

# ST KEVIN'S COLLEGE - GLENDALOUGH CAMPUS

TOWN PLANNING REPORT  
December 2020

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## PROJECT TEAM

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LANDSCAPE	Tract Consultants
TRAFFIC AND CAR PARKING	GTA Consultants
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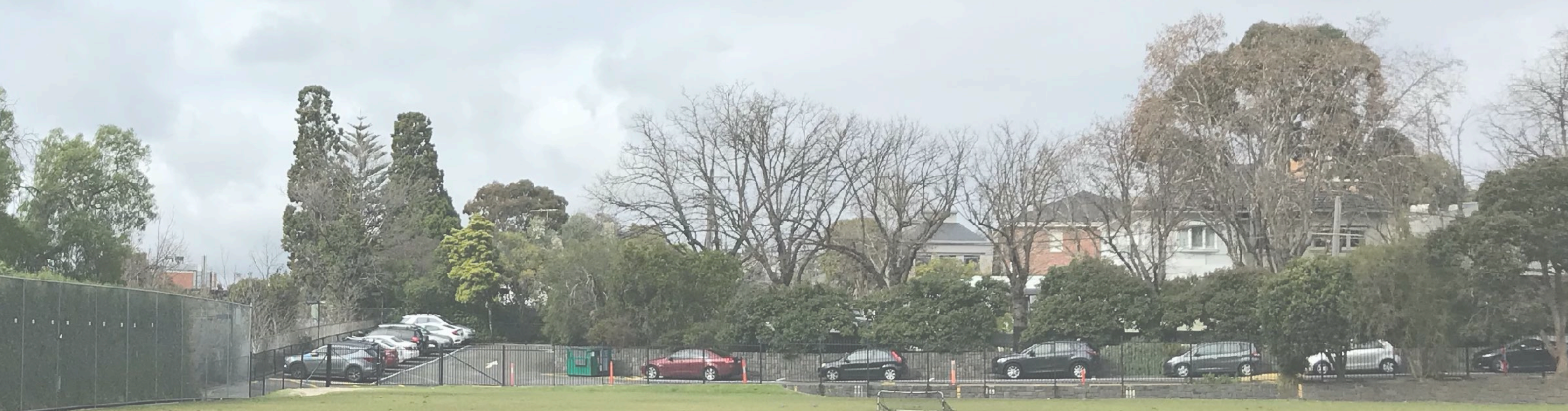
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# 1.0

## INTRODUCTION

This planning report has been prepared on behalf of the permit applicant - St Kevin's College Glendalough Campus (Glendalough Campus) and accompanies the development submission for the junior campus of the school, located at 73 – 75 Lansell Road, Toorak. The proposal seeks to upgrade and enhance the school's services and facilities and is fundamental in ensuring that the Glendalough Campus continues to deliver a high quality teaching and learning environment to address the future educational needs for the short, medium and long term benefits of its students and the broader community.

The purpose of this report is three fold. The first section sets out to examine the site and its context, its historic development and buildings and details the Glendalough Campus' current operations. This analysis feeds into the second section, which aims to establish the vision for the school moving forward recognising the opportunities and constraints of the campus and the design outcomes. This section also provides an outline of the land use and development as proposed and details the current planning policy framework relevant to the consideration of this proposal. The third section of this document provides an analysis of the accompanying design proposal against the policy framework, as well as associated impacts on the significant values of the site and surrounds, car parking and traffic assessment and the landscape design response.

Chandler Architecture have developed a carefully considered design concept for the Glendalough Campus having had regard to the site constraints and specific brief requirements. The proposed development involves a new built form that will provide for improved and efficient administrative and teaching spaces as well as a new on site basement car park that will cater for all parking needs of the school. The proposed development is considered to effectively integrate with the existing residential context, sensitively address the interface with the Yarra River as well as enrich the educational offering afforded at the Glendalough Campus.

The proposal also positively responds to the objectives governed by Planning Policy Framework, local planning policies of the Stonnington Planning Scheme, the Design and Development Overlay Schedule 3 that is relevant to the Yarra River interface and other detailed requirements including the General Residential Zone and Significant Landscape Overlay that collectively guide the future development of the site.

In line with the City of Stonnington's aspirations to support a thriving educational and employment base, the proposal advances a high quality design that will assimilate comfortably within this established residential context of Toorak so as to allow the Glendalough Campus to continue to cater for primary school needs in line with modern day educational requirements.

*“... to deliver a high quality teaching and learning environment to meet the future needs of the College...”*



## 2.0

# BACKGROUND

### 2.1 HISTORY OF ST KEVIN'S COLLEGE

St Kevin's College is one of the many schools throughout the world established by the Christian Brothers. The first school was founded in 1802 by Edmund Rice in Waterford, Ireland. Edmund's followers arrived in Melbourne in 1868 – led by Br Ambrose Treacy. Today, St Kevin's College is governed by Edmund Rice Education Australia.

In 1918, St Kevin's College opened in East Melbourne, as a central matriculation college for other existing Christian Brother's schools in Melbourne. In 1932, the College moved to Toorak on the corner of St. Georges Road and Orrong Road. In 1960, the Senior School was established at 31 Moonga Road, Toorak, commonly referred to as the Heyington Campus. The Heyington Campus accommodates the main reception and administrative facilities of the College. The Junior School remains on the original Toorak site.

Following the issuance of a Planning Permit (ref: 70/261) in 1972, the junior school for boys (Glendalough Campus) was opened on Lansell Road, Toorak. In 1999, the Glendalough Campus became a full primary school, catering from preparatory to year 6. This campus has evolved over time, including building programs so as to provide its pupils with modern, purpose built facilities. Namely, the last major expansion (allowed under Planning Permit (ref: 1013/11) was completed in 2014 and involved the construction of a 'Music House' – commonly referred to as the Victor McMahon Music Centre.

### 2.2 GLENDALOUGH CAMPUS

The Glendalough Campus has been operating at 73-75 Lansell Road since 1972 and for the most part, operates independently from the the Heyington Campus. The school facilities at both campuses are accessible to the pupils and are within easy walking distance of one another.

A review of existing records confirms that there is no formal cap on student numbers or staff. At present, St Kevin's College comprises over 2000 students from ELC to Year 12. This includes 521 students at the Glendalough Campus (preparatory to year 6) and 67 staff (part and full time).

A total of 35 car spaces are currently provided on the Glendalough Campus. This includes limited staff car parking located at the site frontage on the southern and eastern boundaries of the site. A drop off/ pick up zone for parents is located on the southern boundary adjacent to Lansell Road, with access from and egress to Lansell Road.

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# 3.0 URBAN CONTEXT

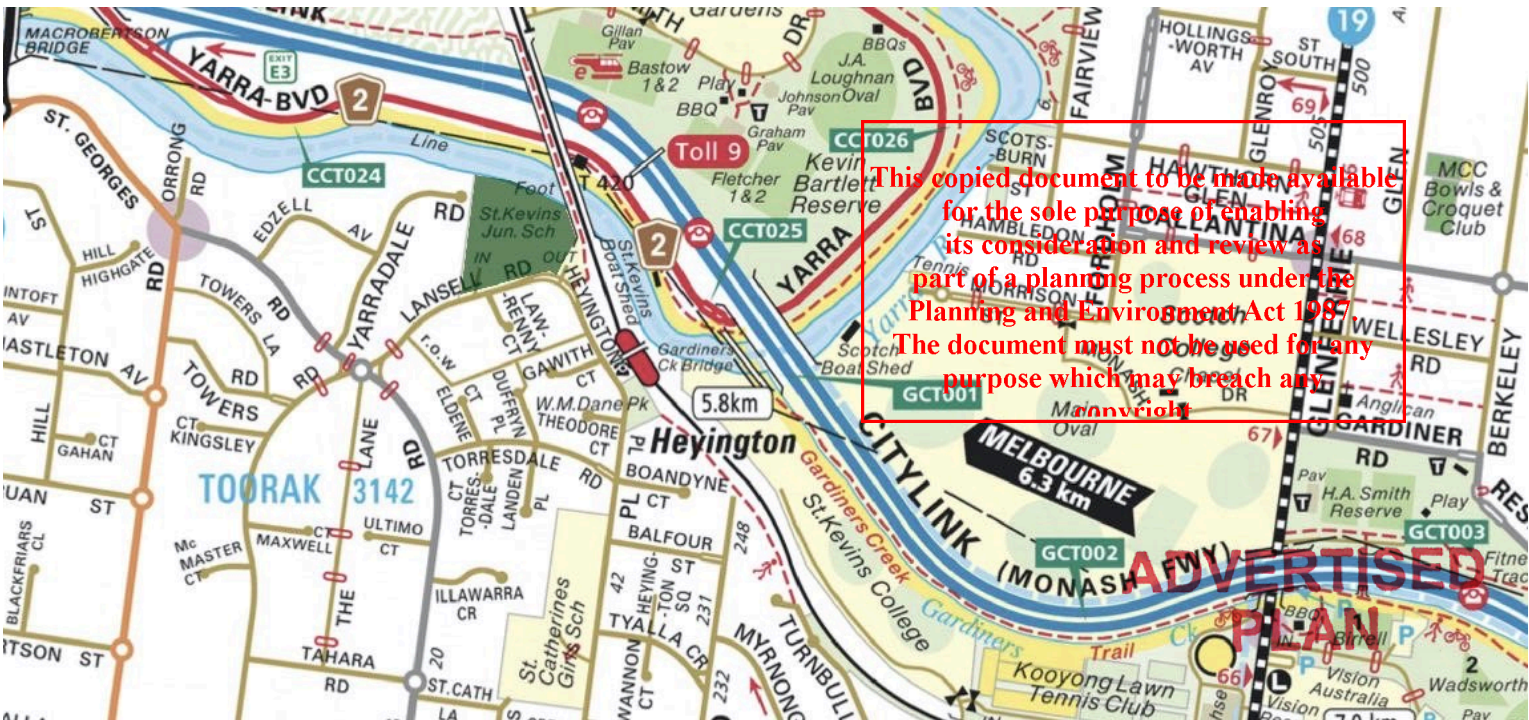
## 3.1 SURROUNDING CONTEXT

The site is located on the northern side of Lansell Road, approximately 250 metres north east of St Georges Road, Toorak. It is bound by the Yarra River and its environs to the north (including land owner and is owned by Melbourne Water), Lansell Road to the south, Yarradale Road to the west and a fixed railway line to east. The Citylink and the Monash Freeway extends further north on the opposite side of the Yarra River.

The site is well positioned in terms of its proximity to public transport facilities including direct connections to fixed rail services with Heyington Railway Station located approximately 290 metres to the south east and extensively used by the staff and students of the Glendalough Campus on a daily basis. The site is also well connected to major arterial roads including the Monash Freeway and surrounding main roads.

The residential nature of the wider area is broken up with the presence of St Kevin’s College (including the senior campus), Kooyong Tennis Club and St Catherine’s Girl School which form part of an enclave of educational and recreation facilities within the immediate area. Scotch College is also located further to the north within the City of Boroondara as well as higher order educational facilities including campuses of the University of Melbourne and Swinburne University which are not far from the subject site.

Further south of the site is the Toorak Road Neighbourhood Activity Centre (Large) and further south west of the site is the Chapel Street Principal Activity Centre.



- 1. View of the Yarra River and its environs to the north of the site
- 2. View of the Monash Freeway located to the north and east of the site
- 3. Heyington Station is located within 300 metres of the site and features a footbridge providing connections to St Kevin's College Heyington Campus
- 4. St Kevin's College - Heyington Campus



### 3.2 SURROUNDING STREETS

The Glendalough Campus has been in operation since 1972 and exists within an established residential neighbourhood predominantly characterised by a mixture of detached and semi-detached dwellings of varying architectural styles and scale. A number of multi – unit buildings are dispersed throughout the area, these are predominantly in the form of 1960s – 1970s flats but also include some more recent apartment development. There are no notable or highly consistent built form features of the area in terms of allotment size, development pattern, layout, form or presentation of dwellings.

The eastern boundary of the site has the only immediate abuttal to residential properties. A driveway to several dwellings at 71 Lansell Road abuts the site's south eastern boundary, noting that there are windows and balconies that have an orientation towards the school. The property at 22 Yarradale Road accommodates a two storey detached dwelling with landscaping positioned adjacent to the common boundary with the school. The principal private open space associated with this dwelling is located to the south. At 25 Yarradale Road, the land is developed with a single storey dwelling with a driveway positioned adjacent to the common boundary with the school.

Lansell Road runs along the southern boundary of the campus. It is a local street with a single traffic lane in each direction and kerbside parking along both sides of varied restriction. The land on the southern side of Lansell Road is residential comprising both large single detached dwellings on landscaped allotments as well as unit and apartment buildings.

Having had regard to its established residential context, it appears that the area is unlikely to experience substantial change with the exception of infill development – as already evidenced with the emergence of new contemporary medium density multi unit developments.



1. View of sites' western boundary from Yarradale Road
2. No. 25 Yarradale Road, Toorak
3. No. 23 Yarradale Road, Toorak
4. No. 22 Yarradale Road, Toorak
5. View of Lansell Road looking west from Heyington Place
6. View of properties on the southern side of Lansell Road
7. Units at No. 48 Lansell Road, Toorak
8. No. 71 Lansell Road, Toorak



3.3 GLENDALOUGH CAMPUS

The campus is currently developed with a series of two storey buildings primarily constructed of brick and built originally in the 1970s with an addition in the mid 1980s and more recently the Boyd Egan Hall and the Victor McMahon Music Centre in 2014. The Glendalough Campus' principal entrance including pedestrian and vehicle entries are via Lansell Road. Existing play areas and open space are apportioned to the north with a number of tennis courts, the central quadrangle and the play equipment and main oval space covering the eastern half of the site. Vegetation is largely concentrated along the northern and eastern site boundaries with some screen planting and dispersed canopy trees to the southern interface with Lansell Road and the play spaces.

The existing buildings are generally focused toward the south western corner of the allotment. As previously outlined the site has experienced an evolution of built form since the 1970's, designed to improve existing school facilities in discrete stages, whilst working within the constraints of the site.

The school continues this approach to address modern education requirements and the current stage of development will take the campus forward for the next decade.



- 1. View of Boyd Egan Hall and Victor McMahon Music Centre looking east from Lansell Road
- 2. View of Victor McMahon Music Centre looking north from Lansell Road
- 3. View of Victor McMahon Music Centre looking north from Lansell Road
- 4. View of administrative building looking north from Lansell Road
- 5. View of side (east) elevation of administrative building
- 6. View of rear (internal north) elevation of administrative building



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1. View of existing vehicle crossover (entry) via Lansell Road to drop off/pick up zone
2. View of existing drop off/ pick up zone and on site staff parking
3. View of existing school oval looking from north to south
4. View of existing school oval looking from north to east
5. View of existing school buildings and play areas looking from north east to south west





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## 4.0 PROJECT VISION

### 4.1 VISION STATEMENT

The future of the Glendalough Campus remains focused on educating the primary aged students of St Kevin's - being prep through to grade 6. The school is not focused on any significant growth in student or staff numbers, but rather, is seeking to build upon both educational and pastoral aspirations, creating a dynamic, supportive and progressive environment for young boys to grow and develop.

To achieve this vision, the project is dedicated to delivering a number of benefits to both the operational side of the school and the creation of additional learning spaces. These objectives include:

- 1. Consolidation of educational support facilities to offer improved services to students in need of Extra Support.**
- 2. Create dedicated class rooms to deliver a STEM program for 21st century primary curriculum.**
- 3. Allow for a third stream of Year 4 to improve the balance of the student body and allow the boys to be integrated across three classes in Year 4.**
- 4. Deliver new facilities for staff including meeting and conference room for permanent staff, parents and visiting specialist staff.**
- 5. Improve the safety and efficiency of student drop off and pick up by developing new traffic management arrangements.**
- 6. Develop greater recreation space for boys that can be used across the seasons.**
- 7. Create a dedicated library and literacy space.**
- 8. Provide for all car parking needs of the Glendalough Campus staff and a portion of the Heyington Campus staff to reduce impacts on the surrounding road network and residential community.**

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### 4.2 SITE OPPORTUNITIES

The campus has been able to accommodate change over the last decade with the addition of the two most recent buildings between 2009 and 2014. It is considered that the layout of the oval and presentation of the school to the streetscape again provides opportunity for modest changes when viewed from the surrounding area. The key opportunities identified at the beginning of the design process include:

- The site encompasses under-utilised parts of the campus such as the oval.
- The site features limited direct interfaces with residential properties.
- The site comprises limited vegetation that is of significance across the play spaces.
- The site is developed with existing two storey buildings and there is opportunity to better utilise and foster greater efficiencies for teaching spaces and facilities within these buildings.
- The site is well established as an education centre in this residential setting.
- The site features direct connections to the fixed rail line.
- On site car parking drop off/ pick up arrangements are extant with clearly defined access points already established.

### 4.3 SITE CONSTRAINTS

It is acknowledged that any school campus within an established residential area and with an interface with the Yarra environments requires careful consideration. The key constraints that have informed the design response include:

- This physical context of the Yarra River environs along the boundaries requires a built form response that addresses this sensitive interface.
- Along the periphery of the site exists significant landscaping and native vegetation.
- The immediate surrounding area is characterised predominantly by lower scale residential properties.
- Access points to the site are limited to Lansell Road and the existence of established street trees.
- Levels across the site require careful placement of new structures.





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## 5.0 THE PROPOSED WORKS

The purpose of this section is to outline the proposed land use and development of the site. The overall intent of this submission focuses on two key aspects. Firstly, the need to deliver the school body and teaching facilities improved education spaces to meet current standards and continue to provide for the appropriate balance across the student body to foster the personal growth and wellbeing of the boys at the later primary school stages.

Secondly, the requirement to ensure efficient vehicle parking and access that will accommodate the ongoing car parking needs of staff and parents. This aspect of the project has been focused on ensuring that the Glendalough Campus takes full responsibility for the car parking needs of the overall operation and limits, where possible, any impact on the broader residential area.

As with all institutions, the Glendalough Campus must continue to modify and upgrade its facilities in order to stay abreast of educational innovations, to provide superior facilities and grounds for its students and teaching staff and meet the expectations of the school community. Of equal importance, the Glendalough Campus is highly cognisant of its relationship with and proximity to the Yarra River environs as well as neighbouring residential properties and appreciates that careful consideration is required with regard to the siting of buildings to ensure the interfaces are appropriately managed.

The proposal involves a series of elements and works designed to substantially improve existing facilities and services provided to the students as well as works to better manage traffic and car parking conditions in the surrounding streets. The individual elements of the application are summarised below and should be read in conjunction with the plans and reports that accompany the application.

### ADDITIONAL STAFF AND STUDENTS

In terms of the current land use activity, the Glendalough Campus proposes to add one additional class, specifically a new Year 4 comprising a maximum of 26 students. The new class would require the addition of one (1) full time teaching staff member. The staff and students will be distributed throughout the campus as is currently the case.

As a result of this additional class the student body will increase from 521 students currently to 547 students (noting that there is no restriction on the total student numbers that can be accommodated on campus). Staff (including all visiting music staff) will increase from 67 at present, to 68.

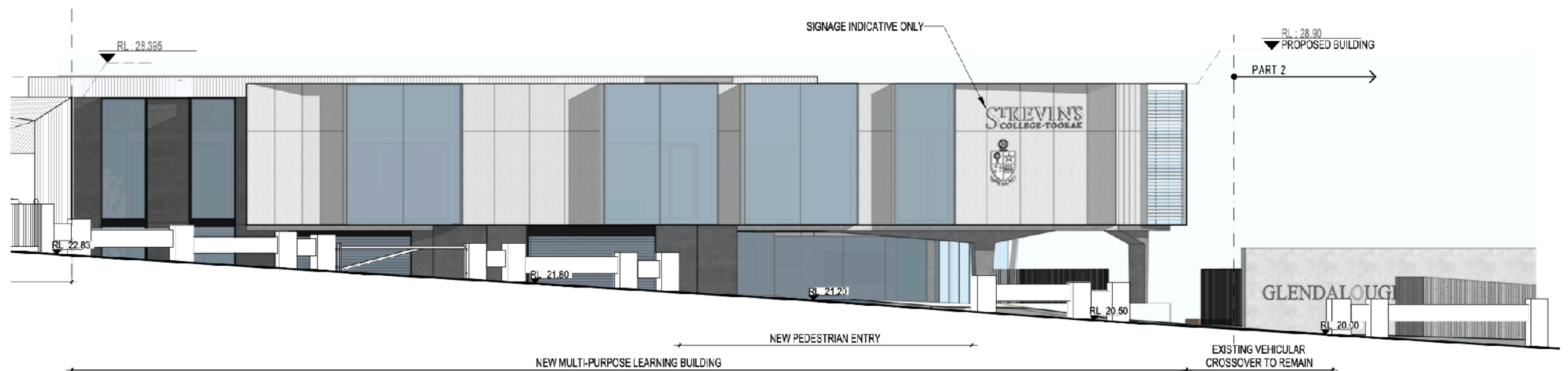
### NEW ADMINISTRATIVE OFFICE AND TEACHING SPACES

The proposal involves the partial demolition of the existing two storey administrative building (external walls only) which currently faces Lansell Road. On the southern side of this existing building will be a new two storey structure which will accommodate new teaching spaces, a dedicated library, class rooms and appropriate staff amenities.

Careful consideration has been given to the siting, architectural design and scale of the proposed new structure and its presentation to Lansell Road. Key features in this regard includes:

- The building will have a combined width of approximately 39 metres, a depth of approximately 28 metres.
- An overall height of approximately 8.88 metres.
- The average building facade setback from Lansell Road will be 8.1 metres.
- The use of the building will be during normal school hours.

The plan package prepared by Chandler Architecture provides detailed images of the building presentation to the streetscape of Lansell Road together with floor plans, elevations and sections of the combined structure.





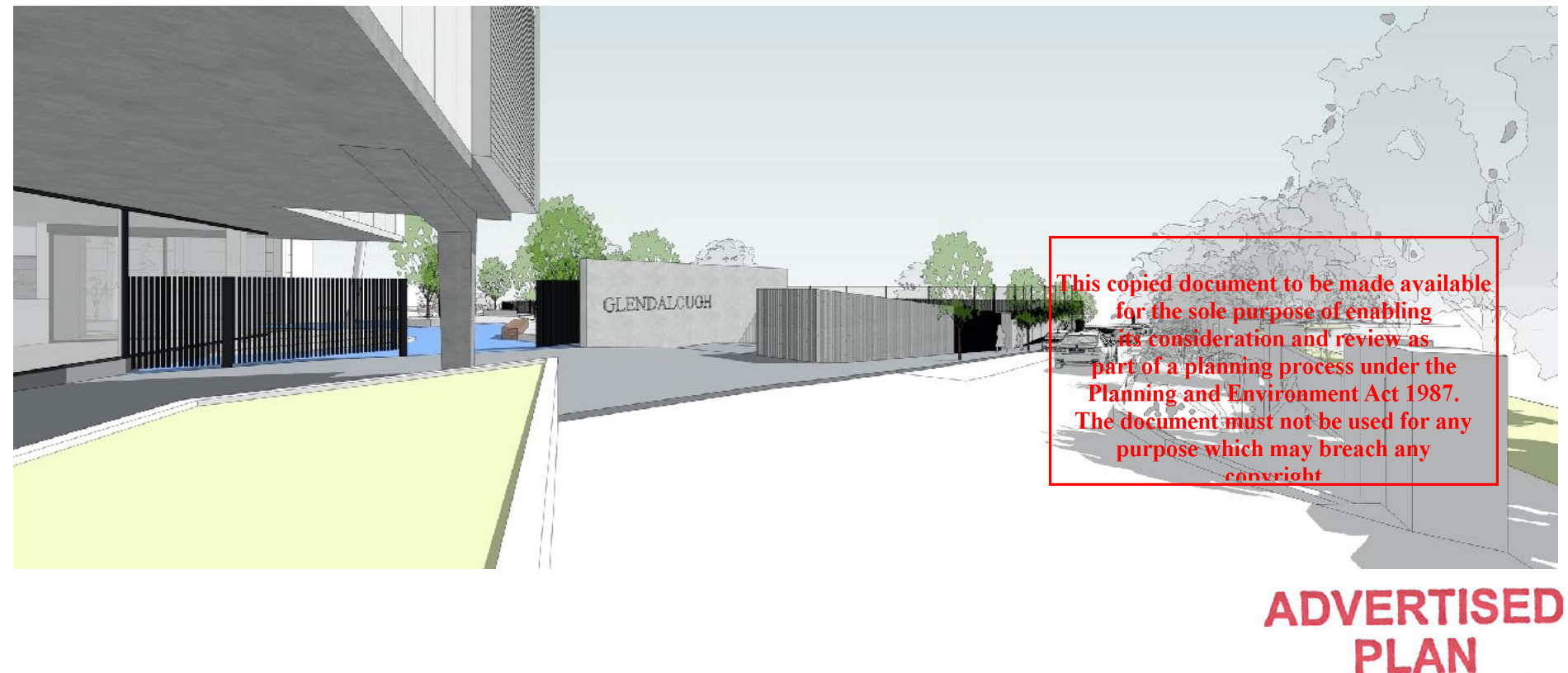
## NEW DROP OFF/ PICK UP ZONE

The existing drop and pick up arrangements, albeit managed carefully within the curtilage of the site to limit impacts on the street network, will be substantially improved as part of the project. The layout of the drop off/ pick up will be reconfigured so as to accommodate 16 new car parking spaces as well as a new lane for passing vehicles.

The drop off/pick up facilities will continue to enter and exit from Lansell Road in a one way movement consistent with the existing conditions and existing points of access. The new layout will then allow for vehicles dropping off or collecting students to move into one of the designated spaces efficiently. At pick up time the school management will call the relevant student from the assembly area to load into the vehicle and once safely on board, the car will be able to leave the school using the access lane.

As outlined in the transport and car parking analysis prepared by GTA Consultants that forms part of the application documentation, there are a number of benefits to this new drop off/ pick up zone including:

- the designated parallel car spaces are designed to allow vehicles to easily enter and exit the space in one movement;
- the access lane will ensure vehicles can leave once the student had boarded rather than the current system that requires all front vehicles to leave first;
- there is a grade separated pathway along the side of the drop off and pick up zone for improved student safety and efficient movement;
- a dedicated traffic warden who will be responsible for keeping vehicles moving through the drop off zone and minimising impacts of traffic on the surrounding streets; and
- the assembly area is a larger space than currently, closer to the drop off pick up zone and with shelter to ensure it can be used at all times of the year. This will aid in the time between students are called up and boarding the vehicle.



## NEW BASEMENT CAR PARK

A key aspect of this project involves the construction of a basement car park which is to be under the existing oval located on the north eastern portion of the site. The car park will accommodate a total of 133 car spaces that will service all staff, visitors and any parents attending the Glendalough Campus throughout the day. It will provide additional parking to the Heyington (senior) Campus close by and will provide parking for parents and staff during after school events. The total net increase in car parking for the Glendalough Campus will be 115 spaces from existing conditions.

Above the proposed car park, the oval will be replaced with synthetic turf with new playing fields. The playing surface will be level and provide improved drainage allowing it to be used throughout the year.

This aspect of the proposal will be completed first, and is proposed to be managed in a way so as to minimise disruption to the school operation and importantly the surrounding streets.





## LANDSCAPING

The landscape works across the site have been designed by Tract consultants in association with Chandler Architecture. To the boundaries of the campus and to the edge of new structures, landscaping changes from existing conditions will include:

- New landscaped garden beds and planting to the front entrance of the new administration building;
- Landscaping along the edge of the car park structure as it interfaces with the pick up zone to provide for a green wall;
- Landscaping along the eastern boundary of the site adjacent to the new car park structure to provide screen planting adjacent to the public walkway and to enhance the landscape character of the surrounding area.

The key landscape works proposed within the Glendalough Campus itself will be an important part of the project to ensure the highest quality play spaces and interactive outdoor learning areas are provided for students. In the centre of the site, between the existing quadrangle and the new oval that will be laid on top of the car park structure, will be tiered steps providing for amphitheatre style seating for students. Either side of these steps will be new play equipment structures, sandpits, and other interactive play spaces.

## VEGETATION REMOVAL AND RETENTION

The design response for all proposed buildings and works across the campus has been informed by the desire to retain significant vegetation. This includes the following:

- Along the north eastern corner of the site is a stand of 3 large eucalypts. These trees are within the boundary of the school land and considered to contribute to the interface with the River and public path. As such the proposed car park structure has been designed to ensure the retention of these trees.
- Along the Lansell Road frontage adjacent to the pick up and drop off zone is continuous row of large shrubs, which provide a dense vegetation screen from the road. This screen planting together with existing fencing and garden bed are to be retained.
- Street trees remaining along Lansell Road are to be appropriately protected with levels and structures within the title boundary designed to limit excavation.

A detailed arboricultural assessment by Arbor Safe has been prepared to address this vegetation and the plans prepared by

Chandler Architecture provide detail regarding tree protection zones for the above.

It is acknowledged that some vegetation within the site boundary around the playground/ and recreational space are required to be removed. The loss of vegetation has been minimised and will be replaced as part of the landscape design prepared by Tract Consultants to ensure that the canopy tree planting remains an important feature for the school moving forward. A total of 31 new canopy trees are to be planted as part of the scheme resulting in a net increase in vegetation across the campus.

## STAGING OF DEVELOPMENT

Given the nature of the project, the proposed development will occur in four planned stages. This is critical in allowing the school to continue to function at an optimal level whilst also minimising off site amenity impacts.

The applicant has advised that the construction is expected to occur over a period of 18 months under four key stages. Please refer to Drawing TP11 rev C in the plan set which provides information with regard to works being undertaken during each stage.

It is noted that the drop off/ pick up facility and associated civil works will be a priority with limited operation on street so as to ensure that Lansell Road remains clear of transport movements in an efficient manner.

It is anticipated that should a planning permit be issued, that a traffic management plan will be necessary to address temporary management of vehicles during the construction phase.

## SEWERAGE EASEMENT

The sewerage / drainage easement as detailed on the title particularly runs through the site. This easement was recently varied following an extensive upgrade to the main sewer line in 2020. It is now positioned such that the proposed basement car park, the expanded new administration building or the revised drop off zone will have no impact and no variation to the easement is required.

## SIGNAGE

This application does not involve any new business identification signs associated with the Glendalough Campus. Any future signage will be subject to a separate planning application.

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## 6.0 PLANNING POLICY FRAMEWORK

The provisions of the Stonnington Planning Scheme govern the land use and development of the subject land. The following summary outlines the key planning policies and controls that affect the proposal and are relevant to the consideration and determination of this planning permit application.

### 6.1 PLANNING POLICY FRAMEWORK

The Planning Policy Framework (PPF) of the Stonnington Planning Scheme seeks to develop the objectives for planning in Victoria (as set out in the Act) to foster appropriate land use, development planning, policies and practices that encompass relevant environmental, social and economic factors.

Of relevance to the proposed redevelopment of the Glendalough Campus within the existing built environment of Toorak are the policies relating to the operation of the Planning Policy Framework (Clause 10), Settlement (Clause 11), the Built Environment and Heritage (Clause 15), Transport (Clause 18) and Infrastructure (Clause 19).

The key policy objectives that are derived from these relevant sections of the PPF and which relate to the proposal include the following:

- To facilitate sustainable development that takes full advantage of existing settlement patterns, and investment in transport and communication, water and sewerage and social facilities.
- To ensure a sufficient supply of land is available for residential, commercial, retail, industrial, recreational, institutional and other community uses.
- To achieve architectural and urban design outcomes that contribute positively to local urban character and enhance the public realm while minimising detrimental impact on neighbouring properties and surrounds.
- Create compact neighbourhoods that have walkable distances between activities and where neighbourhood centres provide access to services and facilities to meet day-to-day needs.

- Ensure development responds to its context and reinforces the special characteristics of the local environment and place by emphasising the underlying natural landscape character, heritage values and built form that reflect community identity and the values, needs and aspirations of the community.
- Ensure the location of community based facilities gives consideration to demographic trends, the existing and future demand requirements and the integration of facilities into communities.
- Locate activities to maximise access by walking and cycling.
- To ensure an adequate supply of car parking that is appropriately designed and located for all new development.

Clause 19.02-2S (Education facilities) of the PPF specifically addresses education centres with the overarching objective to “assist the integration of education and early childhood facilities with local and regional facilities”. The desire to see primary schools well connected with the local residential community and public transport networks supports the proposition that the integration of these types of land uses are important to the creation of well rounded neighbourhoods including existing and future demand requirements.

The strategies outlined in this Clause that are of specific relevance to the application includes:

- *Ensure childcare, kindergarten and primary school and secondary school facilities provide safe vehicle drop off zones.*
- *Facilitate the establishment and expansion of primary and secondary education facilities to meet the existing and future education needs of communities.*
- *Recognise that primary and secondary education facilities are different to dwellings in their purpose and function and can have different built form (including height, scale and mass).*

Plan Melbourne (2017-2050) is also relevant to the consideration of this application. Outcome 4 and Outcome 5 of the document is focused on building liveable communities and has a series of policy directions that promote diverse and interconnected neighbourhoods where a range of services and facilities including education facilities

are highly accessible, encouraging sustainable and healthy communities.

In response to these policies the overall approach to new development within established precincts as well as growth areas must respond by:

- Creating neighbourhoods that vary in density and land uses and are focused on the concept of ‘living locally’. This approach seeks to create a series of ‘20 minute neighbourhoods’ and is based on giving people the ability to meet most of their everyday needs within a 20 minute walk, cycle or local public transport trip from their home.
- Promoting a healthy, safe and vibrant network of neighbourhoods that are interconnected, promotes walking and cycling and ensure the community have access to a broad range of services and facilities.
- Ensuring new development promotes design excellence and where relevant, respects our heritage built form that contributes to the character of an area.

### 6.2 LOCAL PLANNING POLICY FRAMEWORK

The clauses of the MSS and local policies, which are relevant to the consideration of this application include:

### 6.3 MUNICIPAL STRATEGIC STATEMENT (MSS)

The MSS of the Stonnington Planning Scheme sets out at Clause 21.03, the vision for the municipality going forward and the intended approach for managing changes in land use, built form, infrastructure and the environment. This long-term vision, which has been developed from Stonnington’s Council Plan states:

Stonnington will be a place of community, individuality and business where an environment is created that fosters the hopes, well-being and aspirations of all people.

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To achieve this vision and the associated mission statements, the MSS focuses on five strategic themes which seek to guide development across the municipality. These themes include:

- Economic Development
- Housing
- Built Environment and Heritage
- Environment and Open space
- Infrastructure

These themes seek to reinforce existing strengths of the municipality, identify new opportunities for development and business opportunities, and minimise any negative impacts of future use and development.

Clause 21.04 is focused on Economic Development. This Clause addresses the need for activity centres and mixed use precincts to provide for the needs of the local community. The objectives include encouraging business that will provide services and employment opportunities for the local community, including:

- high technology, communication and distribution services
- services associated with the education, health, medical and professional services
- services associated with creative / artistic activities and visitor accommodation

The broad policy intent of this Clause is consistent with Plan Melbourne, seeking to ensure that the community has convenient and efficient access to a range of social infrastructure and services including education facilities. This is reinforced at Clause 21.04-4 which provides some guidance with respect to appropriate locations for community uses and seeks to “ensure non-residential uses in residential zones are located to achieve maximum accessibility to the communities they serve and respect the preferred character of the area and residential amenity”.

Clause 21.06 is focused on Built Environment and Heritage and establishes a number of strategies to address the changing scale and form of new development within the municipality, including areas which have recognised heritage value. These strategies are focused on the following key themes:

- Overall urban structure
- Landscape character
- Amenity
- Built form character
- Public realm and pedestrian areas
- Solar access and wind protection
- Noise and air quality
- Energy, water and waste efficiency
- Designing for safety, universal access and social inclusion
- Heritage

Whilst a number of the key issues, objectives and strategies contained within this Clause are generally focused on different forms of new housing, the fundamental principles are equally applicable to non residential activity. The Clause seeks to ensure new development and consolidation of existing activities are focused on land that is well connected with activity centres, on main roads and principal public transport networks. Objectives to minimise impacts on residential amenity, reinforce landscape qualities and respect the built form character along the Yarra River are relevant considerations for this application.

Clause 21.07 relates to Open Space and Environment including the need to manage the impact of private development in and beside public open space and protection of the environmental and landscape values of vulnerable and exposed land in private development along the edge of the Yarra River and Gardiners Creek.

This Clause outlines a number of strategies to ensure that vegetation removal along the Yarra River environs has minimal impact on the defined landscape and environmental values, protection of significant trees on private property, seek opportunities to increase and replace significant trees and minimise intrusion buildings beside waterways.

Clause 21.08 is focused on the Infrastructure needs and objectives across the municipality and considers the promotion of sustainable transport modes, improved car parking and road access as well as management and location of community facilities.

The key issues impacting on the delivery of community infrastructure are identified at Clause 21.08-5 as:

- Acknowledging the importance of public institutions to the economic and social viability of the City.
- Addressing the potential impacts of many institutional uses on residential amenity in residential areas.
- Providing residents and institutional bodies effective guidance and greater certainty about the development of schools, hospitals and similar facilities, especially in residential areas.
- Addressing future community needs in response to increasing demand and the uneven distribution of community services (currently concentrated in the west of the City).

6.4 LOCAL POLICY

There are three local policies that are of relevance to the consideration of the application. They can be summarised as follows:

CLAUSE 22.05 ENVIRONMENTALLY SUSTAINABLE DEVELOPMENT

Clause 22.05 relates to Environmentally Sustainable Development and applies to all residential and non-residential developments. This

policy directive seeks to ensure developments “achieve best practice in environmentally sustainable development from the design stage through to construction and operation”.

This Clause contains a range of objectives relating to energy performance, water resources, indoor environmental quality, stormwater management, transport, waste management and urban ecology.

CLAUSE 22.16 INSTITUTIONAL USES POLICY

Clause 22.16 applies to 29 different institutional facilities located within the municipality of Stonnington including 4 hospitals (Cabrini Malvern and Prahran, Victoria House and Vision Australia), 2 tennis clubs (Kooyong Lawn and Royal South Yarra) and the balance (23 sites) being primarily primary and secondary schools. St Kevins School is one of these schools.

The policy recognises that the external institutional uses in residential areas of Stonnington is substantial and that they service the needs of not only the local community but also a wider regional population.

Clause 22.16 goes on to note that “these institutions employ a significant number of people and generate economic activity that benefits local businesses. They also are important in ensuring that Stonnington remains a prime residential location for families and students.” This clause states that “masterplans could provide greater certainty in managing future development” of institutional uses. “A masterplan could outline how the site is to accommodate any new buildings, works and future uses. The plan could address built form matters and attempt to pro-actively plan for and deal with issues relating to parking and traffic problems surrounding the institution.”

Clause 22.16-3 does not stipulate that an institution must prepare a masterplan. In cases where “an institution elects not to prepare a masterplan, the requirements of the policy equally apply to any planning permit application for buildings and works relating to the institution (where appropriate).” The Clause goes on to set down those matters that a masterplan or planning permit application should describe or show.

CLAUSE 22.18 - STORMWATER MANAGEMENT (WATER SENSITIVE URBAN DESIGN)

Clause 22.18 promotes the use of water sensitive urban design, including storm water reuse. As well, the use of techniques that improve the quality and reduce the flow of water discharged to waterways is encouraged. Clause 22.18-4 requires that a permit application “be accompanied by a Water Sensitive Urban Design Response” including measures such as a STORM rating report and the harvesting and reuse of collected rainwater.



6.5 ZONING

CLAUSE 32.08 GENERAL RESIDENTIAL ZONE - SCHEDULE 8 (GRZ8)



The site is zoned General Residential – Schedule 8 (Garden Estate). The purpose of the zone includes:

- To implement the Municipal Planning Strategy and the Planning Policy Framework.
- To encourage development that respects the neighbourhood character of the area.
- To encourage a diversity of housing types and housing growth particularly in locations offering good access to services and transport.
- To allow educational, recreational, religious, community and a limited range of other non-residential uses to serve local community needs in appropriate locations.

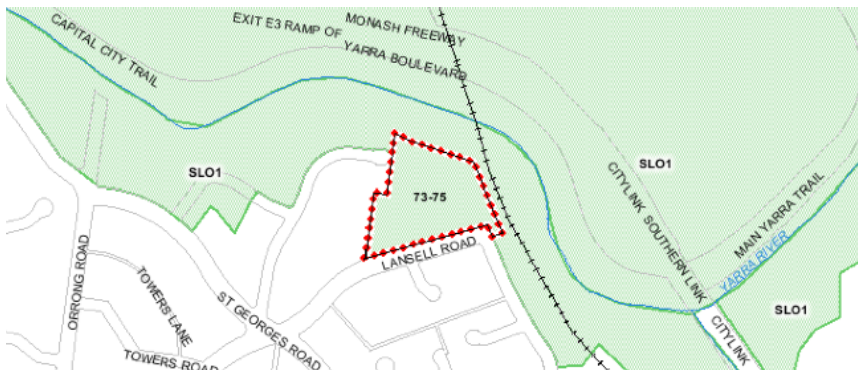
Under the General Residential Zone, the following is applicable:

A primary school is defined as an ‘education centre’. Pursuant to Clause 32.08-2 of the GRZ, a planning permit is required to use the land as an education centre (Section 2 Use). Given the land has been used as a primary school for a period of more than 47 years (continuously), existing use rights can be established and no planning permit is required for the use.

Pursuant to Clause 32.08-9 of the GRZ, a planning permit is required for buildings and works associated with a Section 2 Use

6.6 OVERLAYS

CLAUSE 42.03 SIGNIFICANT LANDSCAPE OVERLAY - SCHEDULE 1 (SLO1)



Schedule 1 of the Significant Landscape Overlay relates to the Yarra (Birrarrung) River Corridor Environs. This Overlay includes landscape character objectives including landscape environmental and cultural values, protection of waterway and riparian zone, public open space access and siting and design of built form.

Pursuant to Clause 42.03-1, a planning permit is required to construct a building or carry out works, to remove, destroy or lop vegetation and to construct a fence within 30 metres of the banks of the Yarra River.

CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY - SCHEDULE 3 (DDO3)



Schedule 3 to the Design and Development Overlay (Yarra (Birrarrung) River Corridor Protection) outlines design objectives in relation to landscape protection, siting and design and site coverage and permeability.

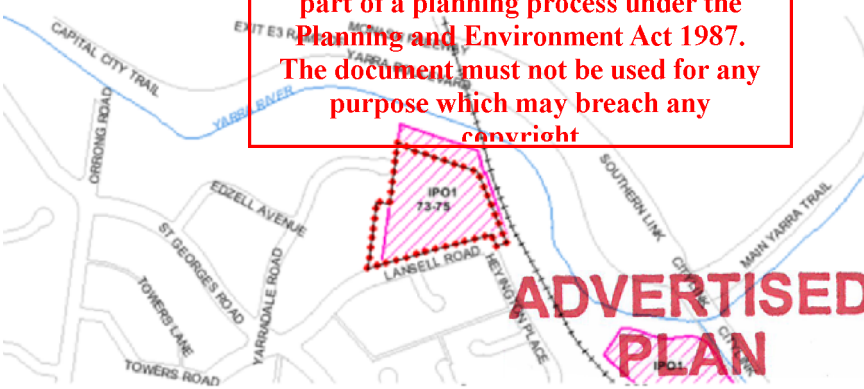
Pursuant to Clause 43.02-2, a planning permit is required to construct a building or construct or carry out works and construct a fence.

In accordance with Section 2.0 of DDO3, the mandatory requirements for DDO3-A are as follows:

- Minimum Setback of 30 metres from the Yarra River measured horizontally in metres from the setback reference line.
- Maximum Height of 9 metres or 10 metres on a sloping site.

It is noted that a referral to Melbourne Water is required in accordance with Clause 4.0 of Schedule 3 to Clause 43.02 to assess the impact of an application for buildings and works on the environmental and waterway values of the Yarra River within 100 meters of its banks.

CLAUSE 43.03 INCORPORATED PLAN OVERLAY - SCHEDULE 1 (IPO1)



The purpose of this overlay is to identify areas which require:

- The form and conditions of future use and development to be shown on an incorporated plan before a permit can be granted to use or develop the land.
- A planning scheme amendment before the incorporated plan can be changed.

A planning scheme amendment is required before the incorporated plan can be changed.

Schedule 1 to the IPO relates to Institutional Uses within Stonnington and sets down the requirements for an incorporated plan.

A planning permit may be granted to use or subdivide land, construct a building or construct or carry out works without an incorporated plan having been prepared or incorporated into the planning scheme. There are a range of decision guidelines that a responsible authority must consider before deciding on a permit application including the local policy at Clause 22.16 – Institutional Uses.

There is no incorporated plan for St Kevin's College, therefore the provisions of this Overlay do not apply.



## 6.7 PARTICULAR PROVISIONS

### CLAUSE 52.06 - CAR PARKING

The provisions of this clause seek to ensure an efficient and appropriate provision of parking is provided to support new and extended land uses. Parking should be well designed and located to not adversely impact on surrounding uses, protect the role and function of the surrounding road network, facilitate the use of public transport and create a safe environment for all users.

A new use must not commence or the floor area of an existing use must not be increased until the required car spaces have been provided on the land.

Where a use is not specified in Table 1, in another provision of this scheme or in a Parking Overlay, car parking spaces must be provided to the satisfaction of the responsible authority. Parking for an education centre is required as follows:

- Education Centre (Primary School): 1 space to each employee that is part of the maximum number of employees on the site at any one time.

The proposal seeks to provide a net increase of 115 car spaces in the form of:

- 133 car spaces within the basement car park.
- 16 car spaces within the pick up and drop zone off Lansell Road.
- 1 DDA car space at the entrance to the building off Lansell Road.

A planning permit is not required for any reduction to the statutory requirement for car parking.

### CLAUSE 52.34 - BICYCLE FACILITIES

The purpose of this clause is:

- *To encourage cycling as a mode of transport;*
- *To provide secure, accessible and convenient bicycle parking spaces and associated shower and change facilities.*

The required number of bicycle spaces associated with the proposed uses is listed in table 1 under Clause 52.34-3. A primary school is required to provide 1 space to every 20 staff and 1 space to every 5 pupils over year 4.

The proposal seeks to provide a net increase of bicycle spaces in the form of 16 bicycle spaces within the basement car park provided in a dedicated and secure zone. This will supplement the existing 11 bicycle spaces on campus currently.

A planning permit is not required for any reduction to the statutory requirement for bicycle parking.

### CLAUSE 53.18 - STORMWATER MANAGEMENT IN URBAN DEVELOPMENT

The purpose of this Clause is to ensure that stormwater in urban development, including retention and reuse, is managed to mitigate the impacts of stormwater on the environment, property and public safety, and to provide cooling, local habitat and amenity benefits.

Clause 53.18-3 states that an application to construct a building or construct or carry out works must meet all of the objectives.

### CLAUSE 53.19 - NON GOVERNMENT SCHOOLS

This clause was recently introduced to the Victoria Planning Provisions as part of Amendment VC 180 and seeks to:

- *To facilitate new non-government schools.*
- *To facilitate upgrades and extensions to existing non-government schools.*

In accordance with the provisions of this clause together with Clause 72.01-1, the Minister is the responsible authority for the application on the basis that the estimated cost of development is greater than \$3 million.

Clause 53.19-2 specifies that an application to which Clause 53.19 applies is exempt from the decision requirements of section 64(1), (2), and (3), and the review rights of section 82(1) of the Act.

### CLAUSE 63 - EXISTING USES

The education centre function of the school has occurred across the campus for an extended period of time. As such a planning permit is not required for use of the site as an education centre. Clause 63.05 requires that a Section 2 use with existing use rights is to obtain a planning permit for any new buildings and works. Accordingly, a planning permit is triggered pursuant to the Clause 63 provisions.

## 6.8 PLANNING PERMIT TRIGGERS

The project requires a planning permit in accordance with the following provisions:

#### Clause 32.08-9 (General Residential Zone)

- To construct a building or carry out works for a use in Section 2 of Clause 32.08-2

#### Clause 43.02-2 (Design and Development Overlay)

- To construct a building or construct or carry out works.

#### Clause 42.03-1 (Significant Landscape Overlay)

- To construct a building or carry out works (above 6 metres in height).
- To remove, destroy or lop vegetation.

## 6.9 CULTURAL HERITAGE MANAGEMENT PLAN

The Glendalough Campus is located within an area of cultural sensitivity. A Cultural Heritage Management Plan is required for high impact activities on land located within an area of cultural sensitivity that has not been subject to significant ground disturbance.

As per the letter prepared by Norton Rose Fulbright (dated 1 November 2019), an exemption applies where:

- The construction or carrying out of works is associated with a purpose listed under s46(1)(b);
- The land was being lawfully used for that purpose immediately before 28 May 2007; and
- The 'land' for the purpose of the exemption is the land upon which the use is being carried out, as opposed to the specific building footprint.

It is considered that in this instance, an exemption applies to the works proposed at the Glendalough Campus on the basis that:

- The proposed works are for, or associated with, the purpose of an education centre, which is a purpose listed under s46(1)(b) of the AH Regulations; and
- The land (being 73-75 Lansell Road, Toorak) was lawfully being used as an education centre immediately before 28 May 2007.

It is submitted that a CHMP is not required under the AH Act and AH Regulations on the basis that the proposed works are exempt under regulation 46(3) of the AH Regulations.

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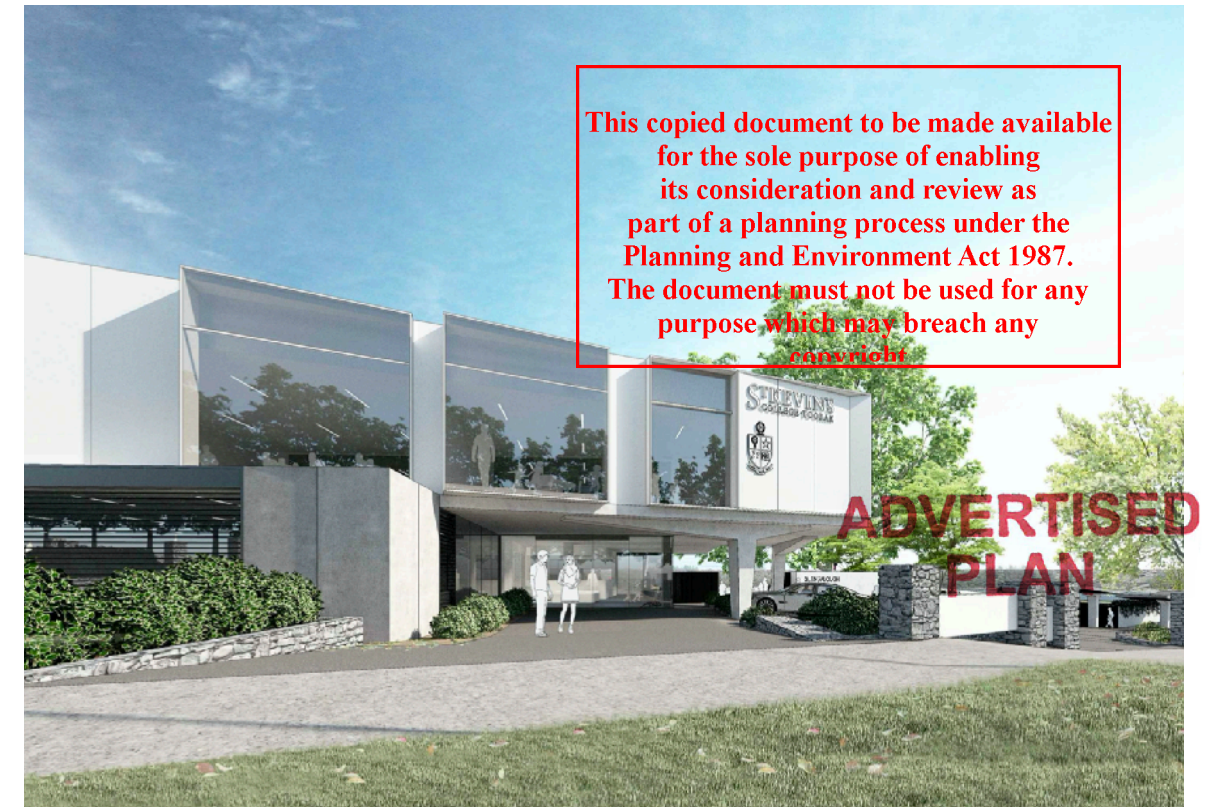
### 7.1 OVERVIEW

Like many independent schools which are located within well established suburbs across Melbourne, the development of the Glendalough Campus of St Kevin's College has been a gradual process, occurring over several decades when opportunities arise to upgrade the various academic and creative arts teaching facilities as well as the sporting and play spaces. This incremental approach to development has the advantage of allowing the surrounding residential community to gradually adapt to the school over time.

The Glendalough Campus has been part of this precinct of Toorak for over 50 years, diversifying the land use mix of the local area throughout this period and contributing to the options for primary education within the broader community. The current proposal to upgrade some of the existing education facilities and provide for significant parking to service staff and visitor needs does not therefore represent a new land use activity or a significant intensification of the existing operation. The focus with this proposal is to improve on the quality of the existing School facilities rather than expansion. It is about rationalising the way the school operates day to day with a specific focus on improved efficiencies and upgrading both the teaching spaces as well as the outdoor recreation areas provided to all students.

A key component of the project is also seeking to significantly improve car parking provided on site to ensure staff and visitors to the campus throughout the day and evening are fully accommodated, establishing a long term solution to an issue that has not been comprehensively addressed to date.

Therefore unlike many permit applications that relate to a non residential activity in a largely residential area, the consideration of this current permit application for the Glendalough Campus is not centred on whether the land use activity is appropriate in the local context. Rather the assessment is about whether the approach to the upgrade of facilities achieves an appropriate balance between meeting the ongoing needs of the school community, the amenity enjoyed by the surrounding residential community and the established character of the built environment. It is an application that must consider whether the proposed development engages and integrates appropriately with the Lansell Road streetscape, whether the rationalisation of activities (including car parking) across the campus will result in improved amenity for the surrounding neighbours, and whether the landscape solution is appropriate for the site and precinct.





## 7.2 RESPONSE TO THE LOCAL PLANNING POLICY FRAMEWORK

The planning policy framework at the local level which provides the applicable guidance for an application seeking to improve and develop an existing education facility is largely contained at Clause 21.06 (Built environment and heritage), Clause 21.07 (Open space and environment) and Clause 21.08 (Infrastructure) of the Stonnington Planning Scheme. Some of the key objectives and strategies contained in these Clauses that are of most relevance to the proposed development seek to ensure:

- Sustainable transport options are integrated to encourage the use of all forms of public and active transport to and from the education use and are encouraged where possible in preference to private vehicle use;
- Adequate measures are in place to minimise the detriment to local amenity from the operation and expansion of institutions;
- The prevailing landscape character of the area is appropriately respected and landscaping is well integrated with any new development;
- The built form character, scale and density of the defined neighbourhood is respected and new development demonstrates a high standard of design;
- Development incorporates sustainable design initiatives that address energy and water use, biodiversity impacts and resource depletion;
- The proposal facilitates social interaction and community inclusion through good design, incorporates informal surveillance of public spaces and allows for good visibility into and within private land.

In essence this application must balance a range of built form, landscape, sustainability and amenity considerations that have the potential to affect the surrounding community and the neighbourhood context with the specific needs of the Glendalough Campus as a long standing education facility within the suburb. This balance effectively must demonstrate a net community benefit as a result of the project overall.

From a planning perspective the application represents a strategic approach to the ongoing management of the Glendalough Campus within its residential context, proposes new built form that will be appropriately located so as to minimise off site amenity impacts and positively responds to the LPPF of the Stonnington Planning Scheme.

More specific policy guidance for institutions across the municipality is also provided at Clause 22.16, St Kevin's College being one of these institutions. However, this policy is focused more on the approach to major development applications where a master plan may be the appropriate tool for documenting future development and growth. The key objectives of this policy however focus on both the important role institutions play within Stonnington and the need to manage change and growth and include:

- *To accommodate, wherever possible, the future use and development needs of institutional uses in recognition of the positive and significant contribution they make to the City.*
- *To ensure that the future use and development needs of institutional uses take place in an orderly manner and are complementary to the context of the surroundings, especially any surrounding residential areas.*

- *To provide certainty and to reach consensus on appropriate future use and development for individual institutions and their surrounding community.*
- *To develop and maintain a cooperative relationship between the responsible authority, the community and the institution on matters relating to the interface between the institutional use and the surrounding area.*

Undoubtedly the upgrade of several teaching spaces, creation of new facilities such as a library and choral room, upgrade to administration accommodation, play spaces and sporting facilities across the Glendalough Campus will deliver substantial benefits to the student body with an enhanced educational experience. This has been the focus during the design phase since the beginning and will ensure that both existing and future students for the next decade are provided with quality learning and recreation spaces.

However the benefits associated with operational improvements and enhanced education facilities go beyond the Glendalough Campus community, particularly with a campus of this size and location. The proposal will also deliver a net community benefit to the surrounding residential precinct and will provide enhancements to the amenity of the neighbourhood. These benefits are described as follows:

- **Increased on site car parking:** The construction of a basement car park that will provide a net increase for the campus of 115 spaces and support the nearby Heyington Campus is a significant improvement compared to current conditions. Removal of cars associated with the Glendalough Campus from the surrounding residential streets and accommodating this demand within a basement car park that does not visually impact on the Campus or streetscape and can be appropriately managed will undoubtedly have a significant community benefit. This additional parking is also balanced with increased bicycle parking and an ongoing commitment to the promotion of public transport to actively encourage staff and students to use the fixed rail network, walking and cycling as more sustainable forms of transport to and from the school;
- **Landscaping treatments on Campus:** The portion of the Campus subject to redevelopment will deliver upgraded play spaces that incorporate canopy trees both existing and proposed, upgraded landscaping to the main frontages of the site to integrate with the established streetscapes and minimise the loss of any established canopy trees. Whilst it is accepted that some canopy trees are to be removed as a result of new built form or the basement car park, these losses have been minimised and must be balanced with the broader benefits of the proposed landscape works, including the new play spaces and a total of 31 new trees across the campus;
- **Built form:** The built form changes to the site are focused along the Lansell Road frontage and have sought to maintain a scale and overall form that is respectful of the streetscape, taking into account the presentation of the school at present and the surrounding residential dwellings. The architectural response for the new administration building in the centre of the Campus frontage is of a high quality design that reflects the form and detail of the adjacent Boyd Egan Hall and Victor McMahon Centre, which are the two most recent additions to the Glendalough Campus. As such it will make a positive addition to this section of the Lansell Road streetscape;
- **Sequencing of development:** The overall approach to the delivery of each stage of development across the Campus and the associated construction works has been determined based on both the impact on the students as well as the surrounding community. Whilst the construction phase of any project is temporary, the approach proposed does limit the impact of what is likely to be a significant phase of building works and will therefore also reduce impacts on the surrounding residential streets.

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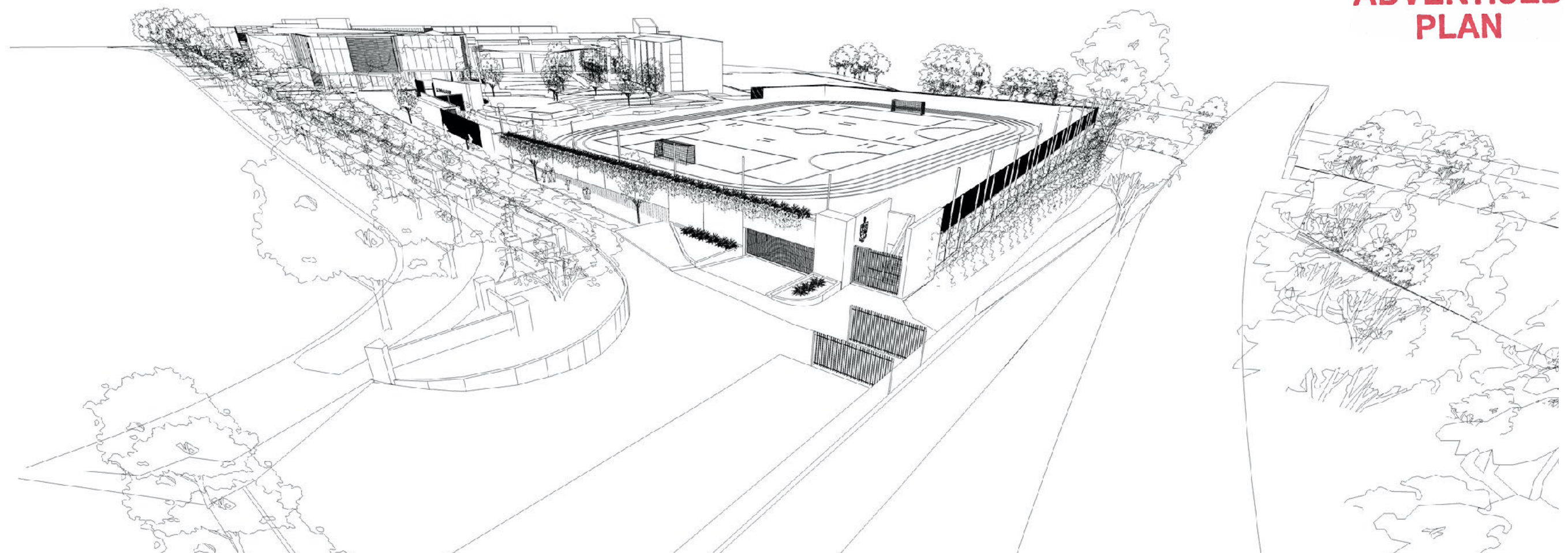
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- **Improvements to the designated drop off zone:** The upgraded drop off and pick up facility within the frontage of the site provides a logical approach to managing the junior school students without loss of on street parking and improving the flow of traffic through the site during the key times. The redesign of this facility will increase the capacity for vehicles to move through the site, collect students more efficiently and provide a safer environment for all. It will remain separate from the large basement car park proposed as part of the project to reduce any potential for congestion or conflict;
- **Minimising increases to student and staff numbers:** The project seeks only to increase student numbers by one year four class and one teaching staff member, which will be amply accommodated with the new teaching spaces and the provision of the on site car parking.
- **Protection of the Yarra River interface:** Furthermore the design of the new car park structure has been focused around ensuring the protection of significant landscaping that extends along the eastern and northern boundaries of the site. These edges of the Glendalough Campus interface with public pathways and the environmentally sensitive Yarra River corridor and therefore has been a key consideration in response to the state and local policies. The scale and form of the new car park structure, fencing detail and presentation of new play spaces have all been designed to minimise any visual impact when viewed from the northern banks of the Yarra River and ensure that additional landscaping be provided to boundaries to further contribute to the landscape character of the precinct.

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### 7.3 BUILT FORM RESPONSE

At the outset of the project, Chandler Architecture together with St Kevin's College identified a number of design objectives to guide the project, particularly in relation to the way built form will engage with the surrounding precinct. These objectives include:

- To continue the architectural language of the Victor McMahon Centre and Boyd Egan Hall which front onto Lansell Road. This approach for the new building will create a cohesive and contemporary presentation of the School buildings;
- Ensure all new built form is of a sympathetic overall height and setback from Lansell Road to that of the Victor McMahon Centre and Boyd Egan Hall so as to ensure it integrates with the streetscape;
- Siting of the car park structure in line with the topography of the land to minimise any visual impact where the structure rises above the current ground level to either Lansell Road or the Yarra River environs; and
- To ensure all new built form maximise the school's functional requirements and minimise any changes to boundaries or access arrangements.

The proposal has two key built form changes that will have an influence on the surrounding land. The first being the new administration and education building sited to front Lansell Road and the second being the new car park structure. Each of these elements and the key policy or built form controls are considered as follows:

### Administration / Education Building

The proposed new administration and education building has been sited and designed to specifically address the Lansell Road frontage and complete the contemporary facade of the school. Positioned to the immediate south of the existing 1970s administration wing, the new structure will be two storeys in scale and of a modern design, reflecting the architecture of the Boyd Egan Hall and the Victor McMahon Centre to the immediate west. It will accommodate both staff and administration facilities as well as the new class room spaces, library and literacy centre. It is a building that has been designed to both engage with the streetscape and provide a seamless connection internally with existing buildings on Campus.

Although the Glendalough Campus has been part of the residential streetscape of Lansell Road for almost 50 years, any new built form that interfaces with the street must appropriately respond to the scale, form, character and design of surrounding development. Such a response addresses the objectives and strategies relating to the built environment contained at Clause 21.06 and ensures successful integration with its surrounds.

In this regard the new building will manage the change to the streetscape by:

- Continuing the front setback that has been established by the Boyd Egan Hall and the Victor McMahon Centre to the immediate west and allow for new landscape spaces to be created;
- Maintaining a two storey building height consistent with that of other existing buildings on the Campus. Given the surrounding residential environment, this two storey scale is considered appropriate and ensures the density and form of the development is sympathetic;
- Incorporates a number of window openings across the facade at ground and first floor levels to ensure it is a building that engages with the streetscape, minimises any large expanses of blank walls and reflects the residential details of the streetscape;
- Use of high quality materials and finishes to reflect the architecture of surrounding development both on and off the Campus; and
- Create a clear sense of address with the primary entry to the school for pedestrians positioned away from sensitive residential neighbours to ensure activity and movement to and from the school does not disrupt the amenity of the street.

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### Car Park Structure and Response to Design and Development Overlay Schedule 3 (DDO3)

The portion of the project that will be sited adjacent to the northern and eastern boundaries of the site, and therefore in proximity to the Yarra River, is the proposed car park and new oval structure. The design has been focused on minimising the extent to which it projects above the current ground level and ensure any mature vegetation to boundaries is maintained. From a functional perspective, the car park access is to use the existing internal access ways and create a level surface for the new oval which is to be positioned on the roof of the car park.

DDO3, which affects all of the Glendalough Campus, is the primary planning control that is focused on ensuring the relationship between new built form and the Yarra River is appropriately managed. The siting and design objectives of this Schedule include:

- To ensure new buildings are appropriately set back from the banks of the Yarra River and adjacent public open space.
- To avoid additional light spill and overshadowing from buildings on the banks and water of the Yarra River, its adjacent public open space, bicycle and shared paths.
- To ensure building elevations are presented at a variety of heights, avoid visual bulk and are stepped back from the frontage of the Yarra River and adjacent public open space.
- To ensure all external colours and finishes are non-reflective and do not create contrast with the natural landscape character setting.
- To ensure public views of buildings are filtered through vegetation and trees.

The position of the new car park structure has therefore been designed to address the above objectives and comply with all relevant requirements, so as to ensure it remains sensitive to the Yarra River environs. The following response to the DDO3 requirements is to be read in conjunction with the plan set prepared by Chandler Architecture:

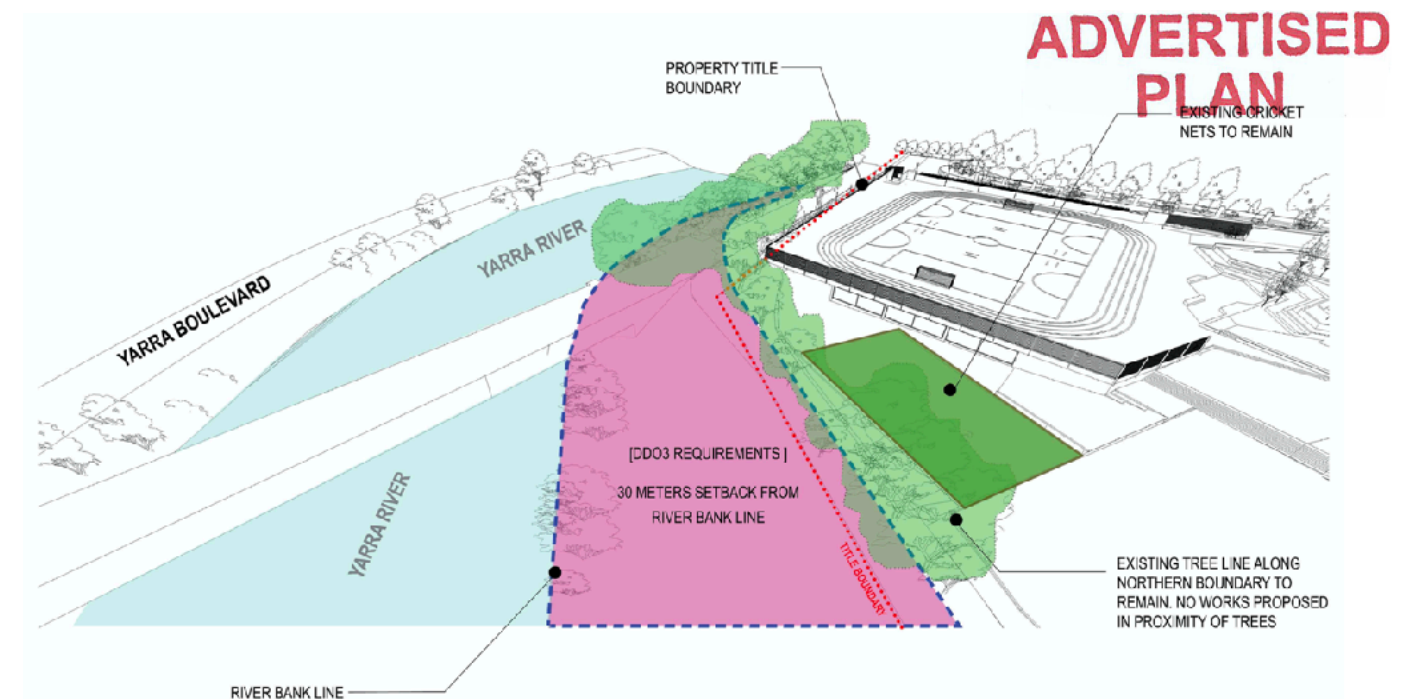
- The setback of the structure is fully behind the Mandatory Minimum Setback line for Area A – Grange Road to Gardiners Creek (east). This setback is detailed on Drawing TP03 rev C of the set of plans prepared by Chandler Architecture and identifies the setback reference line (River edge), the title boundaries and the mandatory minimum setback line. The plan demonstrates that all elements of the car park structure will be within the mandatory minimum setback line and therefore complies with the requirement;
- Siting of the car park structure such that only the north eastern corner and portions of the eastern boundary are to be visible from the pedestrian and cycle path located on the northern bank of the Yarra River – some 110 metres in away from the Campus. As detailed on Drawing TP22A and TP22B rev C of the set of plans, these view lines are currently dominated by dense vegetation along both the northern and southern sides of the River and due to the siting of the proposed structure and the height above the current ground level, will ensure it is not visually dominating or overwhelm this environment;
- The overall height of the structure above the current ground level is below the 9 metres as specified in DDO3;
- All new fencing proposed along the edge of the car park structure will not be within the mandatory minimum setback line and has been designed to be transparent (75% open) and/ or in the form of retractable netting. This approach will ensure any flood waters across the site are not impeded but also

avoids any impact on views from the northern bank view lines (see Drawing TP24 rev D for detailed information on the placement and design of all fencing);

- No overshadowing from the car park structure will occur to any land outside of the title boundary of the Glendalough Campus (see Drawings TP23A rev C and TP23B rev C for the overshadow diagrams prepared for the nominated hours on 22 June and 22 September); and
- All materials and finishes to the new car park structure are non reflective and together with the landscaping to boundaries is designed to blend with the natural landscape character of the area (see Drawing TP21 rev C for detailed information on the materials and finishes for all proposed structures and fences).

It is acknowledged that the proposed site coverage of all buildings is to be 45% and therefore is higher than what DDO3 seeks to encourage. In this instance, it is considered that the site coverage is appropriate having had regard to the educational function of the land and the fact that a substantial proportion of the existing Glendalough Campus buildings or structures are not being altered or developed. Notwithstanding this, the water sensitive urban design plan prepared by ARK Resources submitted with the application details how the proposal addresses stormwater as a result of the site coverage.

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## 7.4 RIVER INTERFACE AND LANDSCAPE DESIGN

The proposed landscape treatment across the Glendalough Campus, particularly to its boundaries, must meet a multitude of policy objectives and practical requirements which are, to a significant extent, a result of the varied boundary interfaces that the Glendalough Campus has at present.

From a planning policy perspective, the landscape contribution that is expected for every new development in Stonnington is addressed at Clause 21.06-2 and encourages planting that is appropriate to the character of the area, provides canopy trees to street frontages as well as side setbacks and retains existing trees and vegetation where possible. Overall it is the desire that any new development reinforce the high quality landscape character of the City.

The Glendalough Campus must also address the policy objectives relating to the Yarra River environs together with the requirements of DDO3 given its northern and eastern boundaries interfacing with the River. Key objectives in this regard include:

- To protect and enhance the natural landscape character of the Yarra River corridor where the waterway, its topography, adjacent public open space and a continuous corridor of vegetation and canopy trees are the dominant features.
- To minimise the visual intrusion of development when viewed from the Yarra River and adjacent public open space, bicycle and shared paths and bridge crossings.
- To ensure development on visible hill slopes, crests, skylines and ridge lines is subordinate to existing local vegetation and natural landscape character setting.
- To ensure sufficient space is provided between buildings to maintain views to the Yarra River and allow for the planting and growth of vegetation, including large canopy trees.

And finally it must also be remembered that landscaping within a school has a different purpose than that of a residential development. It must balance the needs of sporting activities which are formal and informal, car parking and access arrangements, built form and in the case of a primary school, a variety of play spaces for learning and recreation. Landscaping within a school has to be multi purposed and meet the needs of a variety of users on a daily basis and must be achieved within constrained spaces and ensure integration with the surrounding residential area.

The landscape design prepared by Tract Consultants together with the layout of new built form by Chandler Architecture has sought to balance all of these competing landscape needs and design objectives to achieve a response that will deliver a net increase in new canopy tree planting and protect significant existing landscaping.

The following provides a summary of the northern, eastern and southern interfaces where the proposed works will be seen. Note: the western boundary of the Campus will remain unaltered as a result of this application.





## Southern Boundary - Lansell Road Streetscape

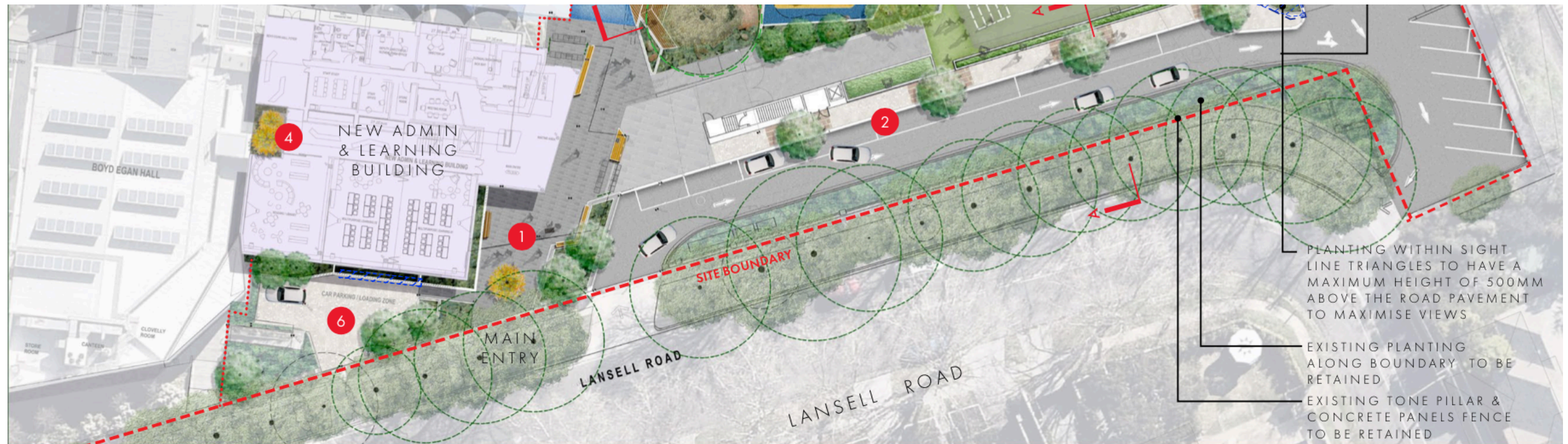
The section of the Lansell Road streetscape which includes the Glendalough Campus is currently influenced by a row of large English Elms planted within the road reserve, a relatively dense hedge to the school frontage and various small trees and garden beds sited against existing built form and adjacent to pathways and parking areas.

Although the proposal is seeking to insert a new building within the streetscape, these three landscape elements will continue to define and contribute to the landscape character of Lansell Road. The position all new built form, the improved drop off and pick up zone and new pedestrian spaces have all been designed to ensure:

- No impact on the existing street trees to ensure they continue to dominate the streetscape. The retention of existing crossovers to the site, retention of existing front fencing and design of paved spaces and garden beds adjacent to the site frontages has been focused on ensuring there is no impact to this important public asset;
- Minimal change to the established Photina robusta hedge which extends along the frontage of the Campus adjacent to the drop off zone. This hedge will therefore continue to screen the internal access ways and maintain the vegetated presentation that the Campus exhibits to Lansell Road;
- Existing front fencing is to be retained so as to ensure it remains a consistent part of the streetscape;
- Landscaping to cascade over the edge of the car park / oval structure to soften any views of this element from Lansell Road; and
- New canopy tree planting within the frontage and extended garden beds in front of the new building to contribute to the streetscape and provide a more cohesive landscape treatment across the whole of the Campus frontage, integrating with landscaping in front of the Boyd Egan and Victor McMahon buildings.



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The Glendalough Campus has a unique interface with the Yarra River. Although appearing on plan to be relatively close to this sensitive landscape environment, the topography of the river bank together with the existing dense vegetation within the public open space areas and to the Glendalough Campus boundaries results in very limited viewing opportunities of the school.

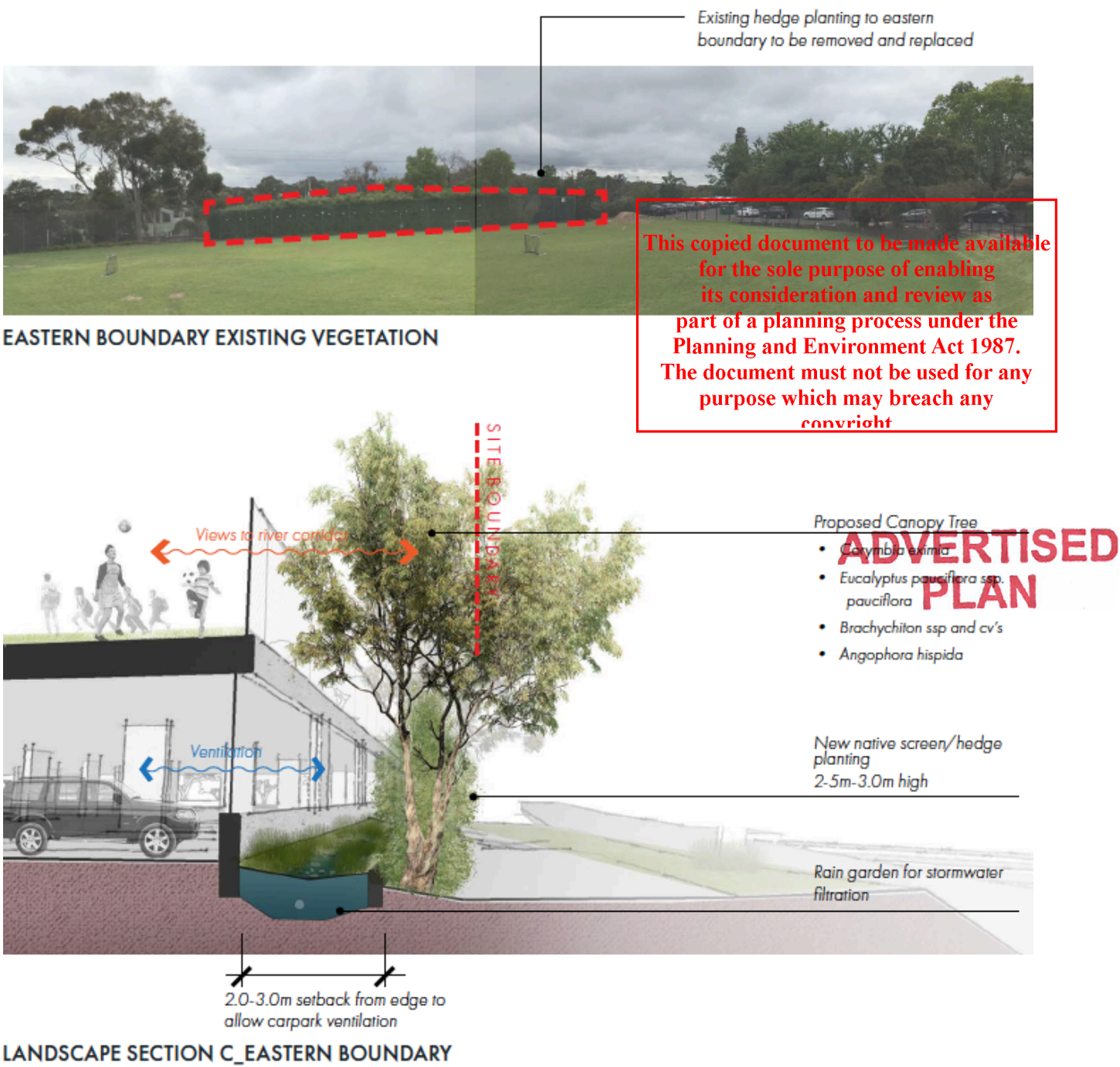
As discussed in the Built Form Response section of this report and outlined in the plans prepared by Chandler Architecture, the proposal has been carefully designed to minimise any visual impact on the Yarra River - accepting that the primary view lines are from the northern bank and are a considerable distance from the Glendalough Campus. The position and treatment of the proposed car park structure seeks to ensure that the natural landscape setting from the public viewing points remains the dominant element.

It is important to highlight that the southern bank of the Yarra River, immediately in front of the Glendalough Campus, does not allow for any public pathway for pedestrians or cyclists. As outlined earlier in this report, this section of the River bank is managed by Melbourne Water and closed off to the public. The elevated rail line also extends along the eastern section and therefore provides a physical barrier to the River bank. Furthermore, the waters edge is approximately 4.0 metres below the northern and eastern title boundary of the Campus, eliminating any public views of the proposed development from this southern River bank to address.

In response to the landscape objectives of DDO3 and addressing the viewing points of the Glendalough Campus from the northern river bank, the proposal will:

- Retain all mature eucalypts along the north eastern corner of the Glendalough Campus to ensure they continue to contribute to the landscape character of the river interface. These trees (identified on plan and in the Arbor Safe report) have been addressed both in the position of the new structure and any fencing around the perimeter to ensure their retention and ongoing growing conditions is not compromised;
- Plant new screening vegetation (canopy trees and screening shrubs) of native species to replace the pittosporum hedge along the eastern boundary of the site. This screen planting is proposed to reach a mature height of 2.5 - 3.0 metres in line with the height of the proposed structure together with four new canopy trees, creating a dense vegetation screen to address views of the campus from the northern banks of the river. It is proposed that a minimum of 50% of this new planting at the eastern will be indigenous to the area together with plants suitable for the filtration of storm water within the rain garden.

The above will ensure that public views of the proposed structure will remain filtered or completely screened by vegetation and mature trees as shown in the view line analysis provided at TP22A rev C of the plans. This outcome achieves the objectives of DDO3 and ensure that all vegetation within the Glendalough Campus boundary can continue to grow and benefit the broader River environs.





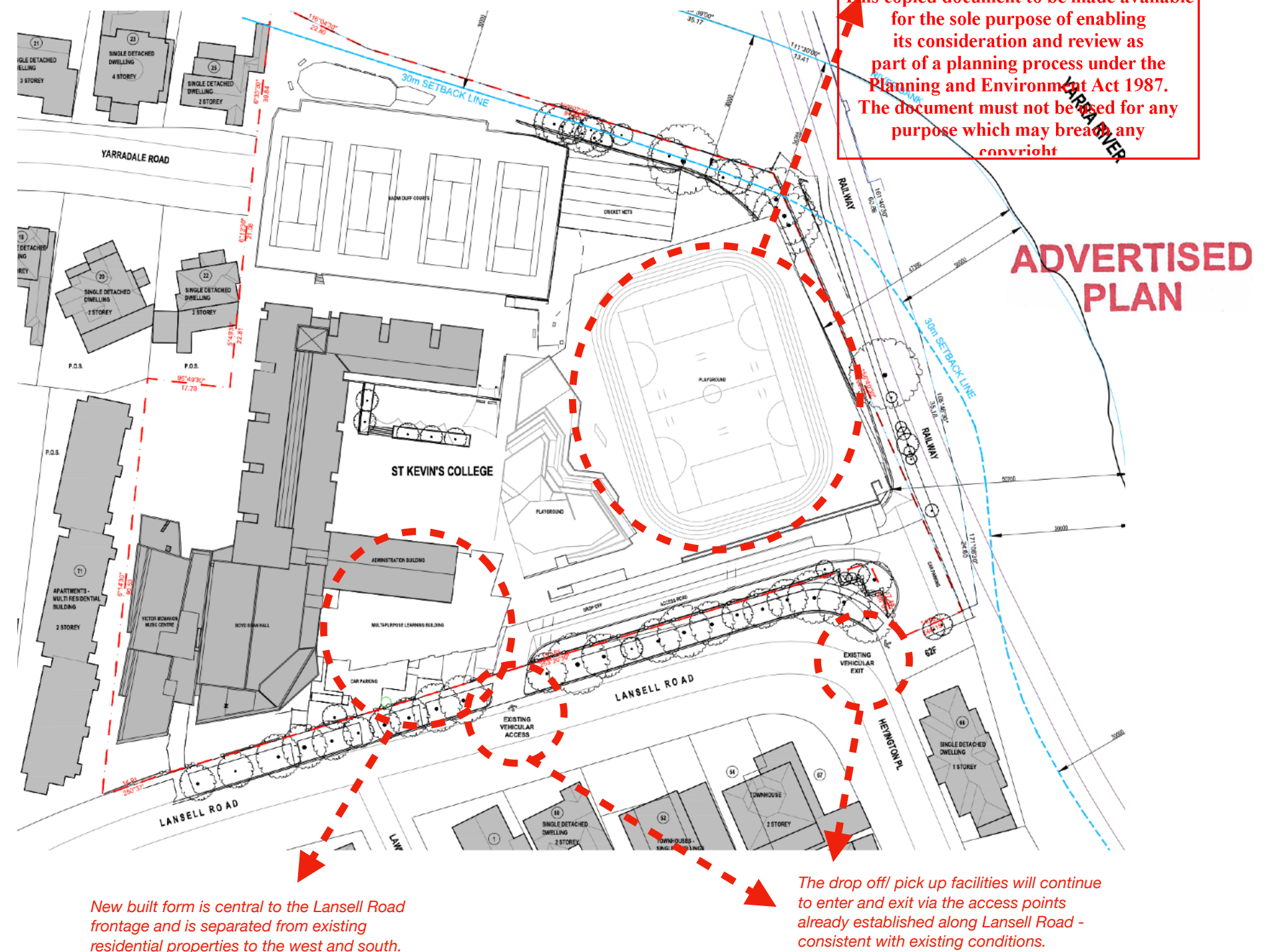
## 7.5 AMENITY IMPACTS

Potential amenity concerns associated with a school (primary or secondary) which is located within a broadly residential area often fall into three broad categories: noise associated with the general school operation (particularly outside of standard school hours), the intensity of the use and traffic/car parking. Typically, these issues are of particular relevance to the surrounding community when a proposal to expand or redevelop a school has a direct abuttal to an adjacent residential property. As previously outlined, the local policy at Clause 22.16 – Institutional Uses provides guidance as to the consideration of how education facilities should address the surrounding residential communities.

Whilst the proposed works overall are substantial and will result in new built form to the site, the project seeks to minimise any potential for amenity impacts to the surrounding residential area through the following:

- The new built form, together with any alterations to the existing buildings, is located centrally on the campus with no direct abuttal to residential land that would alter existing conditions. This will ensure that immediate neighbours will not experience any change to their outlook from private open space or habitable rooms. Concerns relating to overshadowing, overlooking or visual impact from new built form are therefore not relevant to the consideration of the application;
- The student and staff numbers will increase only by one year four class and one teacher and therefore will not represent any significant intensification of the land use. In essence the project is more focused on consolidating the junior school operations, improving facilities for students and providing more comprehensive parking on site rather than expanding or intensifying the school activity;
- The project will provide a significant increase to the on site car parking supply (net increase of 115 spaces) to address staff needs together with an improved drop off and pick up arrangement for parents within the frontage of the site. The provision of a parking supply of this nature is expected to materially improve the current conditions and reduce the use of on street parking, particularly to the residential streets; and
- Screen planting along Lansell Road is to be retained together with the planting of new canopy trees and other vegetation, maintaining the vegetation screen to the street and enhancing the contribution that the school makes to the landscape character of the precinct.

Given the above points the amenity currently enjoyed by the adjacent properties, as well as the many neighbours that are located opposite the campus along Lansell Road, will not be diminished. Through the siting of the built form on the Campus, the approach to substantially upgrading the car parking provided on site and the landscape treatments to the boundaries, the proposal is looking to significantly improve the amenity of the local community.





7.6 ACCESS, CAR PARKING AND TRAFFIC MANAGEMENT

A Transport Impact Assessment has been prepared by GTA Consultants and is included with the application documentation. This assessment deals with access, car parking and traffic management. A summary of this report is provided for ease of reference.

Existing Parking Conditions

The Glendalough Campus currently provides a total of 35 car parking spaces within the curtilage of the site. These spaces are located in the western car parking area in front of the existing administration building and the parallel spaces on the southern side of the existing pick up/drop off area and to the south eastern corner.

As a result of this limited parking there is a reliance on publicly available on street car parking within the vicinity of the site to accommodate the ‘overspill’ car parking demand that is generated by staff and visitors.

Public Transport

The Campus has excellent access to public transport noting its proximity to Heyington Railway Station which is serviced by the Glen Waverley rail line, approximately 200 metres south east. Further, there is a clear segregated pedestrian footpath from the railway station to the school providing safe and convenient access

In August 2019, the students were surveyed (499 students present on the day) to ascertain the proportion using public transport. The survey showed:

- 56% travelled by private vehicles
- 42% travelled by public transport
- 2% walked or cycled

As highlighted in the Transport Impact Assessment, these results confirm a very high proportion of students use the Heyington Station for travel to and from the school given the age of the students – these movements are in fact specifically encouraged and facilitated by the school.

In terms of existing staff (67 in total which include 47 full time and 20 part time) travel modes, the surveys illustrate the following:

- 76% travelled by private vehicle
- 16% travelled by public transport
- 8% walked or cycled

Proposed Parking Management

The proposed redevelopment will incorporate additional on site car parking and involve the relocation of some of the existing car parking spaces. A total of 150 on site car spaces will be provided on site – which will represent an increase by 115 spaces compared to extant conditions. The car spaces will be allocated as follows:

- Accommodate 133 spaces within the basement including 4 DDA spaces
- Accommodate 16 visitor spaces to the east and pick up/ drop off spaces
- Retain existing 1 DDA parking space at the entrance to the building off Lansell Road

On Site Parking

There will be an increase in the number of student numbers from 521 to 547 and the addition of 1 full time staff member.

Clause 52.06 of the Stonnington Planning Scheme requires that new or extended schools provide parking rates of:

- 1.0 space per primary school employee that is part of the maximum number of employees on the site at any time.
- 1.2 spaces per secondary school employee that is part of the maximum number of employees on the site at any time.

An additional staff member is required for the additional Year 4 class proposed. As such, the application generates a statutory requirement of one additional car space. Given a total of 150 on site car parking spaces will be provided across the campus (compared to the current 35 car spaces), the minimum statutory requirement is well exceeded.

On this basis, the proposal is expected to greatly improve on street car parking conditions to Lansell Road and Heyington Place given that car parking demands will be fully accommodated on site.

Pick Up/ Drop Off

The proposed development will result in significant improvements to the pick up/ drop off arrangements as follows:

- The removal of the parking lane to the south of the zone will serve to facilitate a through traffic lane. This will allow through traffic to travel through this area to access the basement car park and as such, reduce the potential for queuing on Lansell Road. This will allow vehicles who have picked up/ drop off to exit and not be held up in the queue.
- Creation of a separated footpath along side the drop off and pick up zone so as to ensure students can access vehicles safely and minimise the collection timing.
- The proposal will involve the lengthening of each of the parallel car parking spaces so that vehicles can enter or exit independently despite other vehicles at adjoining bays. This design approach is expected to assist with the reduction in queuing on Lansell Road. These car spaces will also allow for convenient short term parking during the day for visitors of the school and subsequently, the removal of on street parking reliance in the vicinity of the school.
- The restriction on parking at the eastern end of the drop off and pick up area to short term visitors only with the area cleared at 2.30pm each day such that it parents can utilise this extra parking area (not currently available).
- The attendance of a traffic warden each day between 2.30pm and 3.30pm to manage the drop off and pick up area as well as the movement of vehicles back onto Lansell Road and take any necessary action with parents or guardians that may park inappropriately or cause unnecessary delays.

The expected impact of the above management and design changes to the existing drop off and pick up area are outlined in the GTA Consultants traffic impact report, including the expected reduction to the queuing of traffic within Lansell Road.

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## 7.7 ENVIRONMENTALLY SUSTAINABLE DEVELOPMENT AND STORM WATER MANAGEMENT

The importance of achieving best practice standards in environmental sustainable design is supported through a breath of planning policies contained in the Stonnington Planning Scheme. Clauses 22.05, 22.18 and 53.18 together aim to encourage sustainable outcomes which also include managing stormwater runoff and encouraging re-use of rainwater and on site infiltration. In the case of the Glendalough Campus, regard must be given to the environmental sensitive of the site, noting that it is adjacent to the Yarra River and its environs and the need to appropriately filter stormwater run off from the site and contain all run off within the drainage system.

Ark Resources were engaged to provide advice with regard to sustainable design initiatives to be adopted and integrated as part of the development and guide the appropriate treatment of stormwater. The application documentation prepared includes two detailed reports by Ark Resources which address the assessment and compliance with policy requirements and appropriate rating tools. The following key sustainable design initiatives have been incorporated as part of this project to achieve the 4 star Green Star rating that is a commitment of the project:

- Rainwater harvesting from the new roof areas (and refurbished building) to service rainwater tanks which will total 24,000 litres. This water will be used for flushing of toilets within the new built form and for irrigation to across the Campus landscaped areas. This will reduce the reliance on mains water by an average of 152 kL per year and provide an average supply reliability of 96% for toilet flushing and 98% for irrigation.
- Creation of rain gardens to key locations on the Glendalough Campus that will have capacity to filter stormwater from various impermeable surfaces and away from play spaces. These rain gardens will exceed the pollutant load reduction targets that are required by the local policy at Clause 22.18 - Storm Water Management and Clause 53.18.
- High performance glazing and energy efficient building services, appliance and fixtures.
- Installation of 20kW of rooftop solar photovoltaic panels.
- Installation of electric vehicle charging points.
- Provision of 16 new bicycle parking spaces to support sustainable modes of travel. This additional parking will be provided within the basement car park and be secure for both students and teachers.

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The proposed development of the Glendalough Campus of St Kevin's College is a project that is seeking to accommodate a range of education, recreation and functional needs to allow the school to meet the requirements and expectation of modern learning environments. It is however, a project that is mindful of its position within the broader community and as such, the development is also sensitive to the residential interface and the adjacent Yarra River environs.

The strategic vision of the proposed development is therefore focused on delivering:

1. **A contemporary learning environment that meets the needs of the current and future students with the addition of a new administration and education building to Lansell Road;**
2. **Improving upon the existing drop off and pick up facility to ensure maximum efficiency and safety for all students and providing for all car parking needs on the Campus for staff, visitors and parents;**
3. **Preserving significant landscaping across the site and further enhancing the landscape character of the precinct;**
4. **Recreation spaces for students that will enhance both their learning environment and promote health and wellbeing through active play.**

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The Stonnington Planning Scheme provides strong policy support at both the State and local levels for the ongoing development of community infrastructure in this manner, including educational facilities and in this regard it is submitted that the proposal addresses all relevant policy objectives relating to the design and operation of non residential uses in a residential area to achieve net community benefit. It is respectful of the existing amenity enjoyed by surrounding properties, responds to the streetscape of Lansell Road through the contemporary built form proposed and addresses the more sensitive River interface with appropriate siting and design of new structures.