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Traffic Impact Assessment

Caulfield Grammar School - Wheelers Hill Campus

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Prepared for Caulfield Grammar School

5 December 2025

244268

Revision Register

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1 INTRODUCTION

1.1 Background

TTW has been engaged by Caulfield Grammar School to prepare a Traffic Impact Assessment (TIA) to support the town planning application for the proposed senior school building located at 74 – 82 Jells Road, Wheelers Hill.

The purpose of this TIA is to:

- Provide an overview of the proposed development
- Evaluate any traffic and transport implications as a result of the proposed development
- Confirm that the statutory parking requirement does not increase, as there are no changes in student and staff numbers
- Determine whether the proposal will result in any adverse impacts on the surrounding road network

1.2 Reference Documentation

In preparing this assessment, review of the following reference documents was undertaken:

- Clause 52.06 and 52.34 of the Monash Planning Scheme
- Australian Standards, including:
 - AS2890.2:2018 – Parking Facilities, Part 2: Off-street commercial vehicle facilities

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2 THE PROPOSAL

2.1 Project Overview

This application proposes to construct a new building and ancillary changes to the school campus, as summarised in Figure 2.1 and Figure 2.2:

- Demolition of three (3) existing multi-purpose courts to facilitate a new three-storey senior school teaching and learning building.
- Replacement of the existing tennis hardcourts with new multi-purpose sports courts located to the southeast of the proposed new building.
- Conversion of the existing dedicated turnaround area at the end of the internal roadway into a service-only access road. This change will remove 19 ninety-degree car parking spaces and 5 pick-up/drop-off spaces to accommodate the expanded multi-purpose sports courts.

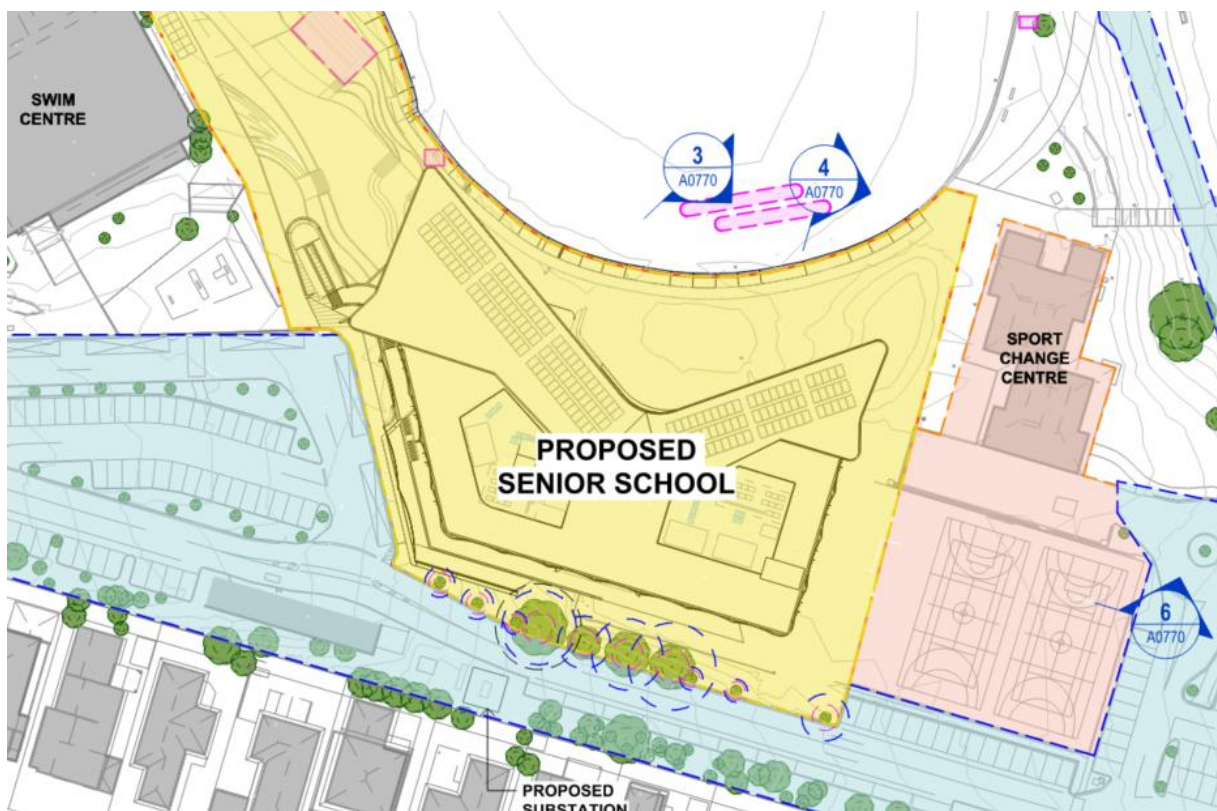


Figure 2.1: Proposed Senior School Building

Source: DesignInc

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Figure 2.2: Proposed Sports Court Expansion and Associated Works

Source: DesignInc

2.2 Operational Characteristics

The proposed development is intended to reinforce the existing students and staff within upgraded facilities, rather than expand the school's enrollment or staffing levels. Accordingly, the school will continue to operate with the same student population and staff cohort as outlined in Table 3.4, with no changes proposed to daily operational characteristics, hours of operation, or overall activity on the campus.

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2.3 Parking Provisions

2.3.1 Car Parking

Table 2.1 summarises the existing and proposed car parking supply within the campus.

Table 2.1: Car Parking Summary

TYPE	EXISTING	PROPOSED	NET CHANGE
ALL-DAY PARKING			
Staff Only Parking	131	131	0
General Parking	204	185	-19
Visitors Parking	7	7	0
SHORT-TERM PARKING			
Pick-up & Drop-off Spaces	120	115	-5
TOTAL	462	438	-24

As mentioned, the proposal will result in a net reduction of 19 general parking spaces, and 5 pick-up and drop-off spaces, primarily due to the reconfiguration of the internal road layout. Overall, the school will deliver a total of 438 spaces post-development.

Section 3.4 illustrates the distribution of parking within the campus, and Section 4 assesses the adequacy of the proposed provision.

2.3.2 Bicycle Parking

The proposal does not include any additions to bicycle parking, bicycle parking facilities are as per the existing conditions detailed in Section 3.3.2.

2.3.3 Bus Parking

There are currently eight (8) bus parking spaces on-site, located to the west of the proposed building. No additional bus parking has been proposed as part of the subject proposal.

2.4 Access Arrangements

The proposal does not include any changes to the existing access arrangements. Vehicles will continue to access the campus as per the existing arrangements as documented in Section 3.1.

2.5 Loading and Waste Collection

No changes are proposed to the loading and waste collection activities on campus.

All loading activities will continue to occur within the on-campus car parks where possible. Loading activities are to be conducted during off-peak hours where feasible to avoid any potential conflicts with pick-up and drop-off traffic, as per the existing arrangement.

Waste collection will continue to be undertaken at the staff parking cul-de-sac along the northern boundary of the site. For details of waste collection, refer to the Waste Management Plan which has been prepared separately as part of the planning application.

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3 EXISTING CONDITIONS

3.1 Site Context and Surrounding Area

The subject site is located at 74 – 82 Jells Road, Wheelers Hill. The campus is bordered by Jells Road to the west, and Scotchmans Creek Trail to the north. The campus footprint is approximately 18 hectares.

The sole vehicle access to the campus is located at Jells Road, as shown in Figure 3.1. The site is zoned as Neighbourhood Residential Zone – Schedule 4 (NRZ4) under the Monash Planning Scheme.

The surrounding land uses are primarily residential to the south, and vegetation (Public Park and Recreation Zone) to the north and north-east.

Figure 3.1 illustrates the location of the subject site within its broader context.

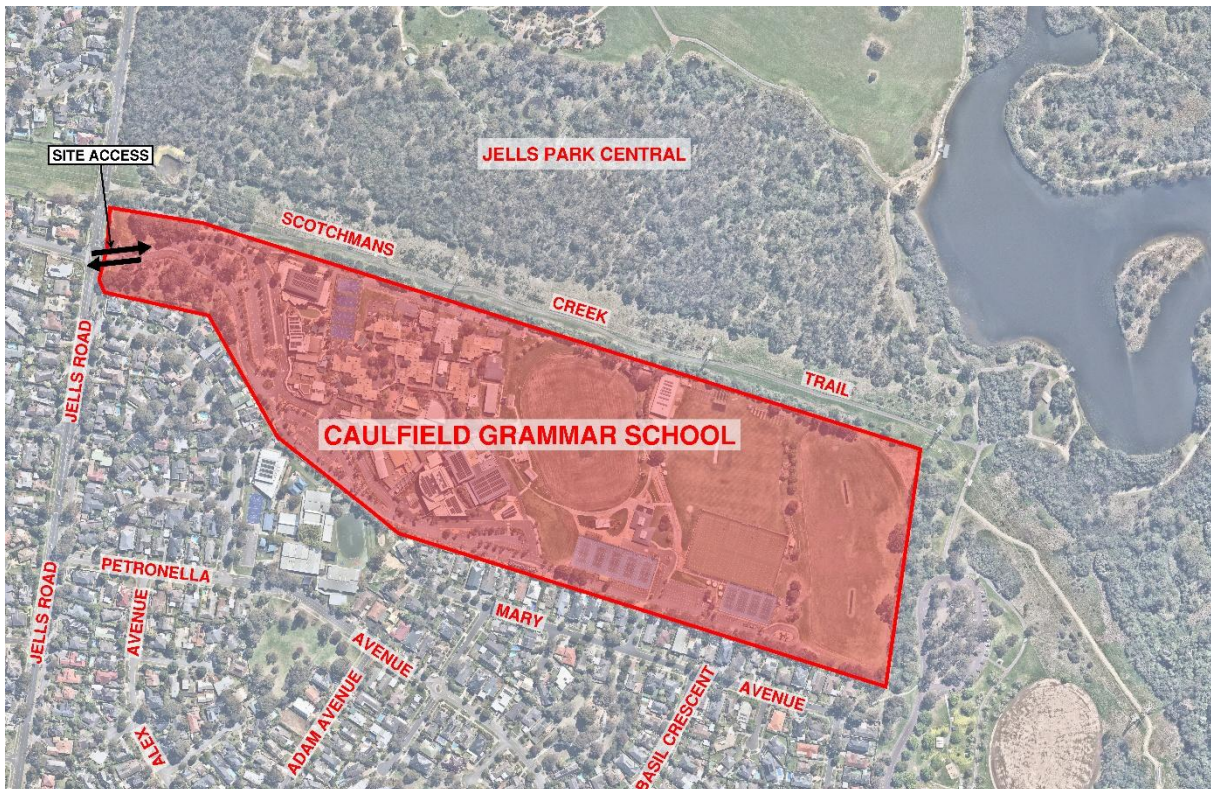


Figure 3.1: Campus Overview Plan

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3.2 Road Network

3.2.1 Jells Road

Jells Road is a north-south arterial road under the management of the Department of Transport and Planning (DTP).

In the vicinity of the site, Jells Road typically comprises:

- A single travel lane in each direction.
- Painted median
- Indented kerbside parking in the northbound direction.

The carriageway is approximately 12.5 metres wide. The indented parking lane accommodates unrestricted parking, with designated bus zones and signposted sections subject to “No Stopping” restrictions.

The posted speed limit along the road is 60 km/h, except during signposted school hours when a reduced speed limit of 40 km/h applies.

A northbound view of Jells Road adjacent to the campus access is provided in Figure 3.2.



Figure 3.2: Northbound View of Jells Road

Source: Google Maps

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3.2.2 Ferntree Gully Road

Ferntree Gully Road is an east-west suburban arterial road managed by DTP, the road forms part of the Metropolitan Route 22 linking inner south Melbourne with the eastern suburbs.

Ferntree Gully Road is a road providing major connection between Jells Road (to which the sole campus access is located at) and other parts of Melbourne.

In the vicinity of the subject site, Ferntree Gully Road features:

- Three traffic lanes in each direction, and
- An intermittent on-road bicycle lane in each direction.

The carriageway is approximately 25.5 metres wide. The road has a posted speed limit of 80 km/h, with kerbside parking prohibited along the road.

A representative view of Ferntree Gully Road adjacent to the Ferntree Gully Road – Jells Road intersection is shown in Figure 3.3.



Figure 3.3: Eastbound View of Ferntree Gully Road

Source: Google Maps

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3.3 Sustainable Transport

3.3.1 Walking

Pedestrian accessibility is a key consideration in evaluating the sustainability and connectivity of any development. To provide an indication of walkability, reference has been made to Walk Score – a recognised online tool that assesses how convenient it is to complete daily tasks on foot.

Walk Score calculates a score out of 100 based on the walking distance to nearby amenities such as shops, parks, schools, and public transport. Amenities within a 5-minute walk (approximately 400 metres) receive the highest points, with a decay function applied to those farther away. The tool also takes into account pedestrian-friendly factors such as population density, intersection frequency, and block length.

According to Walk Score, the subject site has a score of 73 out of 100, which falls under the “very walkable” category. This suggests that the walkability of the site is relatively high, and most errands can be accomplished on foot. Hence, it is expected that students living nearby may travel to school on foot.

The approximate 15-minute walking catchment from the site is shown in Figure 3.4.

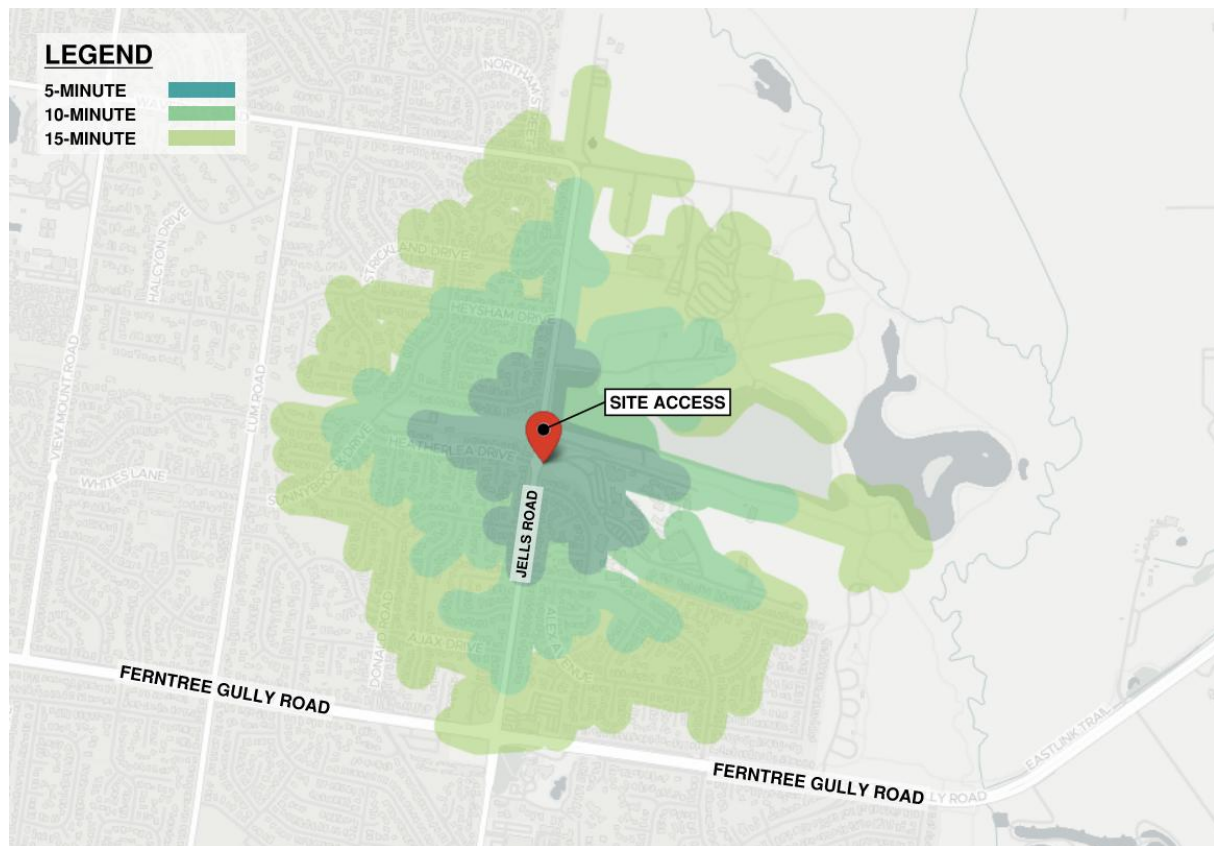


Figure 3.4: 15-Minute Walking Catchment

Source: Targomo

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3.3.2 Cycling

Cycling is a low-impact, sustainable transport mode that produces zero emissions, minimal noise, and contributes positively to public health and wellbeing. It also serves as a viable alternative to car travel for many trips.

The following roads in the vicinity of the campus allow for bicycle use:

- Intermittent / Informal bicycle lanes along Ferntree Gully Road
- Shared user paths along Scotchmans Creek Trail

These facilities provide active transport accessibility for students, staff, and visitors who may choose to cycle. The area reachable by bicycle within a 15-minute bicycle catchment is shown in Figure 3.5.



Figure 3.5: 15-Minute Bicycle Catchment

Source: Targomo

The campus currently provides a total of four (4) bicycle parking spaces within the bicycle storage area located at the rear of the function room. In addition, it is understood that the fencing in the vicinity of the Ullin Centre may also be used by students to secure their bicycles.

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3.3.3 Public Transport

Bus

The subject site is currently served by Route 754, which connects Stud Park Shopping Centre to Glen Waverley Station, linking Melbourne’s middle-ring eastern suburbs with the train network, as shown in Figure 3.7.

The closest bus stops in the vicinity of the site are located within a 5-minute walk, as detailed below.

Table 3.1: Bus Service Details

BUS ROUTE	ROUTE DESCRIPTION	DIRECTION	NEAREST STOP	APPROX. TRAVEL TIME & DISTANCE (WALKING)
754	Glen Waverley via Caulfield Grammar & Wheelers Hill	To Rowville	Homestead Dr / Jells Rd	6 mins, 400 m
		To Glen Waverley	Heatherlea Dr / Jells Rd	5 mins, 270 m

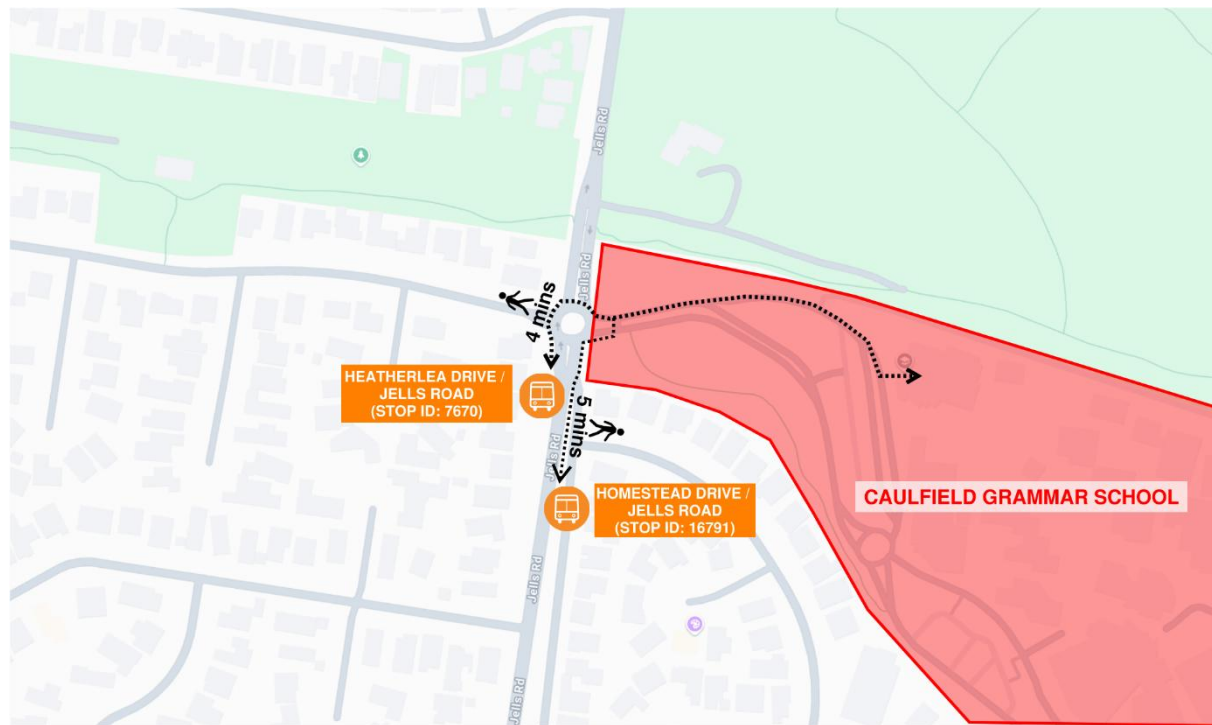


Figure 3.6: Public Transport Network Map

Source: Google Maps

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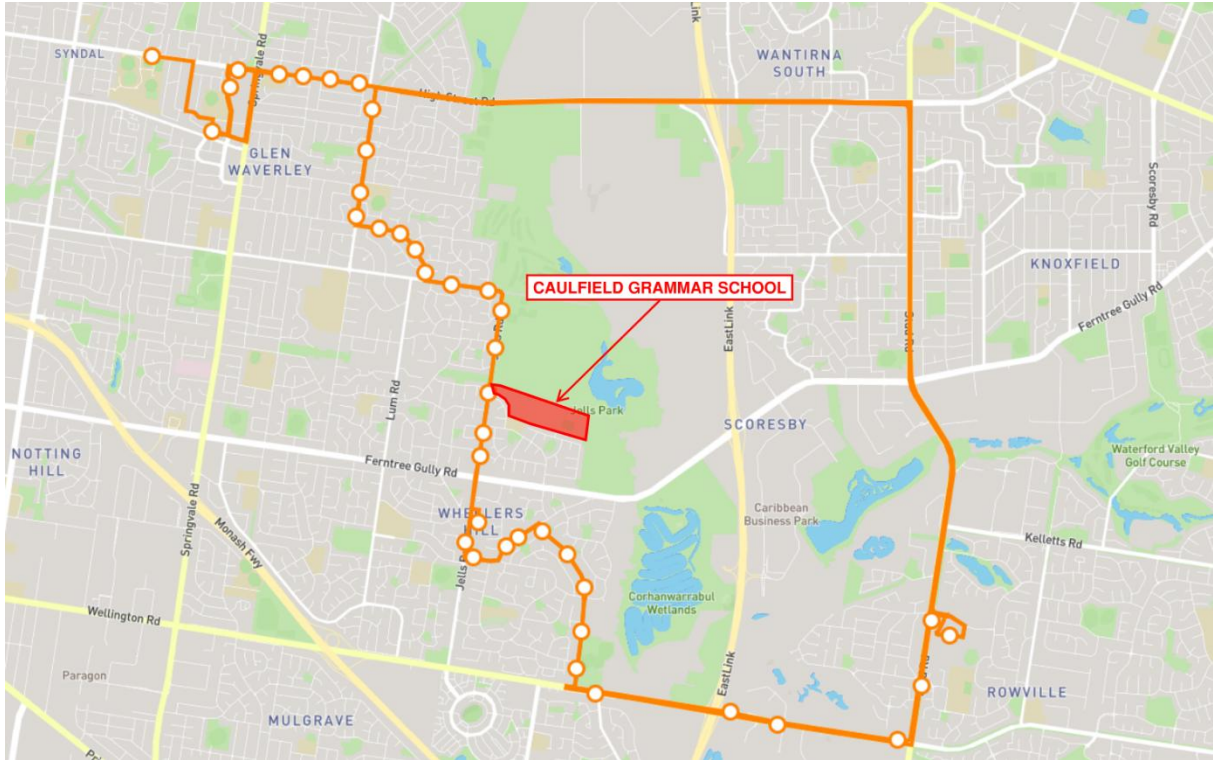


Figure 3.7: Bus Route 754 Servicing Area

Source: Transport Victoria

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Train

There are no train services available in the vicinity of the campus. However, trains could be utilised as part of a multi-modal journey as bus route 754 provides connection to the Glen Waverley train station.

Table 3.2: Train Service Details

STATION / LINE	CONNECTION	APPROXIMATE FREQUENCY
Glen Waverley / Glen Waverley Line	Bus 754	Approximately every 15 minutes

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3.4 Car Parking

Several parking areas, comprising a total of 462 car spaces and eight (8) bus spaces, are provided across the campus for use by staff, parents and school buses.

Figure 3.8 and Table 3.3 provide a breakdown of the parking currently available throughout the campus. As noted in Section 2.3.1, 19 general parking spaces, and five (5) pick-up and drop-off spaces (i.e., Area 4b) will be removed as a result of the subject proposal.

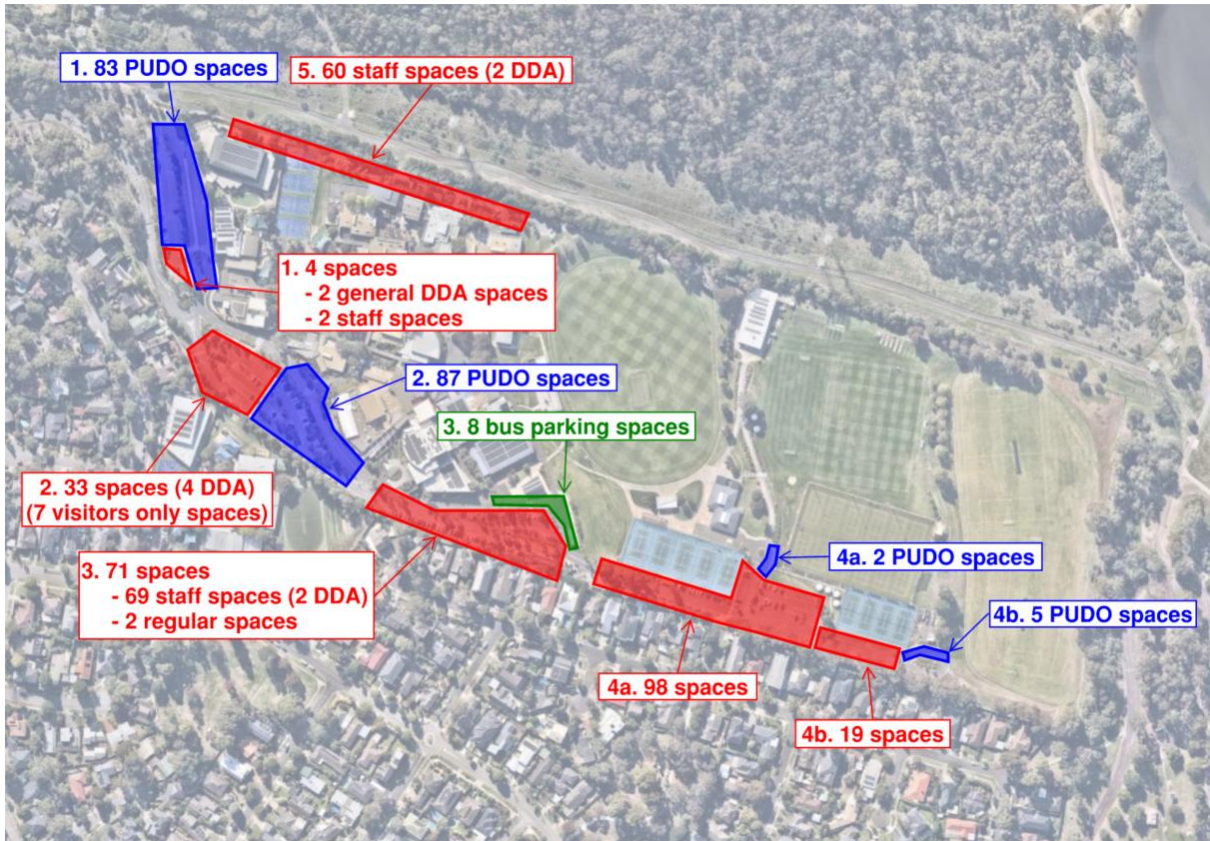


Figure 3.8: On-campus Parking Supply

Source: Nearmap

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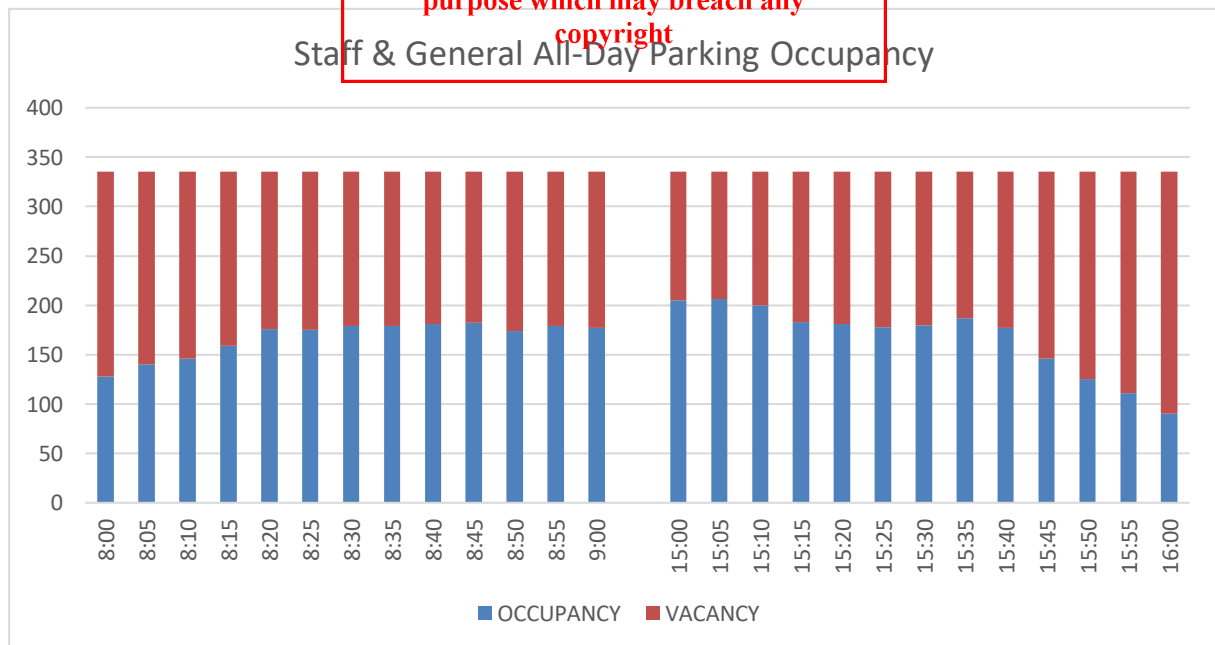
Table 3.3: On-campus Parking Supply Breakdown

AREA	ALL-DAY PARKING (STAFF ONLY)	ALL-DAY PARKING (GENERAL)	ALL-DAY PARKING (VISITORS ONLY)	PICK-UP & DROP-OFF (PUDO)	BUS
1	2 spaces	59 accessible spaces	-	26 spaces	-
2	-	26 spaces (inc. 4 accessible)	7 spaces	87 spaces	-
3	69 spaces (inc. 2 accessible)	2 spaces	-	-	8 spaces
4a	-	98 spaces	-	2 spaces	-
4b	-	19 spaces	-	5 spaces	-
5	60 spaces (inc. 2 accessible)	-	-	-	-
TOTAL	CARS				BUS
	131 spaces	204 spaces	7 spaces	120 spaces	8 spaces
	462 spaces				

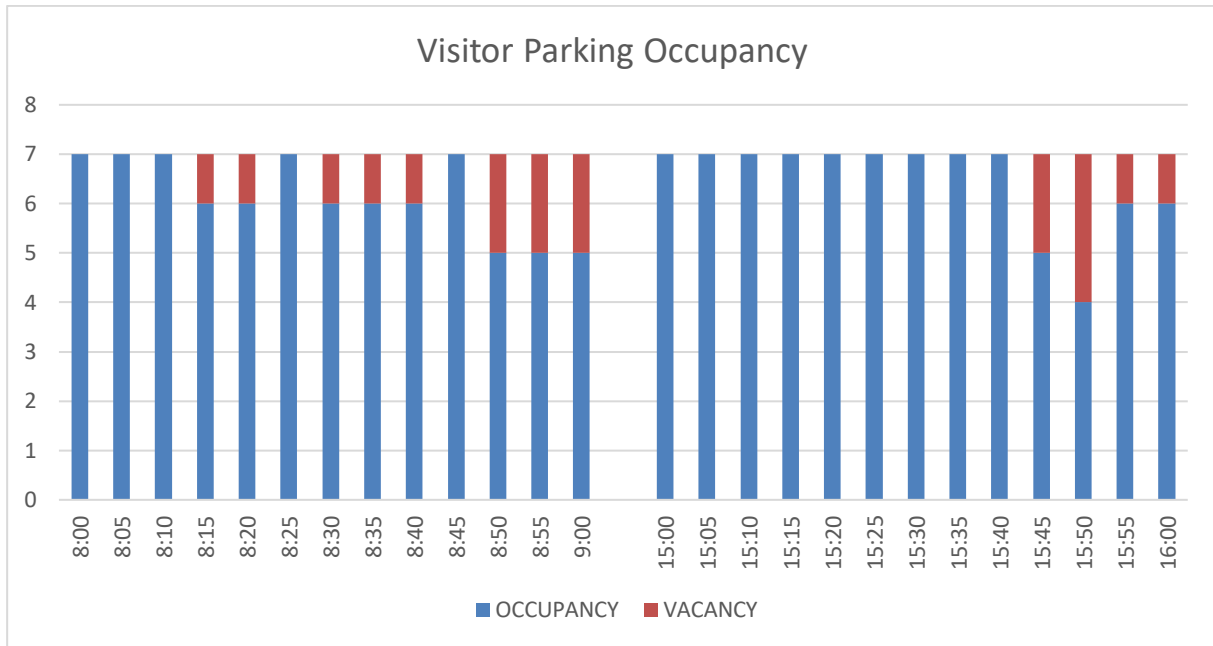
To understand the existing parking occupancy within the campus, parking occupancy surveys were commissioned by TTW within the campus grounds. The survey was undertaken during the school term period on 7 August 2025 from 8:00 am to 9:00 am during the AM peak period, and 3:00 pm to 4:00 pm in the PM peak period, in 5-minute intervals.

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All-Day Parking



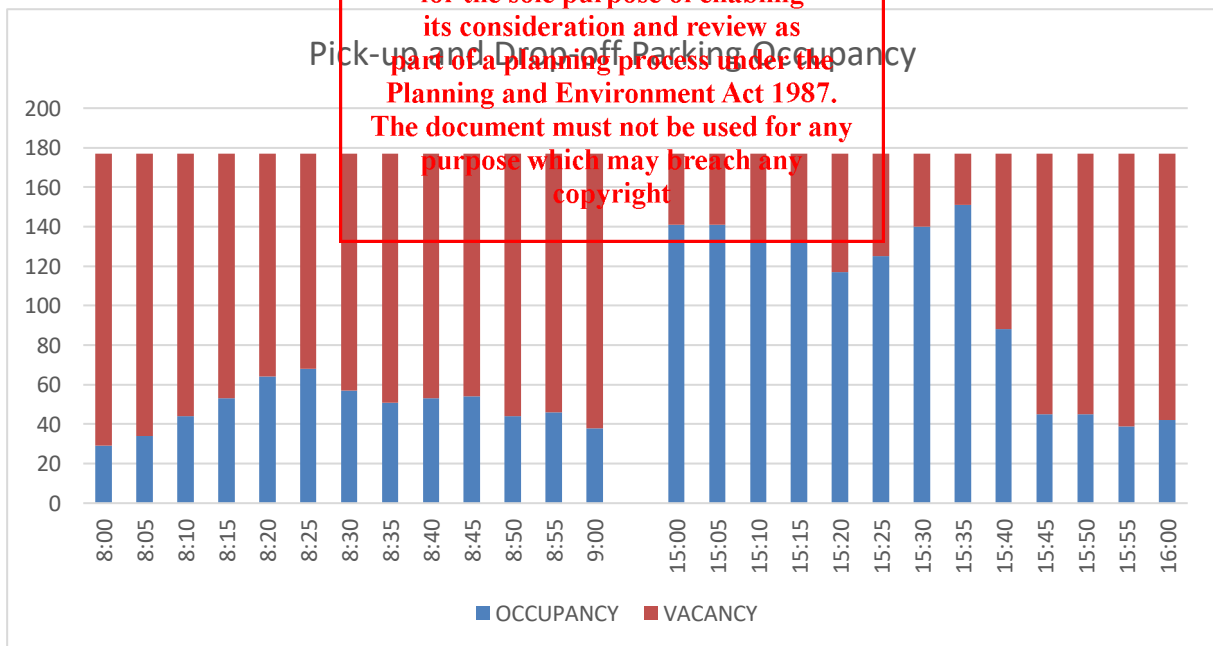
For staff and general all-day parking, during the AM peak, the peak parking occupancy rate is 54% (182 spaces). In the PM peak, parking occupancy peaks at 61% (206 spaces), confirming that long-term parking remains well within capacity throughout the day.



The all-day visitor only parking spaces are generally mostly or fully occupied during the AM and PM peak periods.

Pick-up & Drop-off Parking

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The peak pick-up and drop-off hours occurred between 3:00pm and 3:35pm, with occupancy reaching its highest level of 86% (103 spaces out of 120 spaces) at 3:35 pm. Importantly, demand never exceeded capacity at any time throughout the day, indicating that the available bays were sufficient throughout the school day.

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3.5 School Operations

Caulfield Grammar School operates from Monday to Friday, with core school hours of 8:30 am to 3:30 pm. The junior and senior school start and finish times are understood to be slightly staggered.

Table 3.4 below provides an overview of the school enrolments of School Year 2025, as advised by the school.

Table 3.4: School Enrolments and Operational Details (As of November 2025)

JUNIOR SCHOOL ENROLMENTS	
STUDENTS	487
STAFF	90
SENIOR SCHOOL ENROLMENTS	
STUDENTS	1086
STAFF	180

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4 CAR PARKING

4.1 Statutory Car Parking Requirements

As no changes are proposed to staff numbers, no additional parking is required under Clause 52.06-2. Rather, this application proposes the removal of 19 general parking spaces and five (5) pick-up / drop-off spaces within the site. That said, the statutory car parking requirements will continue to be met, as confirmed by the car parking demand assessment below in Section 4.2.1.

4.2 Car Parking Demand Assessment

4.2.1 Total Parking Provision

As noted in Section 2.3.1, following the removal of spaces, the post-development parking provision will total 438 spaces. This equates to a provision of approximately 1.62 spaces per staff, which comfortably exceeds the statutory parking requirement for both Primary School (1 space per staff) and Secondary School (1.2 spaces per staff) under Clause 52.06-2.

Given that the post-development provision remains well above the relevant statutory rates, the proposed reduction in existing car parking is considered entirely reasonable and will continue to adequately support campus operations without generating any parking issues.

Sections 4.2.2 and 4.2.3 below provide a detailed breakdown of the nominated parking spaces for the relevant user classes, and their respective adequacies.

4.2.2 All-Day (Long-Term) General Parking Availability

Referencing the parking survey results in Section 3.4, all-day staff / general parking occupancy within the school is only 56% (153 spaces out of the existing 335 spaces) at the absolute peak period, indicating ample availability of all-day parking within campus grounds.

This confirms that there are currently no car parking provision issues on campus, and the proposed removal of 19 general parking spaces will not adversely impact parking availability. Notably, these 19 spaces are located at the far eastern end of the campus and are rarely occupied, further supporting the conclusion that their removal will have negligible effect on overall parking capacity.

4.2.3 Pick-up & Drop-off (Short-Term) Parking Availability

Similarly, the detailed parking survey results show that the five (5) pick-up & drop-off spaces (located furthest east of the campus) which are proposed for removal are typically never utilised as these spaces are out of the way and students do not wait here, indicating these spaces have historically been surplus to requirements.

Hence, the removal of these five (5) pick-up and drop-off spaces will not have any adverse impacts on ongoing pick-up and drop-off activities.

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5 TRAFFIC CONSIDERATIONS

As noted in Section 2.2, the student and staff numbers are proposed to remain unchanged. The development simply replaces existing facilities and does not expand the school's operational capacity.

Accordingly, no additional traffic impact is anticipated as a result of the proposal.

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6 BICYCLE FACILITIES

6.1 Statutory Bicycle Parking Requirements

Clause 52.34 of the Monash Planning Scheme outlines the details of bicycle facilities provisions.

As the proposed development does not increase student or staff numbers, no additional bicycle facilities are required under this clause.

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7 LOADING AND WASTE COLLECTION

7.1 Loading Considerations

Clause 65.01 of the Planning Scheme states:

'Before deciding on an application or approval of a plan, the responsible authority must consider, as appropriate, the adequacy of loading and unloading facilities and any associated amenity, traffic flow and road safety impacts.'

Loading activities at the school is anticipated to remain infrequent. Consistent with current operations, any required loading activities are expected to continue within the on-campus car parks.

7.2 Waste Collection Considerations

No changes are proposed to the current waste collection arrangements.

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8 CONCLUSION

This Traffic Impact Assessment has been prepared to support the proposed development at the Caulfield Grammar School Wheelers Hill campus.

Key findings of the assessment are summarised as follows:

- The application seeks to rehouse existing students and staff within upgraded facilities at a new teaching and learning building, and does not increase enrolment or staffing numbers.
- A net reduction of 19 general parking spaces and 5 pick-up/drop-off spaces are proposed. Survey data confirms that existing parking demand can be readily accommodated within the revised total of 438 on-site spaces.
- The proposed total parking provision provides a rate of 1.62 spaces per staff member, demonstrating suitable on-site parking capacity.
- The removal of five pick-up/drop-off spaces is acceptable, as surveys indicate these bays are historically unutilised.
- No changes to access arrangements are proposed, and all movements will continue to operate as per existing conditions.
- No additional traffic generation is expected beyond existing conditions.
- There is no requirement to provide additional bicycle parking, and none are proposed.
- Loading and waste collection activities will continue under current arrangements and are considered adequate.

Overall, the works are deemed supportable from a traffic and parking perspective, with no unreasonable impacts on the surrounding road network.

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