

Emmanuel College, St Paul's Campus

Planning Permit Application

423 Blackshaws Road, Altona North VIC 3025

Submitted to: Department of Transport and Planning

On behalf of: Watson Young Architects Pty Ltd

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'Dagura Buumarri'

Liz Belanjee Cameron

'Dagura Buumarri' – translates to Cold Brown Country. Representing Victoria.

The river system illustrated in this visual image is bound in greens and golds to acknowledge the warmth often felt in a colder climate. The rich earth hues of green, reds and browns reflect the local landscapes of this state while the extensive use of rhythmical patterning captures the unique landscapes of flat and mountainous areas. The use of earth colours imparts a sense of strength and serenity while contrasting greens throughout the image reminds us of the lushness of the natural world, where animals and humans once lived in harmony – it reminds us of the importance to protect the lands, waterways and skies and care for our localised environment. Scattered throughout the image are bold colours of oranges – a source of energy that continues to be felt as a life-giving source. The orange hues also portray the varying sunsets in which many Victorians enjoy seeing earth, waterways and skies.

Ethos Urban acknowledges the Traditional Custodians of Country throughout Australia and recognises their continuing connection to land, waters and culture.

We acknowledge the Wurundjeri Woi Wurrung people, of the Kulin Nation, the Traditional Custodians of the land where this document was prepared, and all peoples and nations from lands affected.

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1.0 Executive Summary

Proposal

This application seeks approval of a masterplan planning permit that enables the following:

- Approval of a 10-year masterplan to guide the development of Emmanuel College, St Paul's Campus;
- Approval of the Stage 1 development; and
- Further stages to be endorsed under the single permit.

Stage 1 of the masterplan includes:

- Demolition of the existing McClusky music centre;
- Demolition of the existing Marianist building;
- Demolition of the performing arts & PE storage sheds;
- Construction of a new two-storey building to be built in the location shown; and
- Upgrades to the existing bus parking on site and the addition of six parking spaces on site.

Site Location and Context

The subject site is located at 423 Blackshaws Road, Altona North approximately 10km south-west of Melbourne's Central Business District. The subject site occupies a rectangular-shaped parcel of land with an approximate area of 79,280m² which accommodates the St Paul's Campus of Emmanuel College.

The site is located at the intersection of Blackshaws Road and Chambers Road and has frontages to both streets along its northern and eastern boundaries, respectively. The site's western interface adjoins the rear of conventional-density, single-storey residential development, which is separated by a wooden paling fence along the boundary. To the south, the subject site abuts a minor business park and automotive engineering practice. A metal security fence separates these commercial uses from the secondary school. The south portion of the property also features large canopy trees along the southern boundary, which provide visual separation from the industrial uses.

The immediate surrounds include residential properties along most interfaces, and industrial development along the southern interface and north-eastern corner.

The broader surrounds feature a range of commercial, industrial, residential and public land uses. The Kororoit Creek Reserve is located 200m west of the site, which offers a number of recreational and active transport opportunities.

There is a range of low and high order commercial and retail activity in the area:

- The Blackshaws Road/Millers Road Medium Neighbourhood Activity Centre is located just opposite Chambers Road to the east.
- The Misten Avenue/Blackshaws Road Micro Centre 65m north-west of the subject site opposite Blackshaws Road.
- The Altona Gate Major Activity Centre is located 500m north-east of the site.

Residential development in the surrounding area is largely single and double storey at conventional densities. There is an emerging pattern of medium density infill. Notably the former government school has been converted into the Project Alfie townhouse development.

The site has public transport accessibility, with the closest bus stops from two bus routes located adjacent to the site along Blackshaws Road and Chambers Road. Four additional bus routes run along Millers Road.

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Planning Permit Triggers

- Pursuant to Clause 32.08-9, a permit is required to construct a building or construct or carry out works for a Section 2 use.
- Pursuant to Clause 52.05-13, a permit is required to erect and display a Business identification sign.

Reasons why the Proposal should be Supported

The development satisfactorily responds to the relevant sections of the Hobsons Bay Planning Scheme and should be supported as:

- The proposed development will enable Emmanuel College to continue to provide for the educational needs of the local community in an integrated and orderly manner, consistent with Clauses 02.03-9, 11.01-1S, 11.01-1R, 11.03-1S, 11.03-1R, 15.01-1S and 19.02-2S in the Hobsons Bay Planning Scheme.
- The proposed masterplan represents an orderly and sequential development that will improve the facilities of the school and enable it to service the growing local community consistent with Clauses 02.03-9, 11.01-1S, 11.01-1R, 11.03-1S, 11.03-1R, 15.01-1S and 19.02-2S.
- The proposed Stage 1 development and subsequent stages of the masterplan at the existing campus are consistent with the purpose, objectives and decision guidelines of the GRZ3 which include the delivery of educational and community uses that support the needs of the local community.
- The scale and intensity of the development are compatible with adjoining residential uses.
- The traffic report confirms the anticipated increase in traffic is considered to be negligible and will have a low impact on the operation of the surrounding road network.
- The design of the new Marianist building is high-quality and provides a highly articulated façade.
- The proposed signage appropriately responds to the relevant decision guidelines of Clause 52.05-8. The signs appropriately identify the use of the building. The signs are consistent with that which can be reasonably expected at a school.
- The development complies with the statutory car and bicycle parking requirements associated with the expected growth of the school.
- The existing waste management arrangements are sufficient to support the increase in student and staff numbers.
- The proposal is consistent with Decision Guidelines at Clause 65.01.

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2.0 Overview and Context

2.1. Introduction

Ethos Urban has been engaged by Watson Young Architects Pty Ltd to prepare, lodge and manage a planning permit application for the development of the New Marianist Building at Emmanuel College, St Paul's Campus, located at 423 Blackshaws Road, Altona North VIC 3025 (the subject site).

The site is currently being used for the purpose of a Secondary school and the campus features a number of buildings and outdoor areas associated with this use.

A Masterplan Planning Permit is being sought as part of this application to:

- Approve the masterplan and the staged development of the new school buildings;
- Approve Stage 1 development; and
- Enable further stages to be endorsed under the same permit.

The masterplan includes six stages with a Learning Centre building to replace the existing Marianist House as part of Stage 1.

This report is prepared to provide an assessment of the new works being proposed for Stage 1 of the masterplan as part of the New Marianist Precinct. This stage includes:

- Demolition of the existing McClusky music centre;
- Demolition of the existing Marianist building;
- Demolition of the performing arts & PE storage sheds;
- Construction of a new two-storey building to be built in the location shown; and
- Upgrades to the existing bus parking on site.

The proposed masterplan indicating later stages has also been submitted to assist the application.

The following documentation has been appended to this report:

- **Appendix A:** Planning Policy Framework Review
- **Appendix B:** Certificate of Title
- **Appendix C:** Masterplan and Development Plans
- **Appendix D:** Landscape Masterplan and Detailed Landscape Plans
- **Appendix E:** Traffic Impact Assessment
- **Appendix F:** Green Travel Plan
- **Appendix G:** Sustainable Design Assessment
- **Appendix H:** Stormwater Management Plan
- **Appendix I:** Waste Management Arrangements Supporting Letter
- **Appendix J:** Arboricultural Assessment Report

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2.2. Planning Summary

The land is located within the General Residential Zone – Schedule 3 (GRZ3). The site is not affected by any overlays. Blackshaws Road, on the northern interface, is a road zoned Principal Road Network (TRZ2). Millers Road, also zoned TRZ, is located 415m east of the site.

Adjacent uses include land zoned General Residential Zone immediately east and west of the site, and Industrial 3 Zone to the south and in the north-eastern interface. Other surrounding land comprises a range of commercial, industrial, residential and public land uses.

Table 1 provides an overview of the relevant information regarding the application, as well as the planning provisions and policies that are addressed in this report.

Table 1 Planning Summary

Item	Application Specifics
Address	423 Blackshaws Road, Altona North VIC 3025
Existing Use and Development	The site is currently being used for the purpose of a Secondary school. Development on-site features a number of buildings and outdoor areas associated with this use.
Proposed Use and Development	<ul style="list-style-type: none"> • Masterplan and staged development of new school buildings; • Stage 1 development, which includes: <ul style="list-style-type: none"> - Demolition of the existing Mc Clusky music centre; - Demolition of the existing Marianist building; - Demolition of the performing arts & PE storage sheds; - Construction of a new two-storey building to be built in the location shown; and - Upgrades to the existing bus parking on site.
Existing Zone(s)	General Residential Zone – Schedule 3
Overlay(s)	No overlays apply to the land
Planning Policy Framework	<ul style="list-style-type: none"> • Clause 02 – Municipal Planning Strategy • Clause 11 – Settlement • Clause 13 – Environmental Risks and Amenity • Clause 15 – Built Environment and Heritage • Clause 17 – Economic Development • Clause 18 – Transport • Clause 19 – Infrastructure
Particular Provisions	<ul style="list-style-type: none"> • Clause 52.05 - Signs • Clause 52.06 – Car parking • Clause 52.17 – Native vegetation • Clause 52.34 – Bicycle facilities • Clause 53.18 – Stormwater management in urban development • Clause 53.19 – Non-government school
Application Triggers	<p>GRZ3</p> <ul style="list-style-type: none"> • Pursuant to Clause 32.08-9, a permit is required to construct a building or construct or carry out works for a Section 2 use. <p>Clause 52.05 – Signs</p> <ul style="list-style-type: none"> • Pursuant to Clause 52.05-13, a permit is required to erect and display a Business identification sign.
Restrictive Covenants & Easements	<p>There is one easement along the western boundary on titles 2 and 6 that relates to drainage. The proposal does not relate to that part of the site that is affected by easements.</p> <p>There are no covenants or Section 173 agreements registered on the title.</p>
Incorporated Documents	No incorporated documents relevant to the proposal.
Reference Documents	<ul style="list-style-type: none"> • Plan Melbourne 2017-2050 • Hobsons Bay Activity Centres Strategy (2019-36)
Title	Lots 1,2,3,4,5 and 6 on Title Plan 366129P

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2.3. Site and Surrounds

2.3.1. Subject Site

The subject site is located at 423 Blackshaws Road, Altona North, approximately 10km south-west of Melbourne's Central Business District. The subject site occupies a rectangular-shaped parcel of land with an approximate area of 79,280m² which accommodates the St Paul's Campus of Emmanuel College..

The site is located at the intersection of Blackshaws Road and Chambers Road and has frontages to both streets along its northern and eastern boundaries, respectively. The site's western interface adjoins the rear of low-density, single-storey residential development, which is separated by a wooden paling fence along the boundary. To the south, the subject site abuts a minor business park and automotive engineering practice. A metal security fence separates these commercial uses from the secondary school. The south portion of the property also features large canopy trees along the southern boundary, which provide visual separation from the industrial uses.

The school's built form is concentrated in the central portion of the site. Existing development on site includes:

- A single-storey McClusky music centre;
- A two-storey Marianist house building;
- A single-storey PE shed;
- A three-storey jubilee building;
- A single-storey Chaminade library;
- A single-storey Cassidy technology building;
- A single-storey Winters building;
- A two-storey administration and McMahon Centre building;
- A single-storey McCoy hall;
- Single-storey maintenance and storage sheds;
- Vehicle parking, access and internal roads;
- Outdoor play areas and small sporting courts around the existing buildings;
- A multi-purpose sporting courts located in the north-western portion of the site; and
- A soccer field in the south-eastern portion of the land.

The north-eastern and south-western portions of the site are currently undeveloped.

The site has one main vehicle access point for students and parents, which is located on the east off Chambers Road; the exit point is located 75m to the south of the entry point. A designated staff and deliveries access only point is located in the north-west corner of the site, off Blackshaws Road. The entry and exit points are connected via one-way internal roads, which run north-south and east-west. The campus currently provides for 103 car parking spaces, including 2 accessible spaces. Car parking areas are currently provided across five locations:

- 50 spaces in the car parking area located in the north-west of the property;
- 2 spaces near the existing Marianist building;
- 10 spaces along the internal north-south road;
- 3 spaces adjacent to the Jubilee building; and
- 38 spaces near McCoy Hall on the east side.

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Figure 1 Locational Context

Source: Ethos Urban and NearMap, 2023



Figure 2 Aerial Image

Source: Ethos Urban and NearMap, 2023

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2.3.2. Subject Context

The immediate surrounds include residential properties along most interfaces, and industrial development along the southern interface and north-eastern corner.

The broader surrounds feature a range of commercial, industrial, residential and public land uses. The Kororoit Creek Reserve is located 200m west of the site, which offers a number of recreational and active transport opportunities.

There is a range of low and high order commercial and retail activity in the area:

- The Blackshaws Road/Millers Road Medium Neighbourhood Activity Centre is located just opposite Chambers Road to the east.
- The Misten Avenue/Blackshaws Road Micro Centre 65m north-west of the subject site opposite Blackshaws Road.
- The Altona Gate Major Activity Centre is located 500m north-east of the site.

Most residential development in the surrounding area features single-storey housing. Exceptions to this are the medium-density townhouse development north-west of the site and some scattered dwellings that comprise two storeys.

The site has public transport accessibility, with the closest bus stops from two bus routes located adjacent to the site along Blackshaws Road and Chambers Road. Four additional bus routes run along Millers Road.

Nearby roads and rail lines include:

- Millers Road, located 415m east of the site;
- The nearest access to West Gate Freeway is located 800m to the north-east, accessed via Millers Road;
- Grieve Parade, located 1.3km to the west;
- Princes Highway, located 1.3km to the north;
- Melbourne Road, located 3.5km to the east; and
- The Werribee Railway Line, which runs 1.5km south of the site and 3.7km to the east, with the nearest station on that line being Spotswood Railway Station, 3.75km from the site.

2.4. Approved Permits

Table 2 provides an overview of the planning permits that have previously been approved on the subject site.

The proposed masterplan does not affect the conditions on these permits.

Table 2 *Approved permits on the site*

Permit Number	Date	What the permit allows
PA0918232	30 April 2009	Alterations and additions to the existing Emmanuel College in accordance with the endorsed plans.
PA1224903	14 June 2012	The installation of shade sails and landscaping works in accordance with the endorsed plans.
PA1430365	9 February 2015	Construction of a roof plant deck and associated screening in accordance with the endorsed plans.
PA1634877	23 August 2016	Alterations and additions to the existing secondary school in accordance with the endorsed plans.
VS1841578	12 December 2018	Alterations to existing building in accordance with the endorsed plans.

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2.5. Certificate of Title and Encumbrance

The land comprises six parcels within six certificates of title, which are referred to as Lots 1,2,3,4,5 and 6 on Title Plan 366129P. There is one easement along the western boundary on titles 2 and 6 that relates to drainage. The proposal does not relate to that part of the site that is affected by easements. There are no covenants or Section 173 agreements registered on the title. A copy of the certificate of title is provided at Appendix B.

3.0 The Proposal

3.1. Overview

The application is proposing a new ten-year masterplan to guide the development of the St Paul's Campus of Emmanuel College. The masterplan includes six stages with a New Marianist building containing Specialist learning & Administration facilities to replace the existing Marianist House as part of Stage 1. An outline of each stage of the masterplan is provided below and illustrated in Figure 2.

Specifically, the application seeks approval of a masterplan planning permit that enables the following:

- Approval of a 10 year masterplan to guide the development of Emmanuel College, St Paul's Campus;
- Approval of the Stage 1 development; and
- Further stages to be endorsed under the single permit.

As the secondary school is existing and operational, this application only relates to buildings and works at the site.

Stage 1 (Anticipated construction date 2023-2024)

New Marianist Precinct. This stage includes demolition of the existing McClusky music centre, Marianist building, performing arts & PE storage sheds. A new two storey "Marianist" building will be built in this location. Improvements are also proposed to the existing bus parking and six additional car parking spaces are proposed.

Stage 2 (Anticipated construction date 2026)

This stage includes a new car park fronting Chambers Road to address future parking requirements. The new car park will have 38 additional car parking spaces.

Stage 3 (Anticipated construction date 2025-2026)

This stage includes following items:

- 3A New sports centre at south- west
- 3B Extension of existing vehicular road up to new sports centre for drop off/pick up.
- 3C Demolition of existing Storage shed and addition of new bicycles parking. 20 additional bicycle spaces to be provided.

Stage 4 (Anticipated construction date 2025)

This stage includes the refurbishment of internal spaces of the existing administration adjacent to McCoy Hall.

- 4A Addition of new reception and waiting area close to main entry of the campus.
- 4B Demolition of internal administration offices and staff spaces, to reuse the space for study hall
- 4C Potential extension of existing canteen to add in food tech. VET area TBC with school

Stage 5 (Anticipated construction date 2026-2027)

This stage includes a potential VET centre. The centre may be used by students from both Emmanuel College campuses and would feature the following spaces:

- Hair & make-up
- VET fashion with a sewing setup
- Dance with sprung floor facility
- VET Warehousing - this an external offering in conjunction with external businesses. Students may spend 3 days on campus and 1 day at TAFE, 1 day at placement.

Stage 6 (Anticipated construction date 2027-2028)

This stage includes further expansion of the established Sport precinct after the Stage 3 works are complete.

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- 6A Potential new basketball court to add to established Sports precinct (stage 3)
- 6B Potential half hockey court
- 6C Potential landscape to integrate pedestrian movements with existing framework
- 6D Potential Cricket net/ Golf practise

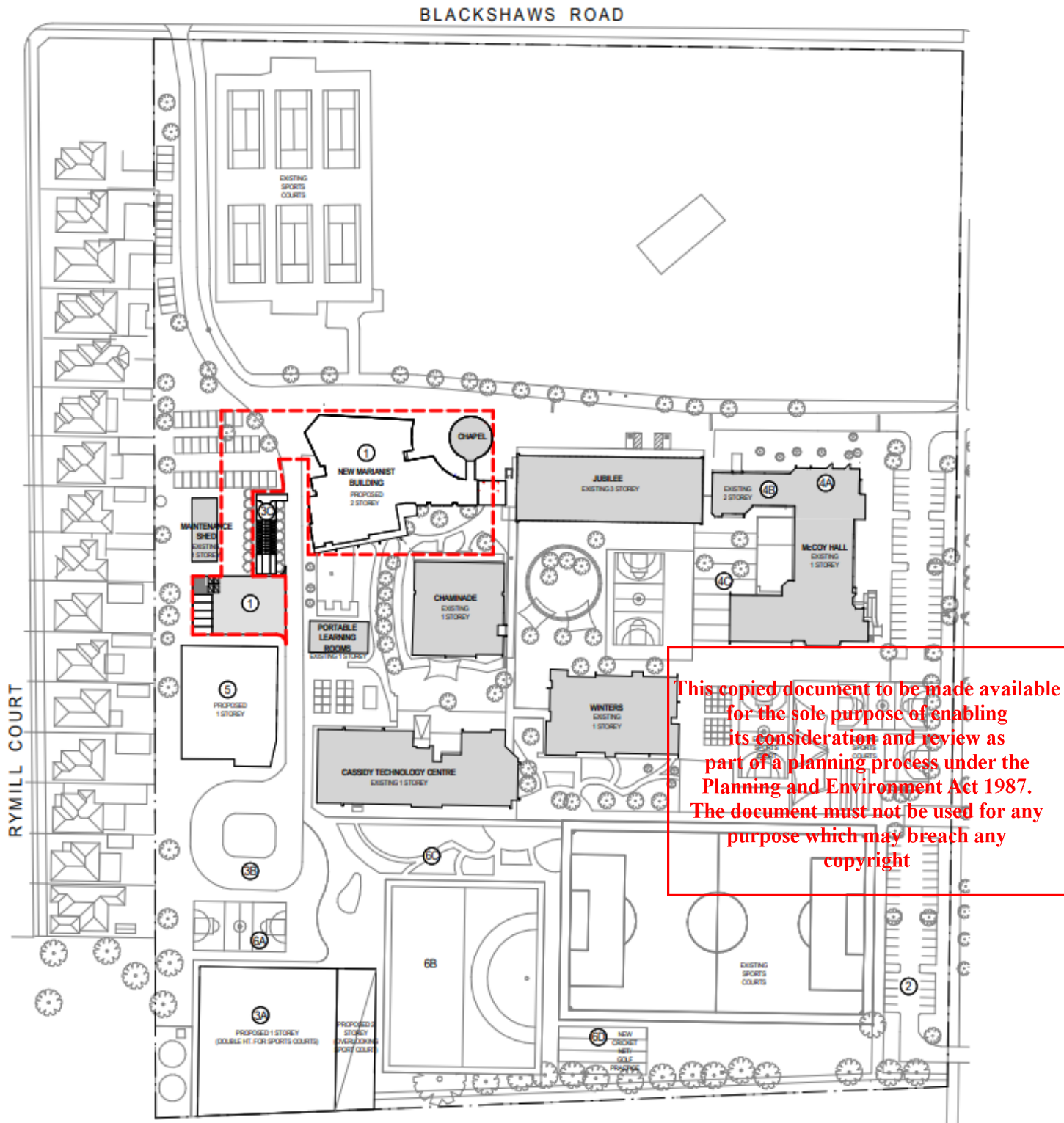


Figure 2 Proposed Masterplan

Source: Watson Young Architects, 2023

Details of Stage 1 works are provided in the following sections.

3.2. Demolition, Buildings and Works

The proposed Stage 1 development will be built where the McClusky music centre, Marianist building and Performing arts and PE storage shed are currently located. as such, demolition, relocation and removal works are required to accommodate this new building. The proposed works include:

- Demolition of the existing McClusky music centre;
- Demolition of the existing Marianist building, including steps and landing structure from walkway to the east;
- Demolition of the performing arts & PE storage sheds;
- Removal of 10 trees;
- Relocation of existing water tank near existing Marianist building;
- Construction of a new “Marianist” building;
- Improvements to bus parking and the addition of four car parking spaces; and
- Associated outdoor improvements and landscaping

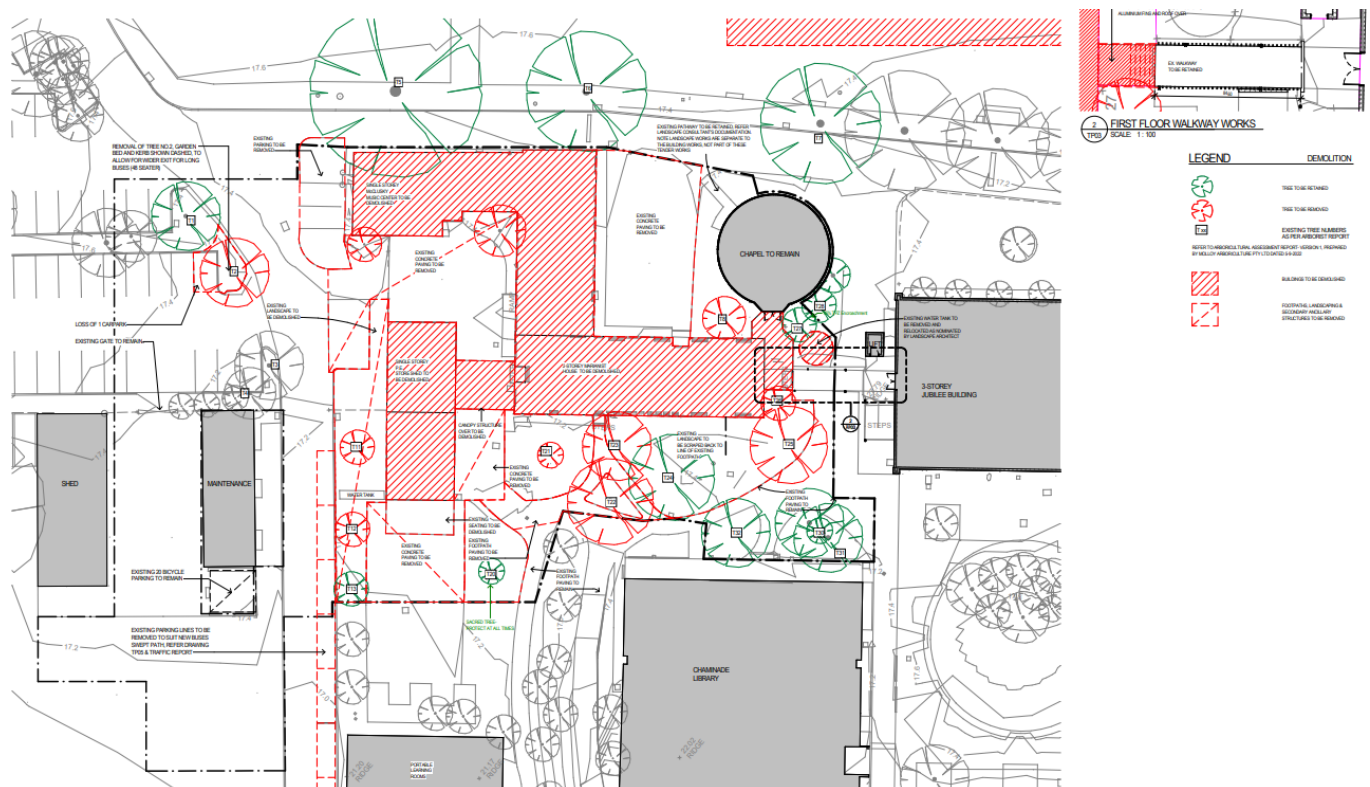


Figure 3 Demolition Plan

Source: Watson Young Architects, 2023

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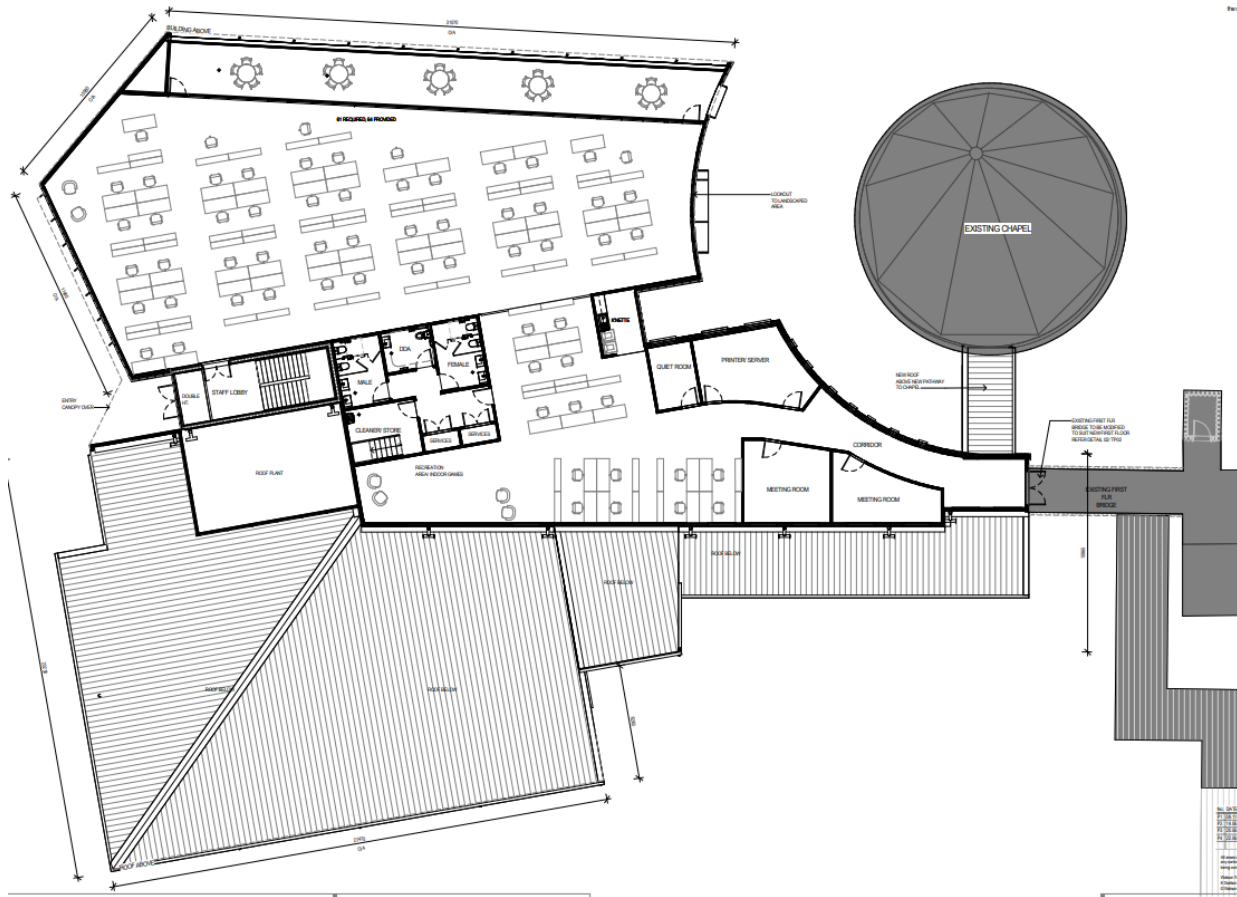


Figure 5 Proposed new Marianist building, first floor

Source: Watson Young Architects, 2023

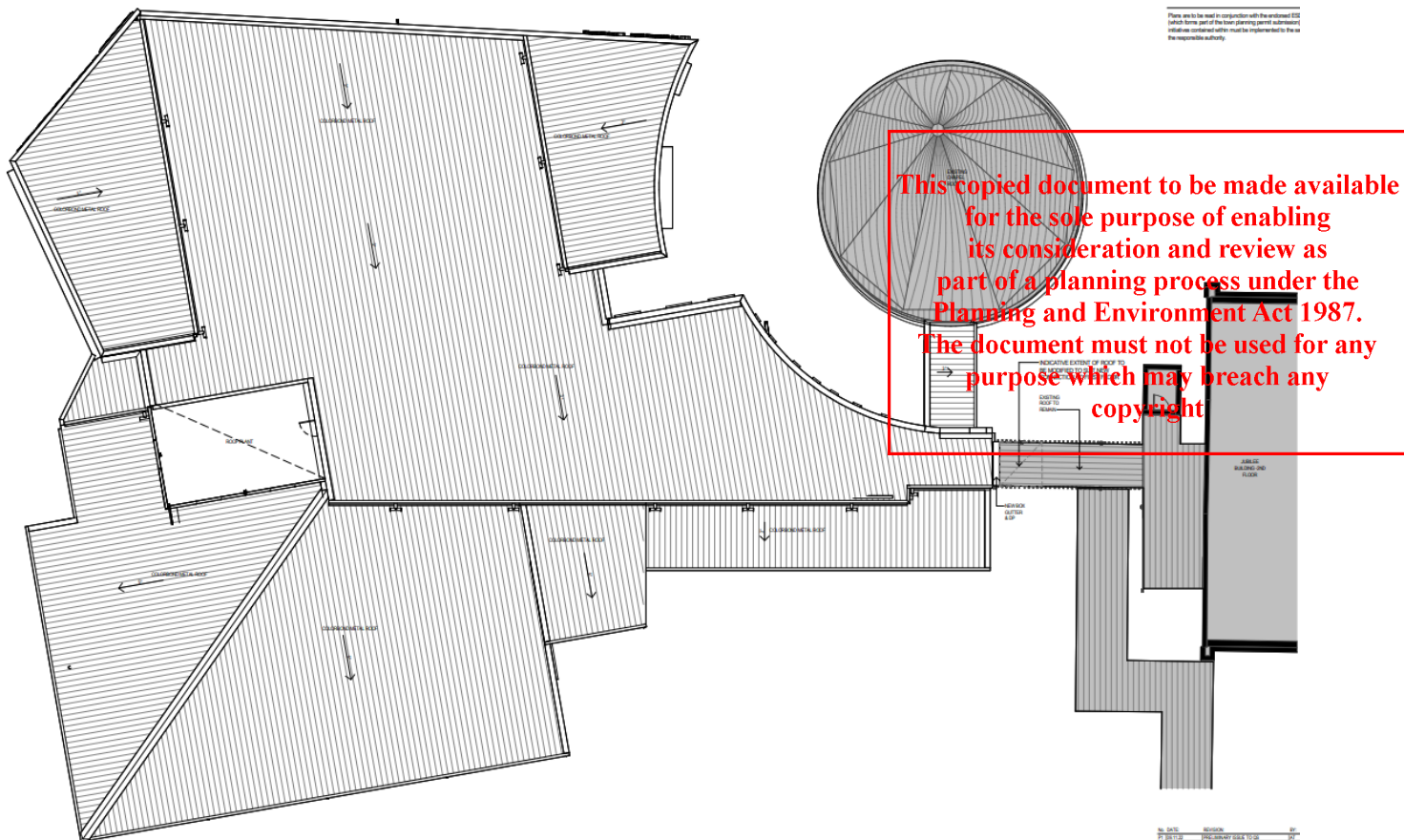


Figure 6 Proposed new Marianist building, roof plan

Source: Watson Young Architects, 2023



3D NORTH-EAST ELEVATION
SCALE



3D SOUTHERN ENTRY
SCALE



3D WEST STAFF ENTRY
SCALE

Figure 7 Renders

Source: Watson Young Architects, 2023

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No.	Revisions	Date
01	PROJECT PRESENTATION	2023-01-01
02	PROJECT PRESENTATION	2023-01-01
03	PROJECT PRESENTATION	2023-01-01
04	PROJECT PRESENTATION	2023-01-01
05	PROJECT PRESENTATION	2023-01-01

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3.3. Built Form and Design Response

The proposed Stage 1 development will be located at the south-east portion of the intersection of the two internal roads. As such, it will be set back from the western boundary consistent with the existing Marianist building providing appropriate distancing from the existing residential interface. The building will also be set back from Blackshaws Road and be positioned behind the existing sporting areas. The eastern portion of the building will encompass the existing chapel.

The building will have three entries: two entries will be positioned along the southern façade and will be accessed via a new paved area, and another entry on the eastern side facing the existing Jubilee building. The building will also incorporate a ramp connecting to the chapel.

The first-floor built form is of a greater scale but lighter appearance than that of the ground level. The building will reach a maximum height of 10.4m above finished floor level, sitting just below the roof line of the existing Jubilee building, and will feature a pitched and hipped roof.

The ground floor has been designed to be laid out along a main east-west internal circulation, to which all administrative uses open. The music room and band room are located on the southern edge of the building to maximise access of natural light. The staff lunch area is on the northern side to maximise sunlight. A staircase on the western side leads to the first floor. Level 1 is an open plan floor with a number of enclosed meeting and quiet rooms. A corridor on the eastern portion leads to a bridge that connects to the Jubilee building. The services on both levels are centrally located.

The development incorporates a number of design elements for the visual treatment of the façade including façade articulation, recessed windows, breaks in the façade, projection of level 1 over the ground floor level, use of external finishes at upper level with a lighter appearance and accent fibre cement sheets or lighter colours to break up the building's height and façade length.

The building will incorporate the following materials for the external finishes:

- Dark and medium grey face brickwork
- Fibre cement sheet
- Profiled colorbond cladding, in dark, medium and light grey tones
- Aluminium windows black powder coated
- Coloured glass
- Painted cement sheet
- Grey mist colourblack glass
- Aluminium shroud
- Powder coated aluminium louvres
- Colorbond fascia

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Figure 8 North elevation

Source: Watson Young Architects, 2023

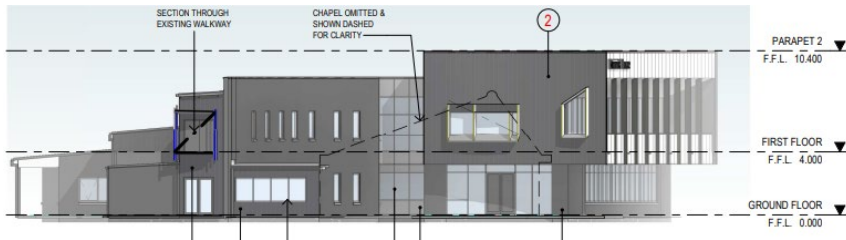


Figure 9 East elevation

Source: Watson Young Architects, 2023



Figure 10 North-west elevation

Source: Watson Young Architects, 2023

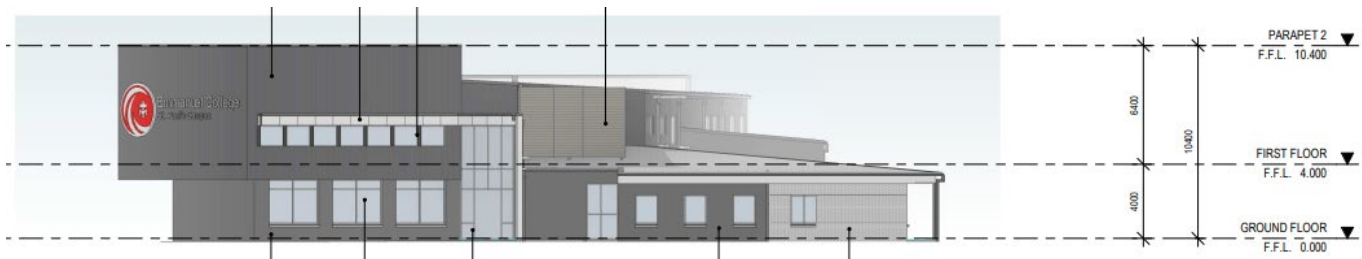


Figure 11 West elevation

Source: Watson Young Architects, 2023

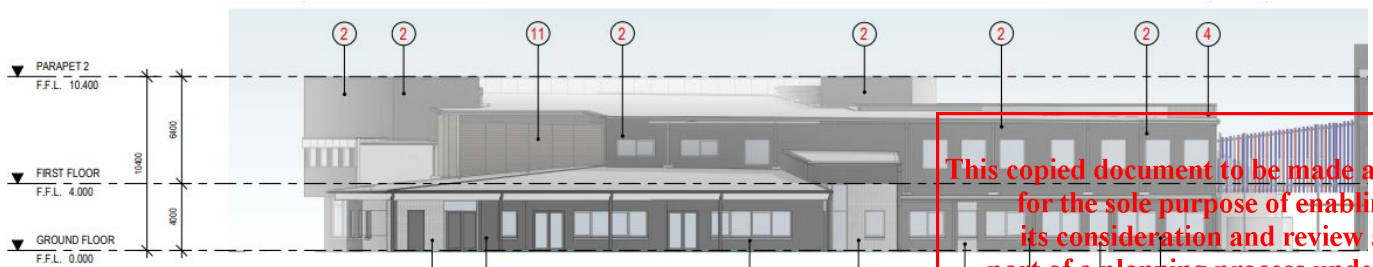


Figure 12 South elevation

Source: Watson Young Architects, 2023

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3.4. Environmentally Sustainable Design (ESD)

A Sustainable Design Assessment (Appendix F) has been prepared which includes initiatives for energy performance, integrated water management, indoor environment quality, transport (supported also by the Green Travel Plan), waste management and urban ecology.

A summary of the major ESD initiatives to be included in the development are listed:

- Net Zero carbon / carbon neutral capability
 - The development will be future proofed and built to enable net zero carbon emissions capability in operation. The project has electricity rather than gas as the fuel source for all energy uses: space heating, hot water heating and cooking.
- Energy
 - 10% increase on required NCC2019 insulation levels for floor and roof elements
 - Energy efficient heating and cooling systems beyond minimum standards (within one star of best available)
 - Energy efficient electric heat pump storage hot water units

- Renewable energy Photovoltaic (PV) Panels of 15kW capacity
- Water and Stormwater
 - On-site water use and infiltration measures to meet CSIRO Best Practice Stormwater Management (Water Sensitive Urban Design) treatment quality requirements
 - Rainwater tanks of 25,000L retention capacity connected to all toilets to reduce potable water consumption and assist with stormwater quality management requirements.
 - Potable (drinking) water saving measures including low flow toilets, showers and taps
- Indoor Environment Quality (IEQ)
 - Daylight levels assessed to Best Practice standards
 - Minimised indoor pollutants from the use of low off-gassing materials such as low VOC paints, carpets and adhesives, and low formaldehyde products
- Sustainable materials
 - Avoidance of the use of endangered rainforest timbers in this development.
 - Use of lower embodied carbon/energy alternatives for concrete.
 - More environmentally friendly material alternatives for timber, insulation and other building components
- Urban ecology
 - Light -medium coloured roofing to help mitigate the effects of the Urban Heat Island effect

3.5. Signage

Stage 1 works also include the display of two Business identification signs:

- 1 sign displaying the name of the education centre and its logo will be located on the north-western façade on first level. The sign will have a display area of approximately 20m².
- 1 sign displaying the name of the building on the north façade at parapet 2 level. The sign will have a display area of approximately 3.5m².

The signs will not be illuminated.

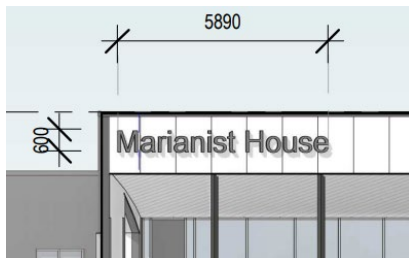


Figure 13 Sign on the north façade

Source: Watson Young Architects, 2023

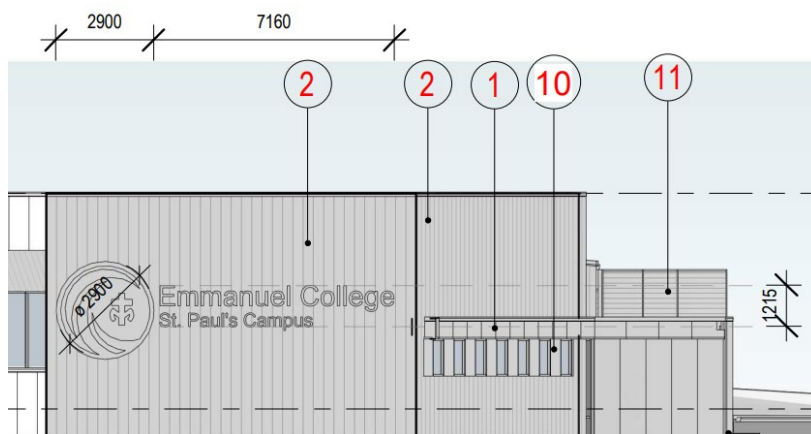


Figure 14 Sign on the north-west façade

Source: Watson Young Architects, 2023

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3.6. Student and Staff Numbers

Stage 1 development will not result in any increase in student or staff population. However, the school expects the campus to see an increase of 56 students by 2041. In line with the student growth, the school anticipates that an additional 5 full-time staff members will be required by 2041.

3.7. Access, Car Parking and Bicycle Facilities

The masterplan will provide 34 additional car parking spaces and 20 additional bicycle spaces on-site to cater for the increase student and staff numbers.

Access to the school will be maintained via the existing two-way vehicular crossover to Blackshaws Road to the north and the existing two-way crossover to Chambers Road to the east. Stage 1 does not propose any changes to the existing vehicular access besides minor kerb modifications to facilitate 12.5m bus turn around. Swept paths are shown in Appendix A of the Traffic Impact Assessment report at Appendix D.

Stage 1 also includes an extension to the existing maintenance and storage area located immediately west of the Marianist Building, which will provide an extended pavement area to improve bus access and turnaround in addition to the relocation of all waste bins to this area to facilitate improved waste collection vehicle access and loading.

3.8. Waste Management

Waste collection is undertaken during school operational hours via a 28-tonne front loading truck at the existing McClusky Music Centre, where the waste bins are currently stored. The proposed Stage 1 works will formalise a waste storage and collection area between the existing maintenance and storage sheds and the new PE storage area. Additionally, it is expected that the proposed paved area will facilitate all large loading activities for the school to be completed outside of operational hours.

It is expected that the minor increase of student and staff numbers by 56 and 5 respectively will not lead to a change in waste generated on-site. The existing waste management arrangements are considered appropriate to support the increase in student and staff numbers and as such an updated Waste Management Plan is not provided as part of this application.

3.9. Landscaping

The development will provide setbacks to the north-south and east-west internal roads, allowing to accommodate landscaped areas surrounding the new building.

A Landscape Masterplan and Detailed Landscape Plans for Stage 1 have been prepared by MemLa and submitted in support of the application. The landscape masterplan details the proposed areas of landscaping that will be delivered as part of each stage. Detailed landscape plans will be submitted with each additional stage of the school to confirm the specific landscape design.

The Detailed Landscape Plan for Stage 1 identifies the proposed canopy trees, shrubs and ground cover planting around the new Marianist building. This includes seven canopy trees. A planting palette that confirms species, planting stock and size at maturity is also detailed. Outdoor surface and pavements treatments are also detailed.

The Sustainable Design Assessment encourages the retention of existing vegetation and planting of indigenous vegetation as part of development proposals. The report also confirms initiatives for landscaping include improving ecological variety and value at the site through the proposed planting. Additionally, the proposed planting will be water efficient and will not require watering after an initial period when plants are getting established.

3.10. Native Vegetation Removal

An Arboricultural Assessment Report (Appendix H) has been prepared to provide the findings of an assessment of 86 significant tree features on the subject site that may present constraints to future works on the site. The arborist report confirms all trees on site have been planted.

Stage 1 development includes the removal of 5 planted native trees, referred to as T2, T8, T22, T23 and T25 in the report:

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- T2 is large trees in maturity and the area provided is considered insufficient with ongoing long-term damage to the adjacent infrastructure expected.
- T8 is a low-value tree. Low-value trees are not considered worthy of retention.
- T22 requires a local law permit for removal.
- T23 is a moderate-value tree. Moderate trees should generally be retained and protected.
- T25 is considered of low value and the removal of this tree as part of any future works would be considered to be appropriate.

A planning permit is not required for the removal of planted native vegetation.

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4.0 Planning Controls, Policy and Strategic Documents

4.1. Zone

The subject site is located within the GRZ3.

The purpose of the GRZ3 is:

- To implement the Municipal Planning Strategy and the Planning Policy Framework.
- To encourage development that respects the neighbourhood character of the area.
- To encourage a diversity of housing types and housing growth particularly in locations offering good access to services and transport.
- To allow educational, recreational, religious, community and a limited range of other non-residential uses to serve local community needs in appropriate locations.

Sub clause 1 of the schedule also includes neighbourhood character objectives that guide the design of dwellings within the GRZ3.

Under the zoning controls of the GRZ3, a Secondary school is a Section 2 use for which a permit is required. While the use is already permitted on the site, pursuant to Clause 32.08-9, a permit is required to construct a building or construct or carry out works for a use in Section 2.

Clause 32.08-14 identifies the zone to be in Category 3 for signs.

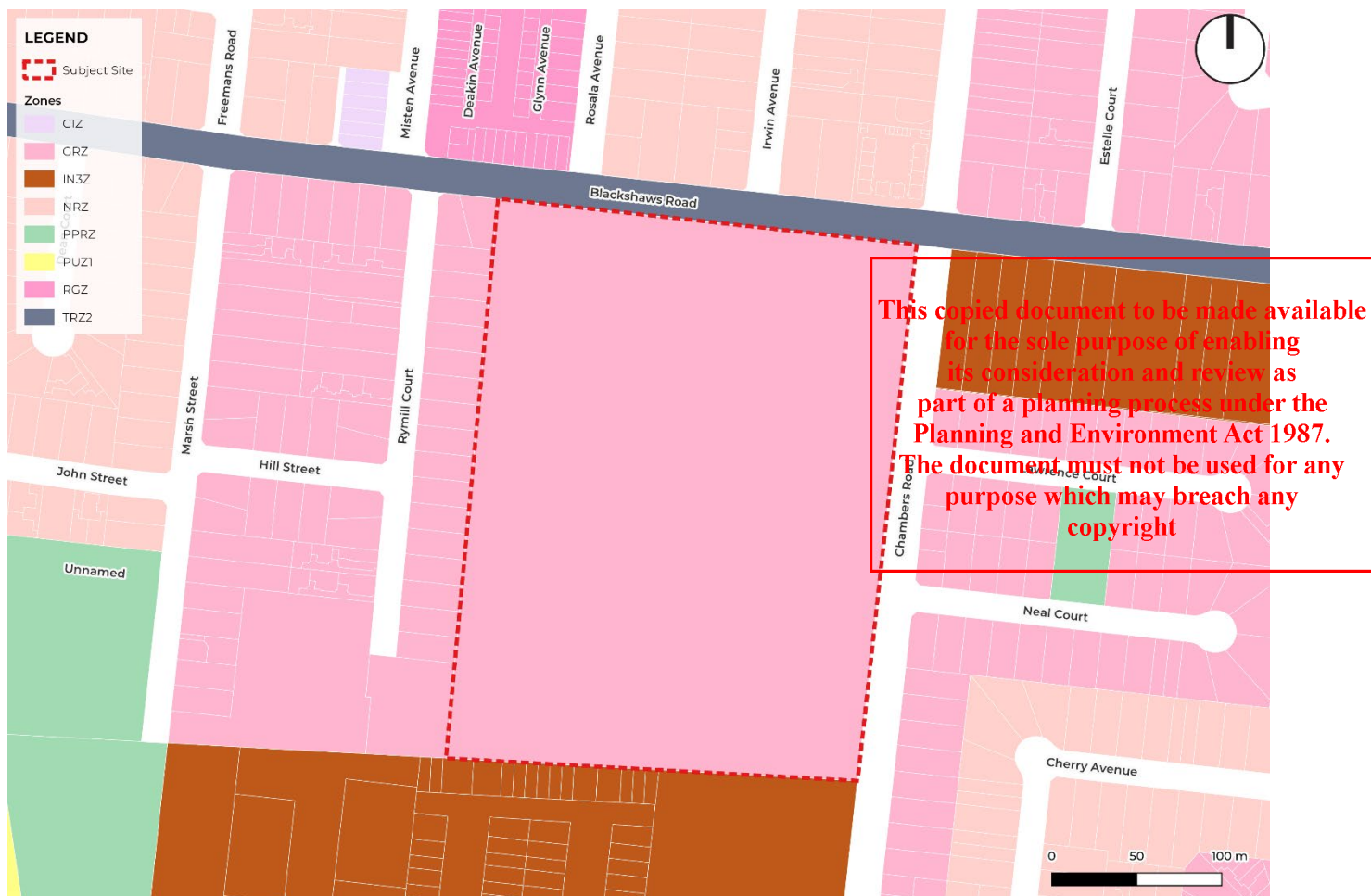


Figure 15 Planning Zones

Source: Ethos Urban, 2023

4.2. Overlays

No overlays are applied to the land.

4.3. Particular Provisions

4.3.1. Clause 52.05: Signs

The Purpose of Clause 52.05 – Signs include:

- *To regulate the development of land for signs and associated structures.*
- *To ensure signs are compatible with the amenity and visual appearance of an area, including the existing or desired future character.*
- *To ensure signs do not contribute to excessive visual clutter or visual disorder.*
- *To ensure that signs do not cause loss of amenity or adversely affect the natural or built environment or the safety, appearance or efficiency of a road.*

Clause 32.08-14 of the GRZ3 identifies the zone to be in Category 3 for signs in high amenity areas. The purpose of this category is *to ensure that signs in high-amenity areas are orderly, of good design and do not detract from the appearance of the building on which a sign is displayed or the surrounding area.*

Pursuant to Clause 52.05-13, a permit is required to erect and display a Business identification sign. No conditions apply.

4.3.2. Clause 52.06: Car Parking

The Purpose of Clause 52.06 – Car Parking include:

- *To ensure that car parking is provided in accordance with the Municipal Planning Strategy and the Planning Policy Framework.*
- *To ensure the provision of an appropriate number of car parking spaces having regard to the demand likely to be generated, the activities on the land and the nature of the locality.*
- *To support sustainable transport alternatives to the motor car.*
- *To promote the efficient use of car parking spaces through the consolidation of car parking facilities.*
- *To ensure that car parking does not adversely affect the amenity of the locality.*
- *To ensure that the design and location of car parking is of a high standard, creates a safe environment for users and enables easy and efficient use.*

Under Clause 52.06-5, the use of Secondary school requires a rate of 1.2 car parking spaces to each employee that is part of the maximum number of employees on the site at any time. This results in 6 additional car parking spaces in order to meet the statutory requirements. The proposed provision of 34 additional spaces exceeds the statutory requirements of this clause.

4.4. Clause 52.17: Native Vegetation

The purpose of Clause 52.17 is:

- *To ensure that there is no net loss to biodiversity as a result of the removal, destruction or lopping of native vegetation. This is achieved by applying the following three step approach in accordance with the Guidelines for the removal, destruction or lopping of native vegetation (Department of Environment, Land, Water and Planning, 2017) (the Guidelines):*
 1. *Avoid the removal, destruction or lopping of native vegetation.*
 2. *Minimise impacts from the removal, destruction or lopping of native vegetation that cannot be avoided.*
 3. *Provide an offset to compensate for the biodiversity impact if a permit is granted to remove, destroy or lop native vegetation.*
- *To manage the removal, destruction or lopping of native vegetation to minimise land and water degradation.*

Pursuant to Clause 52.17-1, a permit is required to remove, destroy or lop native vegetation, including dead native vegetation. However, the table to Clause 52.17-7 specifically states that a permit is not required to remove planted native vegetation. The arborist report submitted in support of the application confirms all trees on site have been planted.

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4.4.1. Clause 52.29: Land Adjacent to the Principal Road Network

The purpose of Clause 52.29 is:

- To ensure appropriate access to the Principal Road Network or land planned to form part of the Principal Road Network.
- To ensure appropriate subdivision of land adjacent to Principal Road Network or land planned to form part of the Principal Road Network.

The masterplan does not propose any alterations to access to access to Blackshaws Road.

4.4.2. Clause 52.34: Bicycle Facilities

The Purpose of Clause 52.34 – Bicycle facilities include:

- To encourage cycling as a mode of transport.
- To provide secure, accessible and convenient bicycle parking spaces and associated shower and change facilities.

Pursuant to Clause 52.34-2, a permit may be granted to vary, reduce or waive any requirement of Clause 52.34-5 and Clause 52.34-6.

Pursuant to Table 1 to Clause 52.34-5 – Bicycle spaces, for the use of Secondary school, 1 bicycle space is required to each 20 employees plus 1 to each 5 pupils. It is expected that the school will grow by approximately 56 students and 5 staff across the campus at the completion of the development. These figures require 11 additional bicycle spaces to be provided at the subject site.

20 additional bicycle parking spaces are proposed to be provided for the Secondary school. This addition will be sufficient to accommodate the expected increase in staff and student numbers.

4.4.3. Clause 53.19: Non-Government Schools

Clause 53.19 – Non-Government Schools applies to an application to use or develop land for a secondary school under any provision of the Planning Scheme other than a VicSmart application.

The Purpose of this clause includes:

- To facilitate new non-government schools.
- To facilitate upgrades and extensions to existing non-government schools.

An application under this clause is exempt from the decision requirements of section 4(1), (2), (3), and (4) and review rights of section 82(1) of the Act.

4.4.4. Clause 53.18 – Stormwater management in urban development

The purpose of this clause is to ensure that stormwater in urban development, including retention and reuse, is managed to mitigate the impacts of stormwater on the environment, property and public safety, and to provide cooling, local habitat and amenity benefits.

An application must be accompanied by details of the proposed stormwater management system, including drainage works and retention, detention and discharges of stormwater to the drainage system.

The stormwater management system must:

- Encourage stormwater management that maximises the retention and reuse of stormwater.
- Encourage development that reduces the impact of stormwater on the drainage system and filters sediment and waste from stormwater prior to discharge from the site.
- Encourage stormwater management that contributes to cooling, local habitat improvements and provision of attractive and enjoyable spaces.
- Ensure that industrial and commercial chemical pollutants and other toxicants do not enter the stormwater system.

The stormwater management system should:

- Meet the current best practice performance objectives for stormwater quality as contained in the Urban Stormwater - Best Practice Environmental Management Guidelines (Victorian Stormwater Committee, 1999).

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- Minimise the impact of chemical pollutants and other toxicants including by, but not limited to, bunding and covering or roofing of storage, loading and work areas.
- Contribute to cooling, improving local habitat and providing attractive and enjoyable spaces.

An application must describe how the site will:

- Protect drainage infrastructure and receiving waters from sedimentation and contamination.
- Protect the site and surrounding area from environmental degradation prior to and during construction of subdivision works.

An application should describe how the site will be managed prior to and during the construction period and may set out requirements for managing:

- Erosion and sediment.
- Stormwater.
- Litter, concrete and other construction wastes.
- Chemical contamination.

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4.5. General Provisions

4.5.1. Clause 65: Decision Guidelines

Clause 65 – Decision Guidelines sets a number of matters to aid the responsible authority in deciding whether the proposal will produce acceptable outcomes.

Clause 65.01 – Approval of an application or plan provides standard decision guidelines for the relevant authority to consider before granting a permit. These are:

- The matters set out in section 60 of the Act.
- Any significant effects the environment, including the contamination of land, may have on the use or development.
- The Municipal Planning Strategy and the Planning Policy Framework.
- The purpose of the zone, overlay or other provision.
- Any matter required to be considered in the zone, overlay or other provision.
- The orderly planning of the area.
- The effect on the environment, human health and amenity of the area.
- The proximity of the land to any public land.
- Factors likely to cause or contribute to land degradation, salinity or reduce water quality.
- Whether the proposed development is designed to maintain or improve the quality of stormwater within and exiting the site.
- The extent and character of native vegetation and the likelihood of its destruction.
- Whether native vegetation is to be or can be protected, planted or allowed to regenerate.
- The degree of flood, erosion or fire hazard associated with the location of the land and the use, development or management of the land so as to minimise any such hazard.
- The adequacy of loading and unloading facilities and any associated amenity, traffic flow and road safety impacts.
- The impact the use or development will have on the current and future development and operation of the transport system.

4.6. Policy Context

The Planning Policy Framework provides the broad objectives and strategies for planning in Victoria. The PPF includes local policies prepared by Council to apply high level State policies to a local context, responding to specific requirements and features of the municipality.

Key provisions of the framework relevant to this application are:

- **Clause 02- - Municipal Planning Strategy**
 - Clause 02.03 – Strategic Directions

- Clause 02.03-5 – Built environment and heritage
- Clause 02.03-8 – Transport
- Clause 02.03-9 – Infrastructure
- **Clause 11 – Settlement**
 - Clause 11.01-1S – Settlement
 - Clause 11.01-1R – Settlement - Metropolitan Melbourne
 - Clause 11.03-1S – Activity centres
 - Clause 11.03-1R – Activity centres - Metropolitan Melbourne
- **Clause 13 – Environmental Risks and Amenity**
 - Clause 13.05-1S – Noise management
- **Clause 15 – Built Environment and Heritage**
 - Clause 15.01-1S – Urban design
 - Clause 15.01-1L-02 – Landscape design and canopy tree cover
 - Clause 15.01-1L-03 – Signs
 - Clause 15.01-2S – Building design
 - Clause 15.01-2L-04 – Environmentally sustainable development
 - Clause 15.01-5S – Neighbourhood character
 - Clause 15.01-5L – Hobsons Bay preferred neighbourhood character
- **Clause 17 – Economic Development**
 - Clause 17.01-1S – Diversified economy
- **Clause 18 – Transport**
 - Clause 18.01-1S – Land use and transport integration
 - Clause 18.01-3S – Sustainable and safe transport
 - Clause 18.01-3R - Sustainable and safe transport - Metropolitan Melbourne
 - Clause 18.01-3L - Sustainable personal transport
 - Clause 18.02-2S – Cycling
- **Clause 19 – Infrastructure**
 - Clause 19.02-2S – Education facilities

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A review of key objectives and strategies for each clause as applicable to this proposal are detailed in Appendix A.

4.7. Strategic Documents

4.7.1. Plan Melbourne 2017-2050

Plan Melbourne 2017-2050 is Victoria's state government metropolitan planning strategy. The strategy will guide the growth of Melbourne for the period. It sets the strategy for supporting jobs, housing and transport.

The strategy defines Major Activity Centres as *suburban centres that provide access to a wide range of goods and services. They have different attributes and provide different functions, with some serving larger subregional catchments.*

Hobsons Bay is within the Western Region. The plan identifies education as one of the key industries in the region, where employment growth is projected at 2.3% per annum between 2015-2031.

This is particularly relevant for the site given its location in close proximity to Altona Gate Major Activity Centre.

4.7.2. Hobsons Bay Activity Centres Strategy (2019-36)

The Hobsons Bay Activity Centres Strategy 2019-36 sets out a framework to inform planning, economic development and decision-making relating to activity centres in Hobsons Bay.

The strategy various different hierarchies of activity centres throughout the municipality. Relevant to the proposal, the subject site is located in the edge of or in close proximity to the following activity centres:

- The Blackshaws Road/Millers Road Medium Neighbourhood Activity Centre is located just opposite Chambers Road to the east.

- The Misten Avenue/Blackshaws Road Micro Centre 65m north-west of the subject site opposite Blackshaws Road.
- The Altona Gate Major Activity Centre is located 500m north-east of the site.

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These categories are defined as follows:

- *Major Activity Centres provide a wide range of goods and services. They have different attributes and provide different functions, with some serving larger subregional catchments. The same three centres identified in Plan Melbourne are identified in the Hobsons Bay network as Major Activity Centres.*
- *Neighbourhood Activity Centres are local centres that provide access to local goods, services and employment opportunities and serve the needs of the surrounding community. Medium Neighbourhood Centres play an important role in meeting the basic needs of surrounding residents, but have a relatively limited range of services.*
- *Micro Centres typically comprise a few shops providing the simplest of convenience shopping and other needs. They can be important in providing basic goods and services within a comfortable walking distance.*

While the site is not located within one of the activity centres, its edge location makes the following directions and associated strategies relevant to the proposal and the subject site:

- *Ensure activity centres are supported by community services and infrastructure*
 - *Advocate for and consolidate government, community, cultural and recreational facilities and uses within activity centres to improve accessibility for local people.*
- *Improve accessibility to, within and between activity centres using sustainable transport options*
 - *Support and advocate for the provision of end-of-trip facilities such as bicycle parking, lockers and change areas in activity centres and at public transport hubs to support walking and cycling.*
 - *Ensure new developments in activity centres have linkages to public transport and other key sites.*
- *Improve amenity and environmental sustainability through highquality innovative urban design and development outcomes*
 - *Encourage best-practice Environmentally Sustainable Development (ESD) in new development and redevelopments.*
- *Have regard to place-based policies and actions*
 - *Support and reinforce the primacy of Altona (Altona Beach), Altona Gate (Altona North) and Williamstown as MACs through whole of council strategies and actions that encourage growth and enhancement of a wide range of activities, facilities and services and employment.*
 - *Encourage appropriate development that will increase residential densities in MACs.*
 - *Where required, advocate for improvements to public transport infrastructure and services to reflect the importance and role of MACs.*
 - *support and augment the role of NACs (Small, Medium and Large NACs) for local convenience shopping and, subject to size and identity, additional uses such as small offices, service businesses and community services*
 - *plan, provide and advocate for a high quality active travel (walking and cycling) network within an appropriate distance for relevant Medium and Small NACs*
 - *ensure that development in neighbourhood centres meets appropriately high standards of urban design and community amenity so that the visual and functional environment of centres is improved, and they become vibrant focal points for local communities*
 - *Consider additional opportunities to increase housing and the mix of uses in Blackshaws Road/Millers Road (Medium NAC).*

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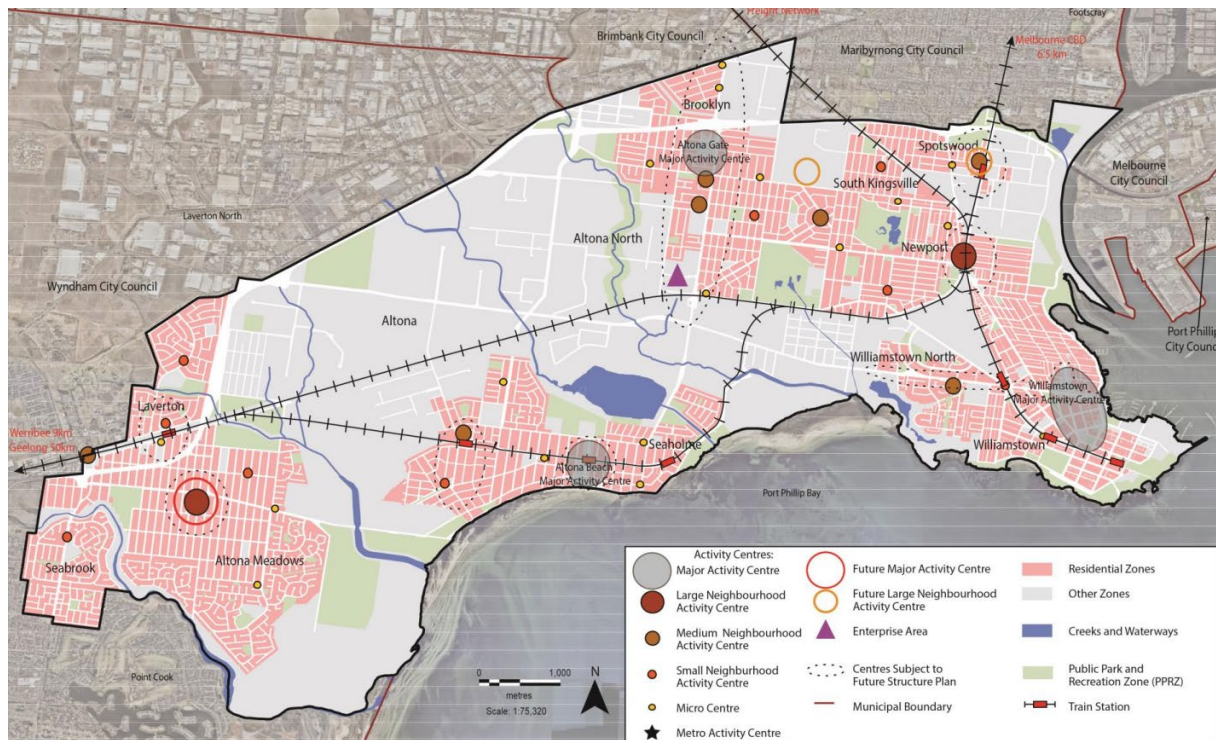


Figure 16 *Hobsons Bay Local Activity Centres Strategy*

Source: Hobsons Bay Activity Centres Strategy, 2019

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5.0 Pre-Application Feedback

A pre-application meeting took place on 27 April 2023 between the project team and Hobsons Bay City Council officers. Council was generally supportive of the proposal and identified minor issues for further consideration and the relevant application requirements. These are outlined in the following table, along with actions undertaken by the project team to respond to the issues raised by Council:

Table 3 *Pre-application meeting advice*

Council Feedback	Response
We are well above the statutory car parking provision by providing 138 car parking spaces at the site	Noted
There is a high pressure gas line on Chamber Road. Service provider will have to be notified.	Noted.
Building in stage five will have to consider any overshadowing impacts if double storey.	Noted. Stage 5 building is proposed to be single-storey.
New crossing locations should avoid street trees where possible.	Noted for subsequent stages.
Proposal will need to address CI 53.18 of the Hobsons Bay scheme (stormwater).	Noted. Please refer to Sustainable Design Assessment at Appendix F.
The following application requirements are relevant to the proposal: <ul style="list-style-type: none"> • Certificate of Title • Planning Report • Traffic Impact Assessment • Sustainability Management Plan - must include an assessment against the requirements of Clause 53.18 (Stormwater Management in Urban Development). • Green Travel Plan • Arborist Report 	Noted. All of these items have been prepared in support of the application.

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6.0 Planning Assessment

6.1 Planning Policy Framework

The proposed new Marianist building as part of Stage 1 and the subsequent stages of the masterplan at Emmanuel College, St Paul's Campus are supported by the relevant State and Local Planning Policy Framework.

- The proposed development will enable Emmanuel College to continue to provide for the educational needs of the local community in an integrated and orderly manner, consistent with Clauses 02.03-9, 11.01-1S, 11.01-1R, 11.03-1S, 11.03-1R, 15.01-1S and 19.02-2S in the Hobsons Bay Planning Scheme.
- The proposed masterplan represents an orderly and sequential development that will improve the facilities of the school and enable it to service the growing local community consistent with Clauses 02.03-9, 11.01-1S, 11.01-1R, 11.03-1S, 11.03-1R, 15.01-1S and 19.02-2S.
- The school is highly accessible due to its close proximity to various hierarchies of activity centres and arterial roads, consistent with Clause 11.01-1S, Clause 11.01-1R, Clause 11.03-1S, Clause 11.03-1R and Clause 15.01-1S.
- The school's location in proximity to various activity centres and main roads will provide convenient access for the community by relying on the existing and planned public transport network. The school will also provide sufficient bicycle facilities consistent with the relevant provisions, minimising car use and improving air quality in Altona North, consistent with Clause 11.01-1S, Clause 11.02-2S, Clause 11.03-1S, Clause 13.06-1S, Clause 15.01-1S, Clause 15.01-2L-01, Clause 17.01-1R, Clause 18.01-1S, Clause 18.01-3S, Clause 18.02-2S and Clause 19.02-2S..
- A Traffic Impact Assessment (Appendix E) and a Green Travel Plan (Appendix F) have been undertaken which confirm that the traffic and road safety arrangements, car parking and bicycle facilities provided will be able to support the full operation of the school. School buses are expected to be the primary mode of transport to and from the site for students. The anticipated increase in traffic is considered to be negligible and will have a low impact on the operation of the surrounding road network. This is consistent with Clause 02.03-8, Clause 18.01-1S, 18.01-3S, Clause 18.02-2S, Clause 18.01-3R, Clause 18.01-3L, Clause 18.02-2S and Clause 19.02-2S.
- The Green Travel Plan recommends incorporating sustainable travel features such as the provision of bicycle parking facilities above the statutory requirements, upgrades to pedestrian site access to facilitate active transport alternatives to and from school, and identification of public transport information at the entry and reception to demonstrate ease of access to the campus. These strategies are consistent with Clause 02.03-8, Clause 11.01-1S, Clause 11.01-1R, Clause 15.01-2S, Clause 18.01-1S, Clause 18.01-3S, Clause 18.01-3R, Clause 18.01-3L and Clause 19.02-2S.
- The proposal has been designed to respect its surroundings given its location within a residential zone, while acknowledging that school facilities have a distinctive built form in terms of massing, height and scale. This is consistent with Clause 02.03-5, Clause 15.01-2S, Clause 15.01-5S, Clause 15.01-5L and Clause 19.02-2S. The town planning drawings for Stage 1 include details on the materiality, finishes and landscape treatment that will provide a high-quality contribution to the precinct.
- The proposal includes the planting of canopy trees and design of landscaped areas to enhance the amenity and attractiveness of the campus, and provides additional visual amenity to the surrounding residential properties to the west, consistent with Clause 11.02-2S, Clause 13.05-1S, Clause 15.01-1L-01 and Clause 15.01-1L-03.
- The proposed signage associated to the use of the building will consist of 2 Business identification signs that will not be visible from all frontages which will be incorporated into the building façade, minimising clutter. The signs will have no detrimental impacts on surrounding amenity, consistent with Clause 02.03-5, Clause 15.01-1S and Clause 15.01-1L-03.
- The Sustainable Design Assessment includes initiatives for energy performance, integrated water management, indoor environment quality, transport (supported also by the Green Travel Plan), waste management and urban ecology, consistent with Clause 15.01-2L-04.
- Sustainable design principles such as building orientation, stormwater management, water and energy efficiency have also been incorporated to the design the building. This is consistent with Clause 02.03-5, Clause 15.01-2S, Clause 15.01-2L-04 and Clause 15.02-1S.
- The Sustainable Design Assessment encourages the retention of existing vegetation, planting of indigenous vegetation as part of development proposals, improvement of ecological value on-site and incorporation of water-efficient planting, consistent with Clause 15.01-1L-02.

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6.2. Buildings and works

The proposed Stage 1 development and subsequent stages of the masterplan at the existing campus are consistent with the purpose, objectives and decision guidelines of the GRZ3, as follows:

- The proposed Stage 1 development and subsequent stages of the masterplan at the existing campus are consistent with the purpose, objectives and decision guidelines of the GRZ3 which include the delivery of educational and community uses that support the needs of the local community. Specifically, the proposal responds to the GRZ3 as follows:
 - The proposal respects the neighbourhood character of the area.
 - The setbacks will provide opportunities for landscape buffers around the school buildings, providing visual and acoustic separation from the adjacent residential uses.
 - The adjacent residential properties will not be impacted by overshadowing from the proposed development.
 - The proposal respects the maximum height for dwellings and residential buildings of 11m and 3 storeys.
- The scale and intensity of the development are compatible with adjoining residential uses. The traffic report confirmed that the anticipated increase in traffic is considered to be negligible and will have a low impact on the operation of the surrounding road network.
- The design of the new Marianist building is of high-quality and provides a highly articulated façade.
- Stage 1 development will have minimum visibility from the street.

6.3. Signage

The proposed signage appropriately responds to the relevant decision guidelines of Clause 52.05-8:

- The signs will not impact on any views and vistas from the public realm due to the location of the building set back at least 100m from Blackshaws Road.
- The proposed signs are non-illuminated.
- The signs are consistent with the character of the area and existing pattern of signage and with which can be reasonably expected at a school.
- The proportion, scale and form of the proposed sign are appropriate to that of the host building.
- The signs will not detract from the appearance of the building in which they will be displayed or the surrounding area.
- The erection of the signs does not require the removal of any vegetation.
- The signs will be mounted to the building and any structures will be integrated to the building and its surrounds.
- The signs will not be illuminated.
- The sign appropriately identifies the use of the building.
- The signs will not impact on the road network or road safety.

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6.4. Access, Car Parking and Bicycle Facilities

Access to the school will remain unchanged.

The increase in 34 car parking spaces will exceed the statutory parking requirement for the expected increase in 5 staff at any one time as a result of the growth of the school to 2041

The proposal will increase the number of bicycle parking spaces by 20 additional spaces, which exceeds the statutory bicycle parking requirement of Clause 52.34 for an increase of 56 students and 5 staff at any one time.

6.5. Waste Management

The existing waste management arrangements are sufficient to support the increase in student and staff numbers and as such an updated Waste Management Plan has not been provided as part of this application.

6.6. Clause 65.01

The proposal is consistent with Decision Guidelines at Clause 65.01 as:

- The proposal is consistent with the Hobsons Bay Planning Scheme and its relevant policies and objectives, as demonstrated above.
- The proposal is consistent with the purpose of the GRZ3.
- The proposal is consistent with the orderly planning of the area.
- The proposal will not have any significant effects on the environment, human health and amenity of the area.
- The proposal does not impact on the adjacent public land part of the Blackshaws Road reserve.
- The proposal is unlikely to cause or contribute to land degradation, salinity or reduce water quality.
- The ESD initiatives will improve the quality of stormwater at the site.
- The ESD encourage the planting of indigenous vegetation.
- The development will not result in an increase in flood, erosion or fire hazard.
- Waste collection vehicle access and loading areas will be upgraded as part of Stage 1 development.
- The traffic report confirms that the proposal will not have a detrimental effect on the operation of the transport systems.

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7.0 Conclusion

As discussed throughout this report, the development satisfactorily responds to the relevant sections of the Hobsons Bay Planning Scheme and should be supported as:

- The proposed new Marianist building as part of Stage 1 and the subsequent stages of the masterplan at Emmanuel College, St Paul's Campus are supported by the relevant State and Local Planning Policy Framework.
- The proposed Stage 1 development and subsequent stages of the masterplan at the existing campus are consistent with the purpose, objectives and decision guidelines of the GRZ3. Whilst most zoning requirements in GRZ3 relate to dwellings and do not directly apply to development at school sites, the proposal adopts relevant controls and uses them to guide development neighbouring residential properties.
- The scale and intensity of the development are compatible with adjoining residential uses. The traffic report confirms the anticipated increase in traffic is considered to be negligible and will have a low impact on the operation of the surrounding road network.
- The design of the new Marianist building is of high-quality and provides an interesting and well-articulated façade.
- The proposed signage appropriately responds to the relevant decision guidelines of Clause 52.05-8. The signs appropriately identify the use of the building.. The signs are consistent with that which can be reasonably expected at a school.
- The development accounts for the provision of additional car parking spaces and bicycle facilities associated with the expected growth at the school.
- The existing waste management arrangements are sufficient to support the increase in student and staff numbers.
- The proposal is consistent with Decision Guidelines at Clause 65.01.

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Appendix A - Planning Policy Framework Review

Clause 02 – Municipal Planning Strategy

Clause 02 – Municipal Planning Strategy sets out the vision and strategic direction for the municipality.

Clause 02.03 – Strategic Directions states Council's aims on the relevant planning matters outlined below.

Clause 02.03-5 – Built environment and heritage

- Regarding Building and urban design, Council seeks to:
 - *Achieve building and urban design outcomes that contribute positively to the local context and enhance the public realm, including in industrial areas.*
 - *Improve the amenity, design, safety and environmental performance of industrial areas.*
- Regarding Signs, Council seeks to:
 - *Support signs that are respectful of and complementary to the character and amenity of sensitive areas, including interfaces, and do not detract from the character of a locality, building or site.*
- Regarding Neighbourhood character, Council seeks to:
 - *Encourage development that respects preferred neighbourhood character.*
 - *Support change and growth while protecting and enhancing local senses of place and identity through built form and design.*
- Regarding Environmentally sustainable development, Council seeks to:
 - Incorporate environmentally sustainable design elements at the time of planning approval to assist in achieving environmentally sustainable development. This approach seeks to:
 - Improve outcomes that may otherwise be compromised if these matters are only considered as part of a building approval.
 - Reduce difficulties or extra costs associated with retro-fitting the development.

Clause 02.03-8 – Transport

- Council seeks to:
 - *Support emerging transport modes and technologies that complement existing travel methods.*
 - *Improve local area traffic and car parking management within and around activity centres.*
 - *Improve accessibility to and within activity centres by supporting the development of sustainable transport infrastructure and networks.*

Clause 02.03-9 – Infrastructure

- Regarding Community infrastructure, Council seeks to:
 - *Support the provision of a range of community facilities and services that meet the needs of both the existing and future community.*

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Clause 11 – Settlement

Clause 11 (Settlement) advocates for planning to anticipate and respond to the needs of existing and future communities through the provision of zoned and serviced land for housing, employment, recreation and open space, commercial and community facilities and infrastructure, recognise the need for a contribute towards health, wellbeing and safety, diversity of choice, adaptation in response to changing technology, economic viability, a high standard of urban design and amenity, energy efficiency, prevention of pollution to land, water and air, protection of environmentally sensitive areas and natural resources, accessibility and land use and transport integration.

Clause 11 also advocates for planning to prevent environmental problems created by siting incompatible land uses close together and facilitate sustainable development that takes full advantage of existing settlement patterns, and investment in transport and communication, water and sewerage and social facilities.

Clause 11.01-1S – Settlement seeks to *facilitate the sustainable growth and development of Victoria and deliver choice and opportunity for all Victorians through a network of settlements.*

Strategies to achieve this objective of relevance to the subject site include:

- *Develop sustainable communities through a settlement framework offering convenient access to jobs, services, infrastructure and community facilities.*
- *Plan for development and investment opportunities along existing and planned transport infrastructure.*
- *Deliver networks of high-quality integrated settlements that have a strong identity and sense of place, are prosperous and are sustainable by balancing strategic objectives to achieve improved land use and development outcomes at a regional, catchment and local level.*
- *Develop compact urban areas that are based around existing or planned activity centres to maximise accessibility to facilities and services.*
- *Ensure retail, office-based employment, community facilities and services are concentrated in central locations.*

One of the strategies in **Clause 11.01-1R – Settlement - Metropolitan Melbourne** is to *create mixed-use neighbourhoods at varying densities, including through the development of urban-renewal precincts, that offer more choice in housing, create jobs and opportunities for local businesses and deliver better access to services and facilities.*

Clause 11.03-1S – Activity centres seeks to *encourage the concentration of major retail, residential, commercial, entertainment and cultural developments into activity centres that are highly accessible to the community.*

Strategies to achieve this objective of relevance to the subject site include:

- *Support the continued growth and diversification of activity centres to give communities access to a wide range of goods and services, provide local employment and support local economies.*

Relevant strategies from **Clause 11.03-1R – Activity centres - Metropolitan Melbourne** include to:

- *Locate significant new education, justice, community, administrative and health facilities that attract users from large geographic areas in or on the edge of Metropolitan Activity Centres or Major Activity Centres with good public transport.*
- *Locate new small scale education, health and community facilities that meet local needs in or around Neighbourhood Activity Centres.*

Clause 13 – Environmental Risks and Amenity

Clause 13 – Environmental Risks and Amenity advocates for planning to *identify, prevent and minimise the risk of harm to human health and amenity.*

Clause 13.05-1S – Noise management aims to *assist the management of noise effects on human health and amenity.*

Relevant strategies include to:

- *Ensure that development is not prejudiced and community amenity and human health is not adversely impacted by noise emissions.*
- *Minimise the impact on human health from noise exposure to occupants of sensitive land uses (residential use, child care centre, school, education centre, residential aged care centre or hospital) near the transport system and other noise emission sources through suitable building siting and design (including orientation and internal layout), urban design and land use separation techniques as appropriate to the land use functions and character of the area.*

Clause 15 – Built Environment and Heritage

Clause 15 addresses the design of the built environment and includes objectives of ensuring land use and development responds to its landscape, character, valued built form and cultural context, and delivering functional, accessible, safe and diverse physical and social environments through the appropriate location of use and development and through high quality buildings and urban design.

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Clause 15.01-1S – Urban design aims to create urban environments that are safe, healthy, functional and enjoyable and that contribute to a sense of place and cultural identity.

Relevant strategies include to:

- *Require development to respond to its context in terms of character, cultural identity, natural features, surrounding landscape and climate.*
- *Ensure development contributes to community and cultural life by improving the quality of living and working environments, facilitating accessibility and providing for inclusiveness.*
- *Ensure the interface between the private and public realm protects and enhances personal safety.*
- *Ensure development supports public realm amenity and safe access to walking and cycling environments and public transport.*
- *Ensure that the design and location of publicly accessible private spaces, including car parking areas, forecourts and walkways, is of a high standard, creates a safe environment for users and enables easy and efficient use.*
- *Ensure that development provides landscaping that supports the amenity, attractiveness and safety of the public realm.*
- *Ensure that development, including signs, minimises detrimental impacts on amenity, on the natural and built environment and on the safety and efficiency of roads.*
- *Promote good urban design along and abutting transport corridors.*

Relevant strategies in **Clause 15.01-1L-02 – Landscape design and canopy tree cover** include:

- *Increase landscape canopy cover to mitigate climate change and enhance the visual appearance and amenity of front setbacks, carparks and other elements that interface with the public realm.*
- *Provide landscaping that:*
 - *Enhances the appearance of key focal points, such as gateways, major boulevards, creeks, open spaces and nature conservation areas.*
 - *Introduces a wider range of species, particularly trees to promote climate, pest and disease resilience.*
 - *Uses indigenous vegetation in natural areas.*
 - *Minimises the need for watering and maintenance.*
- *Integrates with all existing planting.*
- *Support the use of species that provide shade in summer and natural light to the built form in winter.*
- *Support locating all service infrastructure underground to maximise the opportunity to retain and plant canopy trees.*

Clause 15.01-1L-03 – Signs seeks to ensure signs do not visually dominate a particular locality, building or site nor detract from its character and to ensure signs do not visually dominate a particular locality, building or site nor detract from its character.

Relevant strategies include to:

- *Where more than one business sign is proposed, plan and design signs as part of a comprehensive scheme to ensure all signs are consistent and complementary with each other in size, location, materiality, style and colour.*
- *Design and scale signs not to visually dominate the building to which they are fixed nor the property in which they are located.*
- *Design and locate signs to complement the period and style of the building to which they are fixed, including through materiality, fonts and paint colours.*
- *Design signs to be only large enough for effective identification of the business or promotions of goods, services, event or other matter it relates to.*
- *Design signs to be only large enough for effective identification of the business or promotions of goods, services, event or other matter it relates to.*
- *Avoid the use of bunting, flags, banners and V-signs.*

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Clause 15.01-2S – Building design seeks to achieve building design outcomes that contribute positively to the local context and enhance the public realm. Relevant strategies include:

- Ensure development responds and contributes to the strategic and cultural context of its location.
- Minimise the detrimental impact of development on neighbouring properties, the public realm and the natural environment.
- Improve the energy performance of buildings through siting and design measures that encourage:
 - Passive design responses that minimise the need for heating, cooling and lighting.
 - On-site renewable energy generation and storage technology.
 - Use of low embodied energy materials.
- Encourage water efficiency and the use of rainwater, stormwater and recycled water.
- Minimise stormwater discharge through site layout and landscaping measures that support on-site infiltration and stormwater reuse.
- Ensure the form, scale, and appearance of development enhances the function and amenity of the public realm.
- Ensure development considers and responds to transport movement networks and provides safe access and egress for pedestrians, cyclists and vehicles.
- Encourage development to retain existing vegetation.
- Ensure development provides landscaping that responds to its site context, enhances the built form, creates safe and attractive spaces and supports cooling and greening of urban areas.

Clause 15.01-2L-04 – Environmentally sustainable development seeks to achieve best practice in environmentally sustainable development from the design stage through to construction and operation.

Relevant strategies include to:

- Facilitate development that minimises environmental impacts.
- Encourage environmentally sustainable development that:
 - Is consistent with the type and scale of development.
 - Responds to site opportunities and constraints.
 - Adopts best practice through a combination of methods, processes and locally available technology that demonstrably minimises environmental impacts.

Energy performance:

- Reduce both energy use and energy peak demand through design measures such as:
 - Building orientation.
 - Shading to glazed surfaces.
 - Optimising glazing to exposed surfaces.
 - Inclusion of or space allocation for renewable technologies.

Integrated water management

- Reduce total operating potable water use through appropriate design measures such as water efficient fixtures, appliances, equipment, irrigation and landscaping.
- Encourage the appropriate use of alternative water sources (including greywater, rainwater and stormwater).
- Incorporate best practice water sensitive urban design to improve the quality of stormwater runoff and reduce impacts on water systems and water bodies.

Indoor environment quality

- Achieve a healthy indoor environment quality, including thermal comfort and access to fresh air and daylight, prioritising passive design over mechanical heating, ventilation, cooling and lighting.
- Reduce indoor air pollutants by encouraging use of low-toxicity materials.
- Minimise noise levels and noise transfer within and between buildings and associated external areas.

Transport

- Design development to promote the use of walking, cycling and public transport, in that order; and minimise car dependency.
- Promote the use of low emissions vehicle technologies and supporting infrastructure.

Waste management

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- Promote waste avoidance, reuse and recycling during the design, construction and operation stages of development.
- Encourage use of durable and reuseable building materials.
- Ensure sufficient space is allocated for future change in waste management needs, including (where possible) composting and green waste facilities.

Urban ecology

- Protect and enhance biodiversity by incorporating natural habitats and planting indigenous vegetation.
- Reduce urban heat island effects through building design, landscape design, water sensitive urban design and the retention and provision of canopy and significant trees.
- Encourage the provision of space for productive gardens, particularly in larger residential developments.

Under this clause, a Sustainability Management Plan and a Green Travel Plan are required for a non-residential building with a gross floor area of more than 1000 square metres or for an extension to an existing non-residential building creating more than 1000 square metres of additional gross floor area (excluding outbuildings).

Clause 15.01-5S – Neighbourhood character seeks to recognise, support and protect neighbourhood character, cultural identity, and sense of place.

Relevant strategies include to:

- Support development that respects the existing neighbourhood character or contributes to a preferred neighbourhood character.
- Ensure development responds to its context and reinforces a sense of place and the valued features and characteristics of the local environment and place by respecting the:
 - Pattern of local urban structure and subdivision.
 - Underlying natural landscape character and significant vegetation.
 - Neighbourhood character values and built form that reflect community identity.

Clause 15.01-5L – Hobsons Bay preferred neighbourhood character seeks to support development of three-storeys in moderate change areas, and two-storeys in incremental and minimal change areas, consistent with the preferred neighbourhood character and housing outcomes.

Relevant strategies include to:

- Respect the garden settings and vegetation character.
- Maintain the openness of streetscapes and the visual connection between buildings and the street through the use of low or transparent fences.
- Design development to maintain the horizontality of buildings in the streetscape and the dominance of roof structures where present, by retaining larger exposed roof forms over front porches, matching ground level, wall heights, and minimising composition and detailing that would give the appearance of narrow, vertical forms.
- Maintain the rhythm and spacing of the streetscape by providing setbacks from side boundaries for new buildings.
- Minimise the dominance of car parking facilities, driveways and crossovers and ensure they occupy a minor proportion of the building frontage.
- Provide vehicular access from a rear laneway or rear access driveway where available and minimise the number of vehicular crossovers.
- Minimise paving and other hard surfaces in front garden areas, including driveways and crossovers.
- Retain space in front and rear setbacks for substantial planting and design these areas to:
 - Include landscaping along side boundaries and driveways, where in the front setback.
 - Accommodate canopy trees.
- Support subdivision and site layout that retains large, established trees and provides for the planting of new trees to minimise visual bulk.
- Design developments to allow vegetation to reach maturity and to be maintained without threatening buildings, services, access and infrastructure in the longer term.
- Use building materials and finishes that complement the dominant pattern within the streetscape.

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Clause 17 – Economic Development

Clause 17 – Economic Development advocates for planning to *provide for a strong and innovative economy, where all sectors are critical to economic prosperity.*

Clause 17.01-1S – Diversified economy aims to *strengthen and diversify the economy.*

Relevant strategies include to:

- *Facilitate growth in a range of employment sectors, including health, education, retail, tourism, knowledge industries and professional and technical services based on the emerging and existing strengths of each region.*
- *Improve access to jobs closer to where people live.*

Clause 18 – Transport

Clause 18 – Transport advocates for planning to *ensure a safe, integrated and sustainable transport system.*

Clause 18.01-1S – Land use and transport integration aims to *facilitate an environmentally sustainable transport system that is safe and supports health and wellbeing.*

Relevant strategies include to:

- *Plan the use of land adjacent to the transport system having regard to the current and future development and operation of the transport system.*
- *Design neighbourhoods to:*
 - *Better support active living.*
 - *Increase the share of trips made using sustainable transport modes.*

Clause 18.01-3S – Sustainable and safe transport aims to *facilitate an environmentally sustainable transport system that is safe and supports health and wellbeing.*

Relevant strategies include to:

- *Support forms of transport and energy use that have the greatest benefit for, and least negative impact on, health and wellbeing.*
- *Design development to promote walking, cycling and the use of public transport in that order, and minimise car dependency.*

A relevant strategy from **Clause 18.01-3R - Sustainable and safe transport - Metropolitan Melbourne** is to *improve local travel options for walking and cycling to support 20 minute neighbourhoods.*

A relevant strategy from **Clause 18.01-3L - Sustainable personal transport** is to *provide end-of-trip facilities such as bicycle parking, lockers and change areas in activity centres, train stations and public transport hubs.*

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Clause 18.02-2S – Cycling aims to *facilitate an efficient and safe bicycle network and increase the proportion of trips made by cycling.*

A relevant strategy is to:

- *Support increased cycling by providing cycle parking and related end of trip facilities to meet demand at education, recreation, transport, shopping, commercial, public transport interchanges and community facilities, significant trip generating developments and other major attractions.*

Clause 19 – Infrastructure

Clause 19 – Infrastructure addresses the provision of social and physical infrastructure and advocates for planning to *recognise social needs by providing land for a range of accessible community resources, such as education.*

Clause 19.02-2S – Education facilities seeks to assist the integration of education and early childhood facilities with local and regional communities. Relevant strategies to achieve are to:

- *Consider demographic trends, existing and future demand requirements and the integration of facilities into communities in planning for the location of education and early childhood facilities.*
- *Ensure childcare, kindergarten and primary school and secondary school facilities provide safe vehicular drop-off zones.*
- *Facilitate the establishment and expansion of primary and secondary education facilities to meet the existing and future education needs of communities.*
- *Recognise that primary and secondary education facilities are different to dwellings in their purpose and function and can have different built form (including height, scale and mass).*
- *Locate secondary school and tertiary education facilities in designated education precincts and areas that are highly accessible to public transport.*
- *Ensure streets and accessways adjoining education and early childhood facilities are designed to encourage safe bicycle and pedestrian access.*
- *Consider the existing and future transport network and transport connectivity.*

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Appendix B - Certificate of Title

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Appendix C - Masterplan and Development Plans

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Appendix D - Landscape Masterplan and Detailed Landscape Plans

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Appendix E - Traffic Impact Assessment

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Appendix F - Green Travel Plan

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Appendix G - Sustainable Design Assessment

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Appendix H – Stormwater Management Plan

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Appendix I Waste Management Supporting Letter

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