

15 August 2024

Planning Report

Emmaus Christian College – Proposed
Expansion

487-503 Springvale Road, Vermont South

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Planning Report

15 August 2024

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Prepared for

Emmaus College

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22MEL-0026

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Document history and status

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https://urpsau.sharepoint.com/sites/MelbourneWork/Shared Documents/Synergy/Projects/22MEL/22MEL-0026 - 503 Springvale Road, Vermont South - Emmaus College Masterplan/Issued/240627_RFISubmission/Superseded/Emmaus College Town Planning Report V3.docx

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1. Introduction

URPS have been engaged by Emmaus College to seek planning approval for a staged development at its main campus at 487-503 Springvale Road, Vermont South (the subject land).

This application is required to facilitate the development to accommodate the evolving needs of Emmaus College. The staged development will comprise the construction of a new administration building and chapel, with associated landscaping and civil works. It is also proposed to modify traffic and bus movements into and around the site, to reduce the impact of traffic on adjoining residential street, and to improve safety within the site for students and staff.

The following drawings and documentation are provided in conjunction with this report:

- Site and Architectural Plans, prepared by Branch Studio Architects,
- School Masterplan, prepared by Branch Studio Architects,
- Traffic Impact Assessment, prepared by Stantec,
- Civil Drawings and Landscape Plans, prepared by Orchard Design,
- Arboricultural Assessment & Tree Management Report, prepared by Arbor Advocacy, and
- A Metropolitan Planning Levy Certificate.

A copy of title for the subject land is included as an attachment to this report

Applicant	Emmaus College C/- URPS
Property Location	487-503 Springvale Road, Vermont South (Emmaus College)
Proposal	Staged development for buildings and works, car parking variation
Zone	Special Use Zone – Schedule 1 (SUZ1)
Overlays	Development Contributions Overlay – Schedule 1 (DCPO1)
Permit Triggers	<ul style="list-style-type: none">• Pursuant to Clause 37.01-4 (SUZ1), a permit is required to construct a building or construct or carry out works

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2. Subject Site and Locality

2.1 Subject Site

The subject site is located at 487-503 Springvale, Road Vermont South and functions as a non-government secondary school. The land is made up of two lots and is formally described as Lot 235 on LP88921 and Lot 1 on TP333893.

Emmaus College Vermont Campus was established in 1980 and is a co-educational facility that caters to students from Year 7-8 and Years 10-12

The site is located to the east of Springvale Road and is generally rectangular in shape. The primary portion of the site extends from Springvale Road to Stanley Road and has a length of approximately 529.3 metres. The width of the site is 151 metres, and the total site area is approximately 79,800 square metres. An additional parcel is annexed to the south of the site and fronts Hawthorn Road.



Figure 1. Subject site (Source: searchcomm.com)

There are three current vehicle ingress/egress points to the site:

- Hawthorn Road: 16.5-metre-wide crossover. Facilitates ingress/egress. Hawthorn road operates as the main entrance and is burdened by the highest number of traffic movements
- Stanley Road: 7-metre-wide crossover. Facilitates ingress/egress
- Springvale Road: 5 metres. Facilitates only egress and is infrequently used.

There are currently 163 onsite parking spaces.

The topography of the subject site is generally flat in the centre; however, there is a notable slope between the hardstand and southern boundary.

The site contains pockets of planted vegetation and shrubs, including some screening vegetation along the northern and southern property boundaries. There is no remnant native vegetation on the site.

The site is affected by an easement (E432033) located at the Hawthorn entry to the site, which contains stormwater infrastructure.

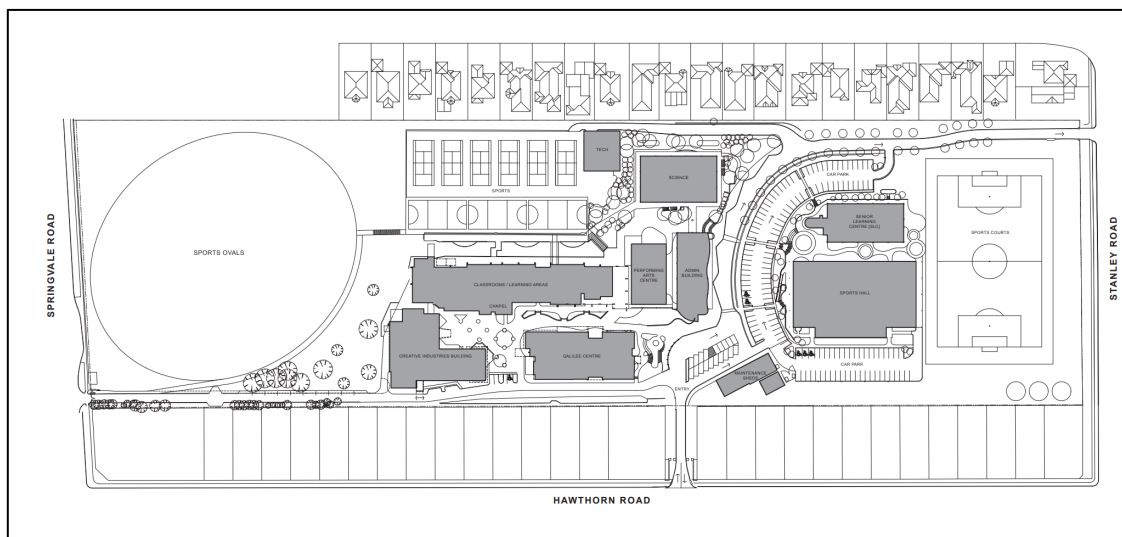


Figure 2: Site conditions Plan



Figure 3: Hawthorn Road Entry/Exit (Source: Google maps)

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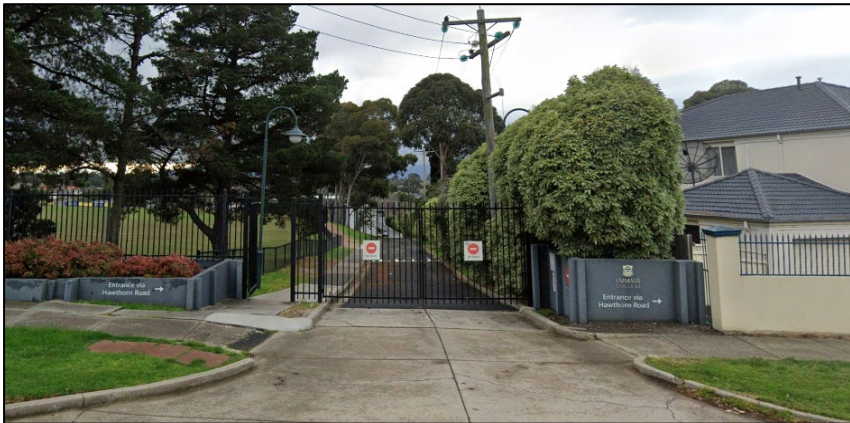


Figure 4. Springvale Road exit (Source: SearchComm/Google maps)

Figure 5. Stanley Road exit (Source: SearchComm/Google maps)

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2.2 Locality

The site is surrounded by residential properties to the north, south, and east. The dwellings to the north and south directly adjoin the subject site. The surrounding area is zoned Neighbourhood Residential Zone, whilst Springvale Road (abutting to the west) forms part of the Principal Road Network (Transport 2 Zone). The existing main entrance bisects the adjoining southern residential properties and connects to Hawthorn Road.

Planning Permit History

Emmanuel College has an extensive planning history associated with the ongoing development of the site as a school.

WH/2002/13240	Construction of buildings and works for additions to the school comprising a two-storey building, storage facilities, the main car park and extending an internal access road	Permit issued
WH/2004/15150	Buildings and works to extend a multi-purpose hall storeroom	Permit issued
WH/2005/671	Buildings and works to an existing school, comprising three new classrooms and a storage area	Delegate permit issued
WH/2005/725	Buildings and works to construct a three storey (administration and education) building	Delegate permit issued
WH/2005/725/A	S72 Amendment	Delegate approved
WH/2009/667	Buildings and works associated with an existing education facility, including construction of a VCE Learning Centre, use and construction of a gymnasium/multi-purpose hall, and additional car parking.	Delegate permit issued
WH/2010/116	Buildings and works to construct a roof canopy to an existing school	Delegate permit issued
WH/2010/557	Relocation of an existing shed on school site	Delegate permit issued
WH/2010/850	New door and glazed screen to an existing building	Delegate permit issued
WH/2011/770	Buildings and works to convert existing hall into a performing arts centre, and associated waiver of car parking requirements	Delegate permit issued

WH/2011/770/A	S72 Amendment to incorporate a left to the east elevation	Delegate approved
WH/2013/825	Display of advertising signage comprising an internally illuminated business identification sign and eight (8) pole signs	Delegate permit issued
WH/2015/371	Buildings and works to convert an existing change room into a multi-purpose learning area.	Delegate permit issued
WH/2015/472	Buildings and works for construction of a two-storey building associated with an existing secondary school	Delegate permit issued
WH/2015/472/A	S72 Amendment	Delegate approved
WH/2015/1105	Buildings and works to construct an outbuilding (tractor garage) and a water tank exceeding 4500 Litres	Delegate permit issued
WH/2019/1035	The removal of 2 trees	Application lapsed
WH/2020/26	Buildings and works associated with an education facility and waiver of the bicycle facilities requirements	Delegate permit issued
WH/2020/26/A	S72 Amendment for alterations to the approved education centre building, carpark, and earthworks	Delegate approved

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3. Proposal

The proposal is for a staged redevelopment, with works to occur primarily through the centre of the site in conjunction with the addition of a new Administration building and Chapel. The site centre is currently dominated by car parking and accessways. Redevelopment is intended to create a pedestrian prioritised environment resulting in an overall safer site layout. The proposal also requires the alteration of the existing bus bays, car parking, and the relocation of the school maintenance compound.

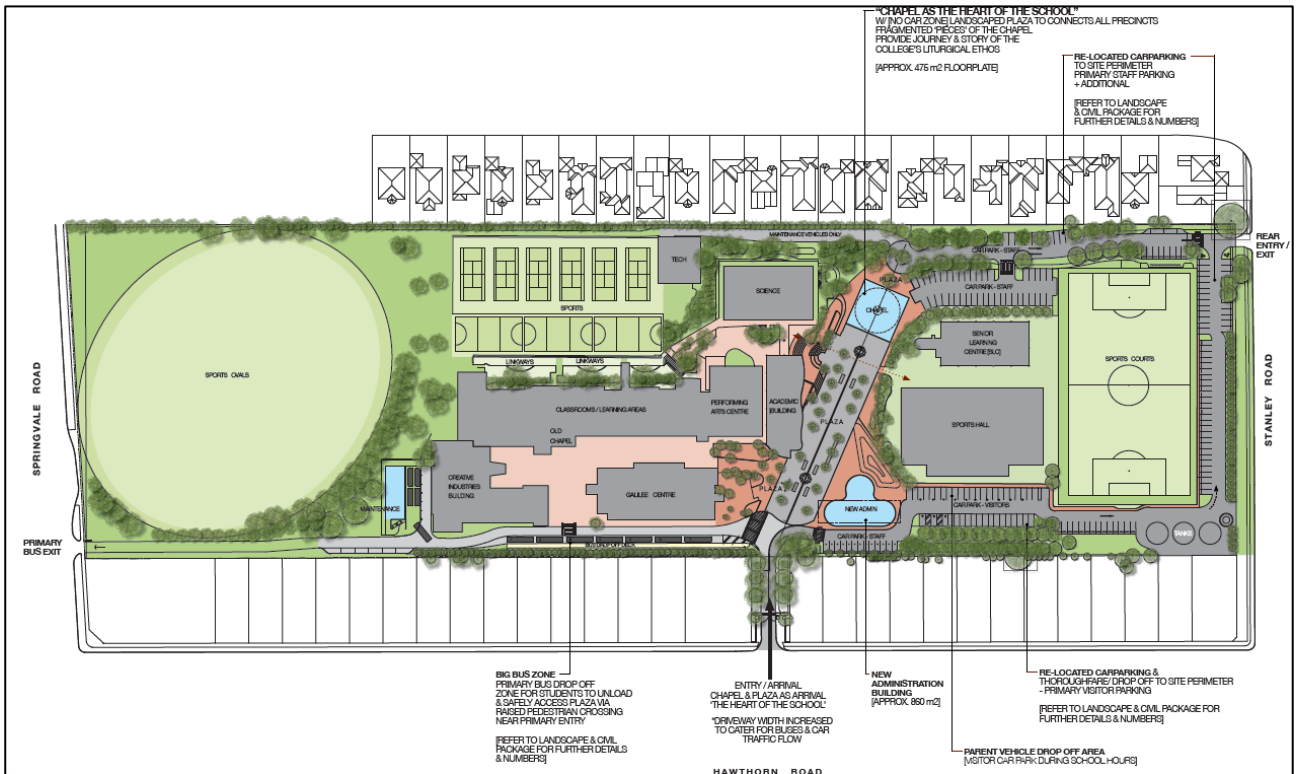


Figure 6. Site context – proposed site plan

Buildings and Works

The key components of the proposal include:

- Reconfiguration of the existing central accessway network to facilitate a landscape entry and walkable plaza and separation of pedestrian movements from bus zones.
- The new walkable plaza will house the two new buildings, being:
 - A new Administration building
 - Gross floor area of 860 square metres
 - Approx 6.9m tall, two storeys
 - External finishes: coloured concrete and red brick, clear glazed windows and doors
 - Approx. 12.95m from southern property boundary

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- Proposed south elevation below, per enclosed architectural drawings

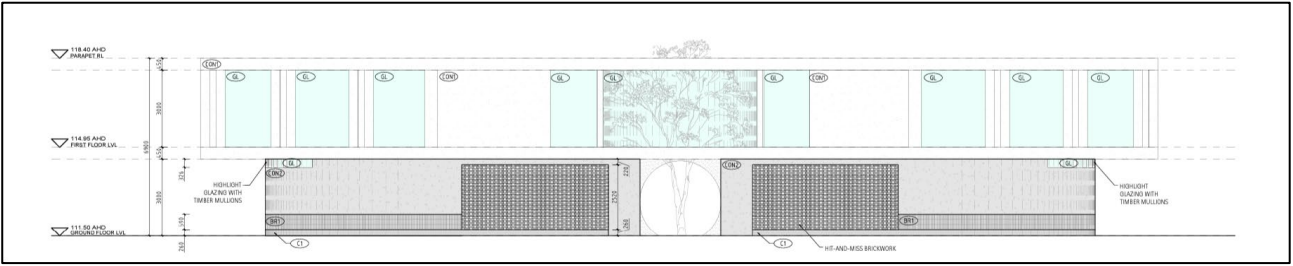


Figure 7. Administration Building Elevation

- A new Chapel
 - Gross floor area of 475 square metres
 - Approx 6.1m tall, single storey
 - External finishes: brass/metal cladding door, coloured concrete and red brick, clear glazed windows and doors, glazed tinted skylights.
 - Approx. 25m from northern property boundary

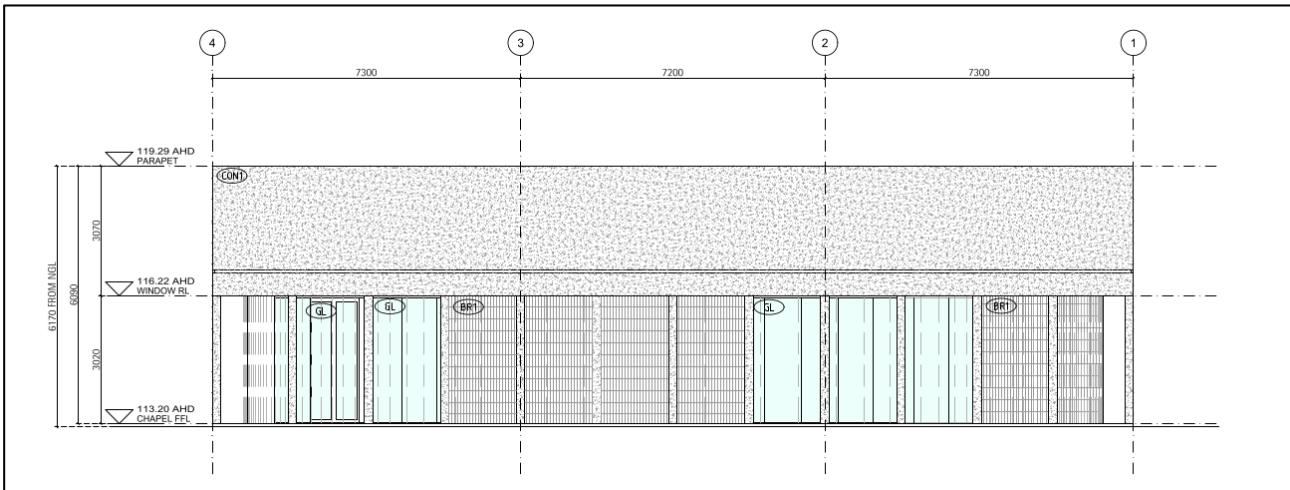


Figure 8. Chapel Elevation

- Relocation of the College maintenance building and waste collection area
 - Currently located near school entry and carpark (circled below), to be relocated to the south-east of the school oval,
 - Southern setback increased from 5 metres to 15.5 metres.

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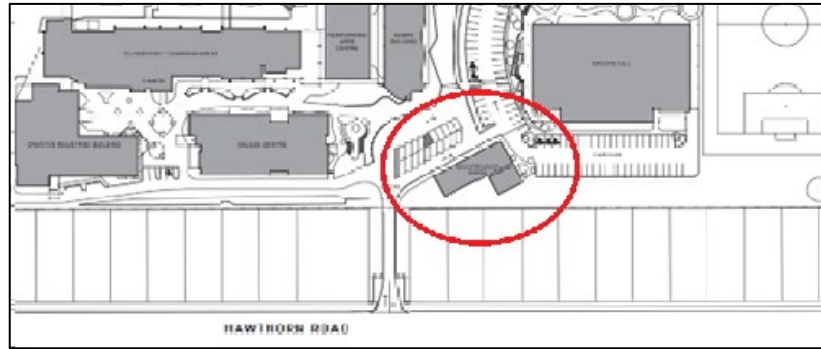


Figure 9. Existing Maintenance Building Location

Parking and Accessways

- Alterations to site vehicle access and car parking layout/provision
 - Increase of 20 car parking spaces, to a total provision of 183
 - Additional 25 bicycle racks
- Construction of new bus pick up/drop off area to the west of the main Hawthorn Road entrance,
 - Designed to accommodate buses of varying size depending on the College’s needs.
 - At capacity, the zone can facilitate up to six (6) full-sized 14.5m coaches, however, smaller buses will also be utilised by the school.
 - Hawthorn Road entrance amended to only provide vehicle ingress, egress capacity is removed.

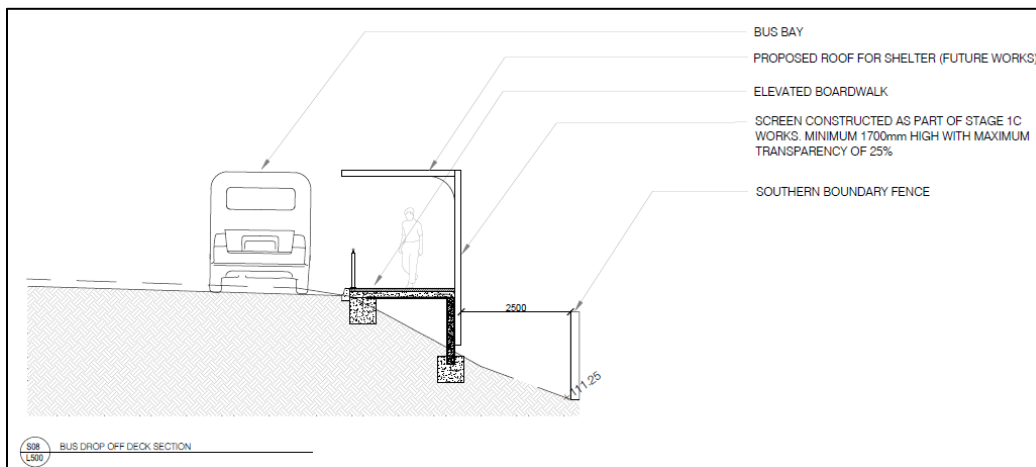


Figure 10. Bus Bay Cross Section

- Landscaping works throughout

The proposed buildings and works will not increase the total number of staff or students at the college.

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Staging

The proposed works are to be undertaken throughout two stages to ensure that the use of the school is not unreasonably interrupted. The staging of the works is depicted below:

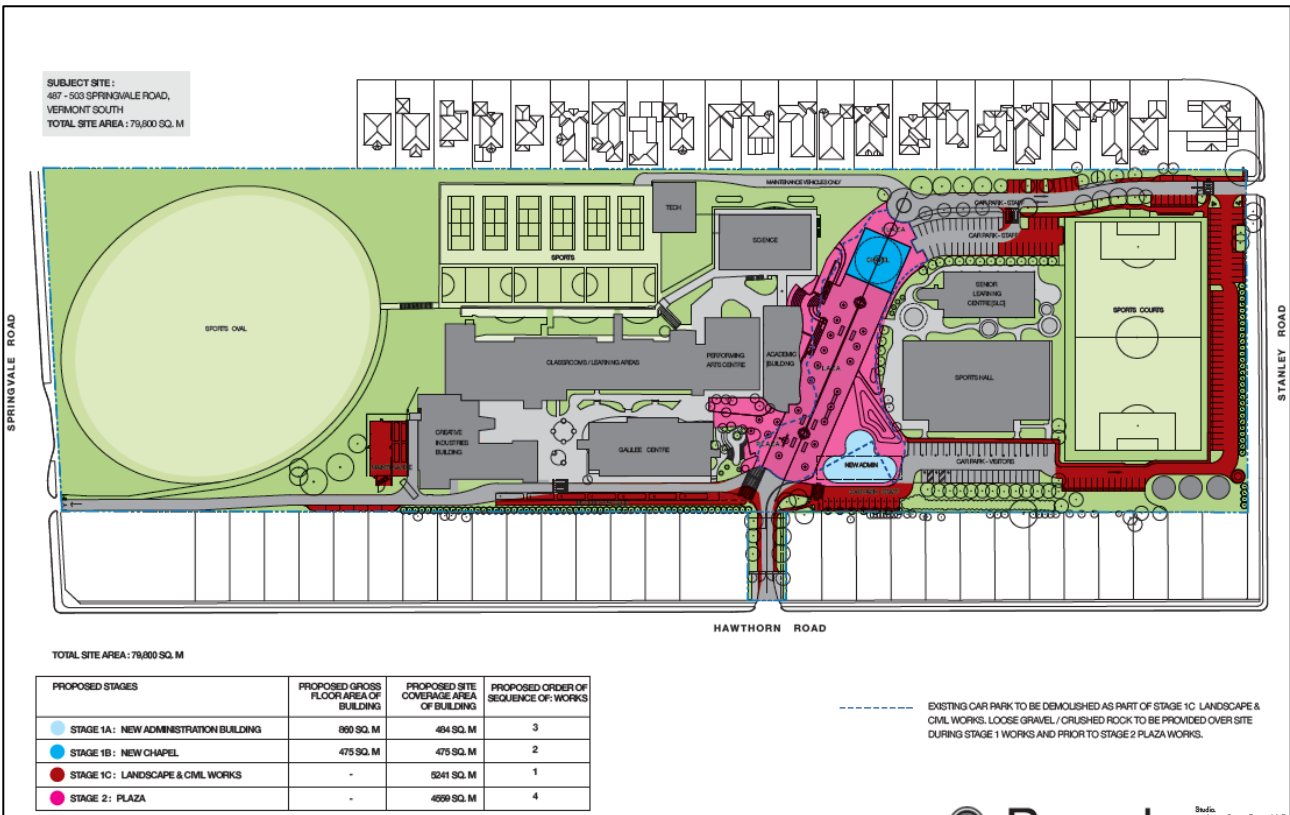


Figure 11. Proposed Staging Plan

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4. Statutory Planning Framework

4.1 Planning Policy Framework (PPF) and Local Planning Policy Framework (LPPF)

The Planning Policy Framework (PPF) provides the broad policy direction within the Victorian Planning Provisions. The planning principles set out under the State Planning Policy Framework (SPPF) are to be used to guide decision making on planning proposals across the state. Relevant policies within the PPF include:

Clause 13 – Environmental Risks and Amenity

- 13.07-1S: Land Use Compatibility

Clause 15 – Built Environment and Heritage

- 15.01-1s: Urban Design
- 15.01-2s: Building Design
- 15.01-4s: Healthy Neighbourhoods

Clause 18 – Transport

- 18.01-1s: Land use and transport integration
- 18.02-4s: Roads

Clause 19 – Infrastructure

- 19.02-2s: Education Facilities
- 19.03-2s: Infrastructure design and provision

The policies provide broad strategic direction to land use and development, encouraging efficient and coordinated planning outcomes that are responsive and adaptable to the changing needs of local communities.

State policy directions are implemented further at the local level in the Municipal Strategic Statement (MSS) and the Local Planning Policy Framework (LPPF). These policies establish land use and development strategies for the municipality. The policies relevant to this proposal include:

Clause 21.03 - A Vision for the City of Whitehorse

Clause 21.03 (A Vision for the City of Whitehorse) establishes the 2013-2017 Council Plan to guide an integrated planning approach for the municipality. Strategic objectives under this Clause aim to maintain, enhance, and develop the built environment to ensure a liveable and sustainable city. Emmaus College forms an integral part of the municipality as an established educational facility providing for students from all cohorts.

Clause 21.06 – Housing

- 21.06-7: Non-Residential uses

Clause 21.06-7 (Housing) guides strategic direction concerning non-residential uses in residential areas. Policy under this Clause stipulates for an integrated built form approach. Whilst Emmaus College is not controlled by a residential zone, it is surrounded by residential dwellings.

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Clause 22.04 – Tree Conservation

Clause 22.01 (Tree Conservation) applies to the entire municipality and contains policy concerning tree conservation. Objectives under this Clause involve the recognition of tree canopy coverage for the municipality and the promotion of regeneration and conservation.

Clause 22.10 – Environmentally Sustainable Development

Clause 22.10 (Environmentally Sustainable Development) is centred around establishing land-use development to ensure sustainable best-practice outcomes for the municipality. The policy contains application requirements to ensure the planning process incorporates ESD objectives.

4.2 Discussion

The proposal fulfils the strategic direction of State and Local planning policies. In support of this statement, the following comments are made:

Amenity Considerations

- Pursuant to Clause 21.03 (A Vision for the City of Whitehorse), the proposal aids in supporting a 'healthy, vibrant, inclusive and diverse community'. Existing conditions of the buildings are described as cold and harsh – undermining surrounding neighbourhood character and fostering an unreceptive learning environment. In meeting the anticipated needs of current and future students, redevelopment of the site creates an ideal opportunity to regenerate the site into one that harmonises with the surrounding neighbourhood and enhances liveability for students, staff, and the wider community.
- The site has been used for educational purposes for approximately 45 years. Given the use and its respective intensity are not proposed to be changed there will not be a material impact on the amenity of the surrounding residential neighbourhood.
- The Special Use Zone is not a residential zone, and accordingly Clause 22.05 (Non-Residential uses in Residential Areas) does not apply. Despite this, it is acknowledged that the subject site is abutted by a residential catchment, and the relevant purposes of the zone which seeks to protect the amenity of the neighbourhood. Notwithstanding, in response to Clause 22.05, the amended design intends to redirect buses exiting the site from residential streets (Stanley Road) to a higher order road (being Springvale Road), forms part of the social fabric of the area and redevelopment of the site is responsive to the services and facilities required to serve by the local community.
- Consistent with the purpose of Clause 15.01-2s (Building Design), the proposed Chapel and Administration Building contribute positively to the local context. The form, scale and setback of the proposed buildings are designed to an acceptable scale in consideration of the shared boundaries with the surrounding neighbourhood.
- Construction of the bus bay along the southern boundary is not anticipated to have a detrimental impact for the adjoining residential properties. Overshadowing is not unduly intensified and is aligned with shadowing created by residential fencing. Further, the sloped area between the residential properties and shelters are to be densely vegetated, as demonstrated by the accompanying landscape plan to aid in preserving amenity.

Transport and safety

- The current layout of the site is inadequate in providing a safe internal road network and contributes to the congestion experienced by the surrounding neighbourhood. Presently the entrance and exits create

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bottlenecks that impact the surrounding local streets during drop-off and pick-up times. The new layout will facilitate improved vehicular movement by directing heavy vehicles to Springvale Road (higher order road). This is consistent with Clause 18.01-1S (Land use and Transport Integration) by improving residential traffic movement.

- The internal facilities will be updated to produce a safer and more efficient site layout and built form. The restructured layout will separate staff and students from using the carpark/accessway as a thoroughfare and limit the likelihood of pedestrian-vehicle conflicts. This is consistent with Clause 18.01-1S (Land use and Transport Integration) and Clause 15.01-4s (Healthy Neighbourhoods). The proposal facilitates an efficient and safer road network and the prioritisation of the practical separation of pedestrians and vehicles.
- The proposed reconfigured internal layout (Figure 5) will have a net positive community benefit. The redevelopment will reduce congestion for the surrounding neighbourhood and provide a safer walkable environment for all pedestrians that will minimise potential vehicle-related harm.

Vegetation Removal

- The proposal prioritises the preservation of vegetation where possible to ensure local amenity to adjoining residential properties is maintained and only proposes the removal of vegetation when critical.
- Consistent with the purposes and objectives of Clause 22.04 (Tree Conservation), a comprehensive landscaping scheme has been prepared which consists of native, densely planted trees and understory vegetation.

Environmentally Sustainable Development

- Pursuant to Clause 22.10 (Environmentally Sustainable Development) the following comments are submitted in relation to the proposed Administration building:
 - Indoor environment quality
 - High-performance double-glazed windows throughout allowing for thermal comfort
 - Cross-ventilation measures, particularly on the first level
 - Orientation of the building to maximise solar aspect and allowing for natural light penetration
 - Energy efficiency
 - Double-glazed windows for increased thermal efficiency
 - Shading: the eave on the north facade eave controls the sun and shades the facade during the hottest times of the day and year.
 - Rooftop garden adds a 'green cushion' to the concrete roof, acting as an additional layer of insulation and allows for urban heat island mitigation
 - Thermal mass considered through the use of solid natural and raw masonry materials
 - The internal courtyard allows for passive cooling and heat reduction
 - Provision for solar panels has been incorporated into the design
 - Stormwater management

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- The large existing water tanks on site to be used to the fullest capability for the collection of stormwater
- Longevity
 - Build to Last - considered, adaptive, and flexible design used in conjunction with quality materials and detailing give the building an increased life span.

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Response to Decision Guidelines

- The proposed bus shelter is located between the new maintenance shed and the accessway from Hawthorn Road, and will accommodate buses of varying sizes depending on the College's needs. The pick up/drop off deck and shelter is installed over the embankment, setback from the southern boundary approximately 2.9 metres. Per the provided landscape plans, screening vegetation will be planted along the southern boundary. This will allow for neighbourhood amenity to be sustained and provide privacy to the residential catchment.
- Two pedestrian crossings are proposed within the bus zone. Both crossings will be supervised during pick up/drop off times by school staff to ensure orderly use by students.
- The three (3) points of access to the subject site will be amended as follows:
 - Hawthorn road: designated as entry-only access for school traffic, buses, maintenance staff, and waste collection vehicles. Entry from the left lane will only allow access to buses, maintenance staff and waste collection via a boom gate.
 - Stanley road: designated entry for staff, exit only for all staff and school traffic. Staff entry is managed by a boom gate.
 - Springvale road: designated as exit-only for all buses, staff maintenance and waste vehicles
- Administration building is approx. 13.6m from southern property boundary; the Chapel building is approx. 24.3m from northern property boundary
 - Whilst not technically applicable, the development would substantially exceed the relevant Rescue standards for side setbacks (B17), overlooking (B22) and overshadowing (B21). As such, it is considered that the proposed built form is appropriate when considered against the sensitive residential interfaces.
- The buildings are designed with a variety of materials and modulated building forms in order to reduce any perceived visual impact.
- Civil works to reconfigure the internal road network will require the removal of several trees across the subject site. An appropriate level of replanting is proposed as part of the landscaping scheme, particularly along sensitive interfaces.
- The 'bus zone' precinct will house the new maintenance shed and waste collection area. Further detail is provided in the attached plans.
- The use and operational hours of the site is unchanged by the proposal and therefore there is no expectation of increased amenity impacts.
- An increase in the provision of parking is not expected to cause any detrimental impact to the surrounding road network. Per the provided Traffic Impact Assessment, the increase of parking spaces equates to one (1) additional vehicle p/minute during school peak hours, which is a negligible impact.
- The provided landscaping plans illustrate the provision of revegetation to preserve the privacy and amenity of the surrounding residential catchment.
- Given the proposal generally concerns the internal redevelopment of the site, the level of amenity to the surrounding residential catchment is not materially impacted.

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- The reconfiguration of the site's internal road network is expected to regulate vehicular traffic during peak hours, particularly around the school's entry and exit points.
- The proposed redevelopment of Emmaus College achieves the purposes of SUZ1 in ensuring that the development takes place in an orderly and proper manner given the staged approach.

5.2 Overlays

Development Contributions Plan Overlay – Schedule 1

Per Clause 45.06-1-4, "land developed for a non-governmental school, as defined in the Ministerial Direction on the Preparation and Content of Development Contributions Plans dated 11 October 2016" is exempt from the requirement to pay any development contribution under the Whitehorse Development Contributions Plan.

5.3 Particular Provisions

Clause 52.06 – Car Parking

Car parking for the subject site is controlled in Whitehorse Planning Scheme under Clause 52.06. Parking rates for land uses are specified in Table 1 to Clause 52.06-5. The applicable rate for a Secondary School is:

- 1.2 spaces to each employee that is part of the maximum number of employees on site at any time.

For an increase to an existing use, the measurement specified in Column C of Table 1 is applied only to the increase, provided there is no reduction in the provision of parking associated with the use. The proposal will not result in an increase to staff or student numbers at the school. As such, the statutory rate for the provision of car parking does not apply for the proposal.

Please refer to the Traffic Impact Assessment prepared by Stantec for further details regarding car parking and access.

The proposal achieves compliance with the relevant standards of Clause 52.06-9

Clause 52.17 – Native Vegetation

Pursuant to the exemption "Planted Vegetation" contained in Clause 52.17, the application is exempt from requiring a permit for removal given all on-site vegetation was planted or grown as a result of direct seedling. Further detail is provided in the accompanying arborist report, prepared by Arbor Advocacy.

Clause 52.34 – Bicycle Facilities

Clause 52.34 (Bicycle Facilities) stipulates the number of bicycle spaces required for any given land use. The provision required for a Secondary School is determined by the number of students and staff employed on-site at any one time.

Notwithstanding the increase of buildings on-site, there is no change to staff or student numbers as a result of the proposal. The development does not, therefore, require an increase to the current supply of bicycle spaces; however, works to the internal road network will result in an additional 25 bicycle spaces added on-site.

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Clause 53.19 – Non-government schools

The purpose of Clause 53.19 (Non-government schools) is to facilitate upgrades and extensions to existing non-government schools and is applicable to Emmaus College.

To support this process the clause exempts the application from the decision requirements of section 64(1), (2), and (3), and the review rights of section 82(1) of the Act.

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6. Conclusion

The report comprehensively details how the proposal:

- Is necessary for the future growth and development of the school to keep up with the growing needs of the community
- Reduces congestion on the surrounding neighbourhood road network and prioritises pedestrian movement
- Is designed to ensure a healthy and sustainable learning environment that is receptive to the surrounding residential neighbourhood

We look forward to a favourable decision from Council and are happy to meet with your office to discuss the proposal and provide any further information as required to ensure a timely decision by Council.

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