be present on-site in the form of staff cars which will likely travel mainly to/from the control room.
- Pedestrian activity is expected to be very low on-site, with accessways shared with vehicles.
- It is recommended to implement traffic management measures to ensure accessways operate safely for all users including adequate security measures, speed signage and wayfinding signage.

APPENDIX 1 DEVELOPMENT PLANS



- LEGEND:**
- - - - PERIMETER SECURITY FENCE
 - EASEMENTS
 - ROAD OUTLINE
 - FUTURE

REFERENCE	DESCRIPTION

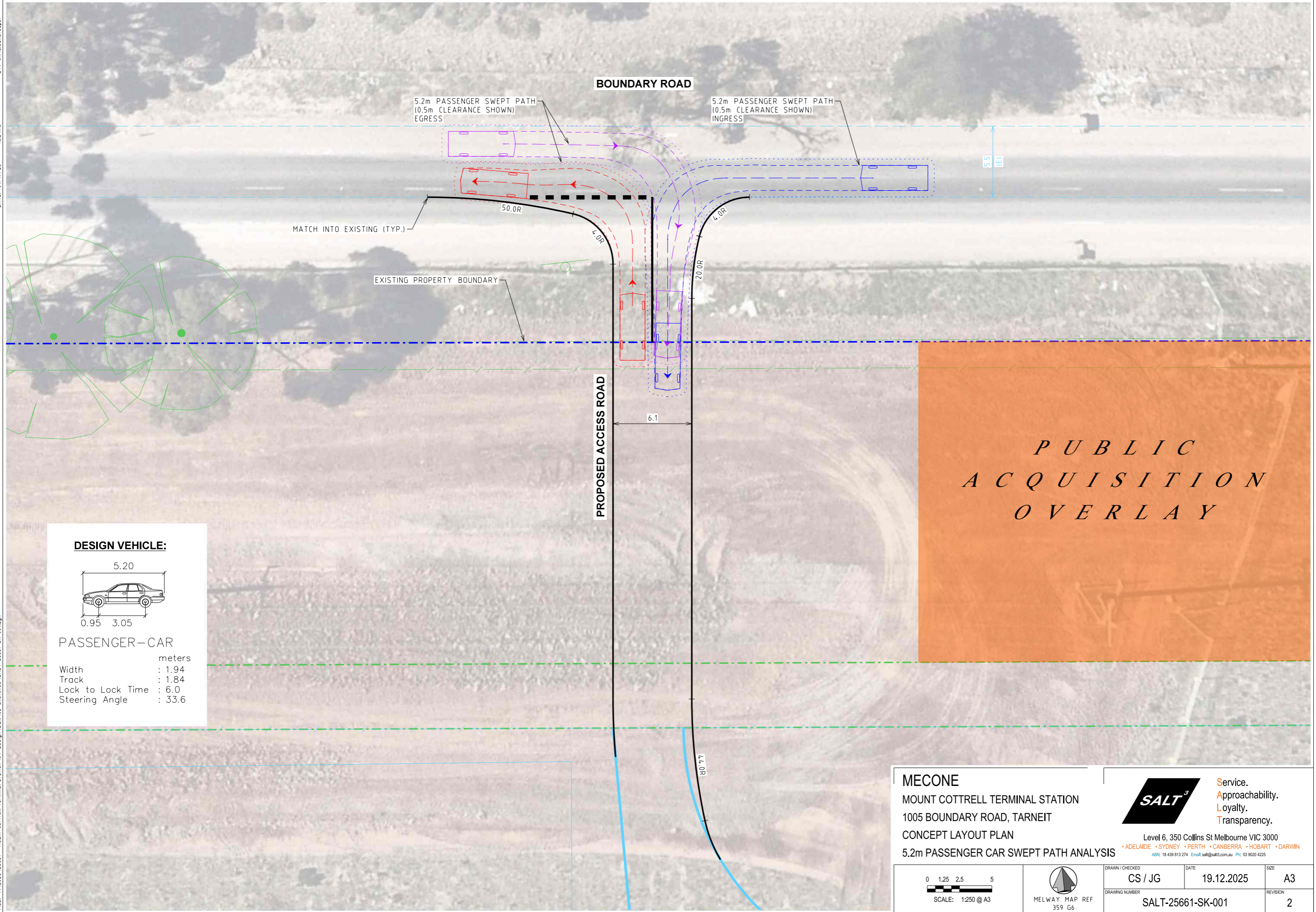
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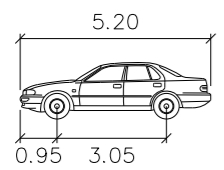
APPENDIX 2 ACCESS DESIGN PLANS

APPENDIX 3 SWEPT PATH DIAGRAMS

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DATE: 07/19/26
16:24:15
FILE: Y:\2025\25661 - Mount Cottrell Terminal Station\04_Design\DGN\02_Sketches\SALT-25661-SK-001.dgn



DESIGN VEHICLE:



PASSENGER - CAR

	meters
Width	: 1.94
Track	: 1.84
Lock to Lock Time	: 6.0
Steering Angle	: 33.6

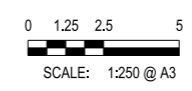
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ACQUISITION
OVERLAY*

MECONE
MOUNT COTTRELL TERMINAL STATION
1005 BOUNDARY ROAD, TARNEIT
CONCEPT LAYOUT PLAN
5.2m PASSENGER CAR SWEPT PATH ANALYSIS



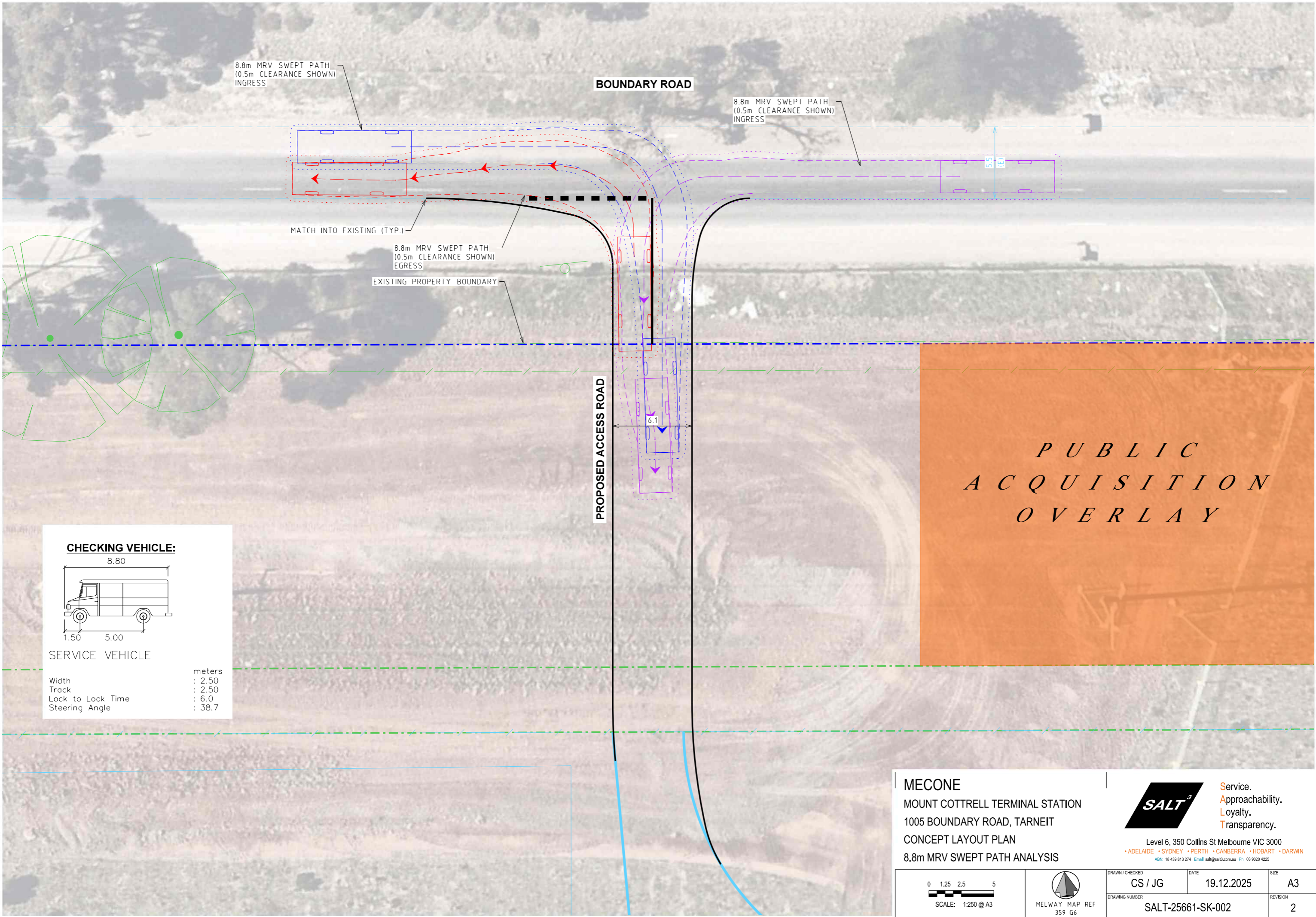
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8.8m MRV SWEPT PATH
(0.5m CLEARANCE SHOWN)
INGRESS

BOUNDARY ROAD

8.8m MRV SWEPT PATH
(0.5m CLEARANCE SHOWN)
INGRESS

MATCH INTO EXISTING (TYP.)

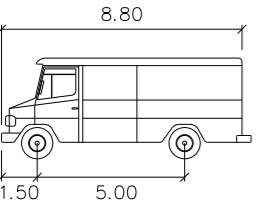
8.8m MRV SWEPT PATH
(0.5m CLEARANCE SHOWN)
EGRESS

EXISTING PROPERTY BOUNDARY

PROPOSED ACCESS ROAD

*PUBLIC
ACQUISITION
OVERLAY*

CHECKING VEHICLE:



SERVICE VEHICLE

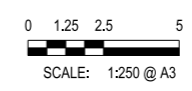
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Width	: 2.50
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Steering Angle	: 38.7

MECONE
MOUNT COTTRELL TERMINAL STATION
1005 BOUNDARY ROAD, TARNEIT
CONCEPT LAYOUT PLAN
8.8m MRV SWEPT PATH ANALYSIS



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