

# Assessment Officer Report

Permit No PA2402910  
487-503 Springvale Road, Vermont  
South  
Emmaus College



Officer Assessment Report  
Development Approvals & Design

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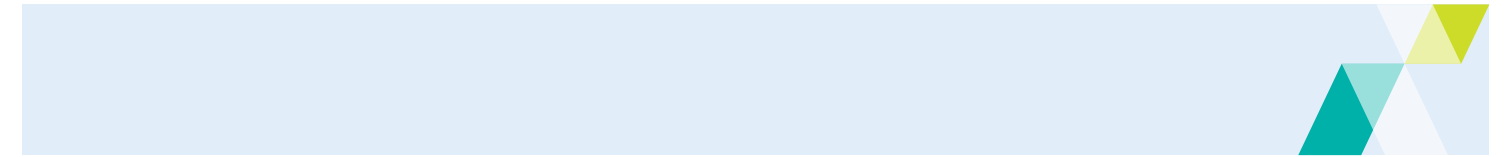
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# Executive Summary



Key Information	Details			
<b>Application No:</b>	PA2402910			
<b>Received:</b>	7 May 2024			
<b>Statutory Days:</b>	60			
<b>Applicant:</b>	Emmaus College c/- URPS			
<b>Planning Scheme:</b>	Whitehorse			
<b>Land Address:</b>	487-503 Springvale Road, Vermont South			
<b>Proposal:</b>	Buildings and works in association with an existing secondary school			
<b>Development Value:</b>	\$10m			
<b>Why is the Minister responsible?</b>	In accordance with the schedule to Clause 72.01 of the Planning Scheme, the Minister for Planning is the responsible Authority for this application because the application is for development of a primary school or secondary school, or education centre that is ancillary to, carried out in conjunction with, and on the same land or contiguous land in the same ownership as, a primary school or secondary school for an estimated cost of development for \$3 million or greater.			
<b>Why is a permit required?</b>	<b>Clause</b>	<b>Control</b>	<b>Trigger</b>	
<b>Zone:</b>	Clause 37.01	Special Use Zone Schedule 1 (SUZ1)	Construct a building or construct or carry out works	
<b>Overlays:</b>	Clause 45.06	Development Contributions Plan Overlay Schedule 1	N/A – Non-government schools exempt	
<b>Particular Provisions:</b>	Clause 52.06	Car Parking	N/A – No reduction is required to parking requirements	
<b>Cultural Heritage:</b>	The site is not in an area of cultural heritage sensitivity			
<b>Total Site Area:</b>	79800	m <sup>2</sup>		
<b>Gross Floor Area:</b>	1335	m <sup>2</sup>		
<b>Height:</b>	2 and 1	Storeys (2 buildings)		
	6.9 and 6.1	Metres (2 buildings)		
<b>Land Uses:</b>	<b>Dwellings</b>	<b>Office</b>	<b>Retail</b>	<b>Other</b>
				Secondary school
<b>Parking:</b>	<b>Cars</b>	<b>Motorcycles</b>	<b>Bicycles</b>	
	183	0	25	
<b>Referral Authorities:</b>	Nil			
<b>Advice sought:</b>	Whitehorse City Council - objection Head, Transport for Victoria – support subject to conditions			
<b>Public Notice:</b>	Notice of the application was undertaken by the applicant at the direction of the Department of Transport and Planning under delegation to the Minister for Planning in the following manner:			

- 
- mail to the owners and occupiers of adjoining land.
  - three (3) signs displayed centrally on the site frontages of Stanley Road, Hawthorn Road and Springvale Road.

2 objections from the community have been received as of 24 December 2024.

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**Delegates List:**

N/A

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## Application Process

- The key milestones in the application process were as follows:

Milestone	Date
Pre-application meeting	1 November 2023
Application lodgement	7 May 2024
Further information requested	28 May 2024
Further information received	14 October 2024
Decision Plans	<p>Plans prepared by Branch Studio Architects, titled 'Vermont Campus Plan' (rev. 10) dated 10 July 2024, 'Proposed New Administration Building' dated 28 June 2024, 'Proposed New Chapel' dated 28 June 2024.</p> <p>Landscape plan prepared by Orchard Design, titled 'Emmaus College, Vermont Campus' and dated 4 July 2024.</p>
Other Assessment Documents	<p>Transport Impact Assessment by Stantec dated 1 July 2024.</p> <p>Planning Report by URPS dated 15 August 2024.</p> <p>Arborist Report by Arbor Advocacy dated 8 February 2024.</p> <p>Sustainable Design Assessment by Sustainable Design Consultants dated October 2024.</p> <p>Civil Drawings (Rev P5) by Ipsum Structures dated 10 July 2024.</p>

- The subject of this report is the decision plans (as described above).

## Proposal Summary

- The proposal can be summarised as follows:

Key Information	Details
Proposal:	Buildings and works in association with an existing secondary school
Total Site Area:	79800 m <sup>2</sup>
Gross Floor Area:	1335 m <sup>2</sup>
Height:	6.9m (Administration building) and 6.1 m (Chapel)
Setbacks:	25.6m from northern property boundary (Chapel) 12.5m from southern property boundary (Administration building)
Land Uses:	Secondary school





<b>Car Parking:</b>	20 additional spaces
<b>Bicycle Parking:</b>	25 additional spaces
<b>Motorcycle Parking:</b>	N/A
<b>Total Car Parking:</b>	183 spaces
<b>Loading and Waste arrangements:</b>	Waste storage and collection will be from the proposed relocated maintenance building and waste collection area, located adjacent to the sports oval on the west side of the school grounds. Waste vehicles will access the site via the Hawthorn Road entrance and exit onto Springvale Road.

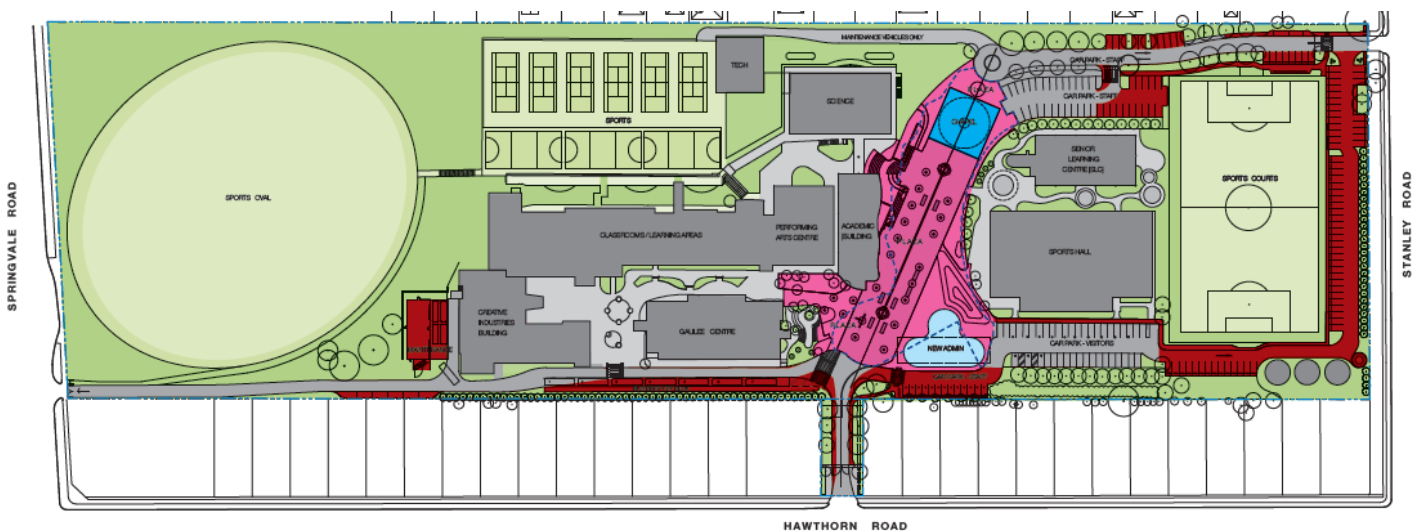


Image: Proposed masterplan showing proposed works in blue, red and pink (Source: Application)

4. Specific details of the application include:

- A new Administration building
  - Gross floor area of 860 square metres
  - 6.9m building height, two levels
  - Setback 12.5m from southern property boundary

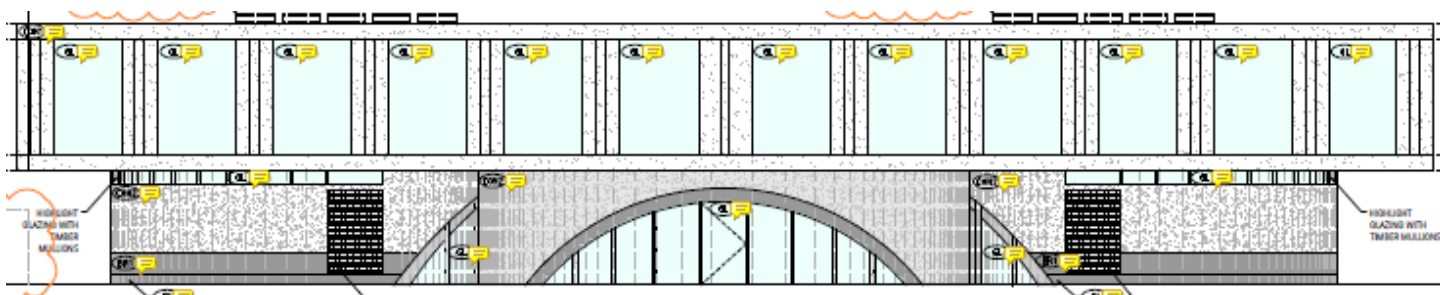


Image x: Proposed Administration building, north elevation (Source: Application)

- A new Chapel
  - Gross floor area of 475 square metres
  - 6.1m building height, single level
  - Setback 25.6m from northern property boundary

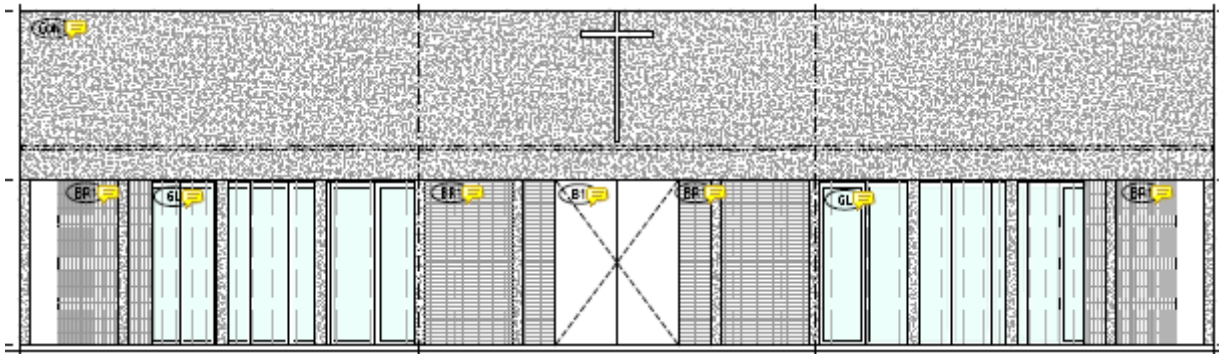


Image: Proposed Chapel building, south elevation (Source: Application)

- Relocation of the College maintenance building and waste collection area to south-east of the sports oval. The maintenance building will be setback 15.6m from the south boundary and will be approximately 5.3m high. It is noted the building drawings are included in the landscape plan package.
- Alterations to site vehicle access and car parking layout/provision as follows:
  - Central carparking area removed, replaced with pedestrianised plaza space
  - New/extended carparking areas, including along the site frontage to Stanley Road and north and south boundaries.
    - Includes earthworks involving placement of fill on the south boundary, to create a carparking area on the south side of the administration building, shown in the image below. The landscape plan indicates the fill would be retained by a retaining wall of 0.8m – 1.4m high, which would extend an additional 0.7m above the finished ground level and a 1m high screen above. The total height on the south boundary of this screen would be between 2.5m – 3.1m high.

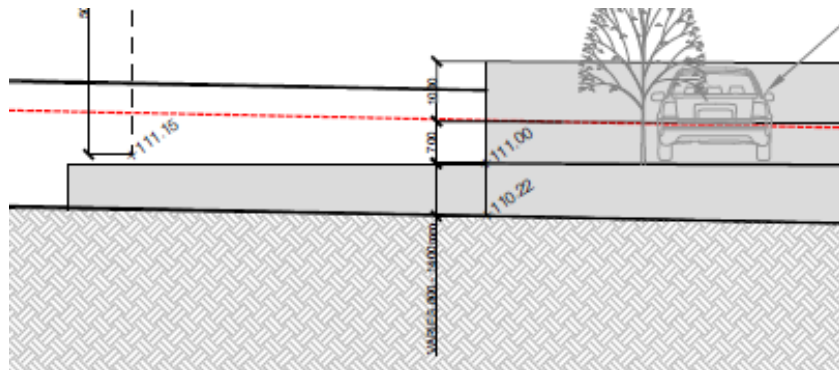
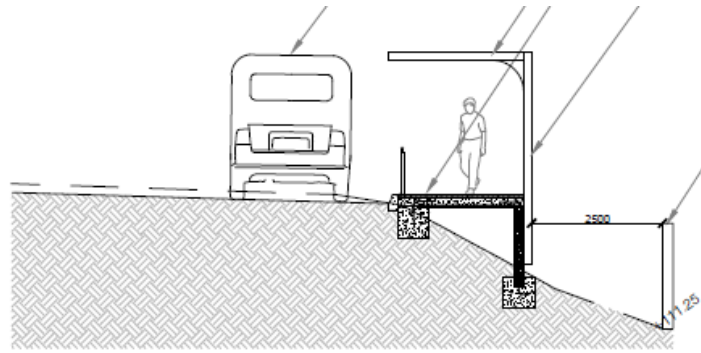


Image: Section of proposed earthworks and retaining wall for carparking adjacent to south boundary (Source: Application)

- Increase of 25 car parking spaces, total provision of 183
- Additional 25 bicycle racks, near the Stanley Road and Hawthorn Road entrances.
- New bus pick up/drop off area to the west of the main Hawthorn Road entrance.
  - A screen is proposed to be constructed on the south side. The landscape plans show the screen setback 2.5m from the south boundary, with a total height of approximately 3.3m above ground.



*Image: Section of proposed bus pick up/drop off area adjacent to south boundary (Source: Application)*

5. Changes to the proposed site access/egress are also proposed and are discussed in the Assessment section of this report.
6. No works are proposed to the Springvale Road entrance, which would otherwise have triggered a permit and referral requirement to Department of Transport under Clause 52.29. It is noted the proposal includes changing the function of this entrance, which is currently used infrequently for egress, to be the egress for buses exiting the site.
7. No increase in student or staff numbers is proposed.





## Site Description

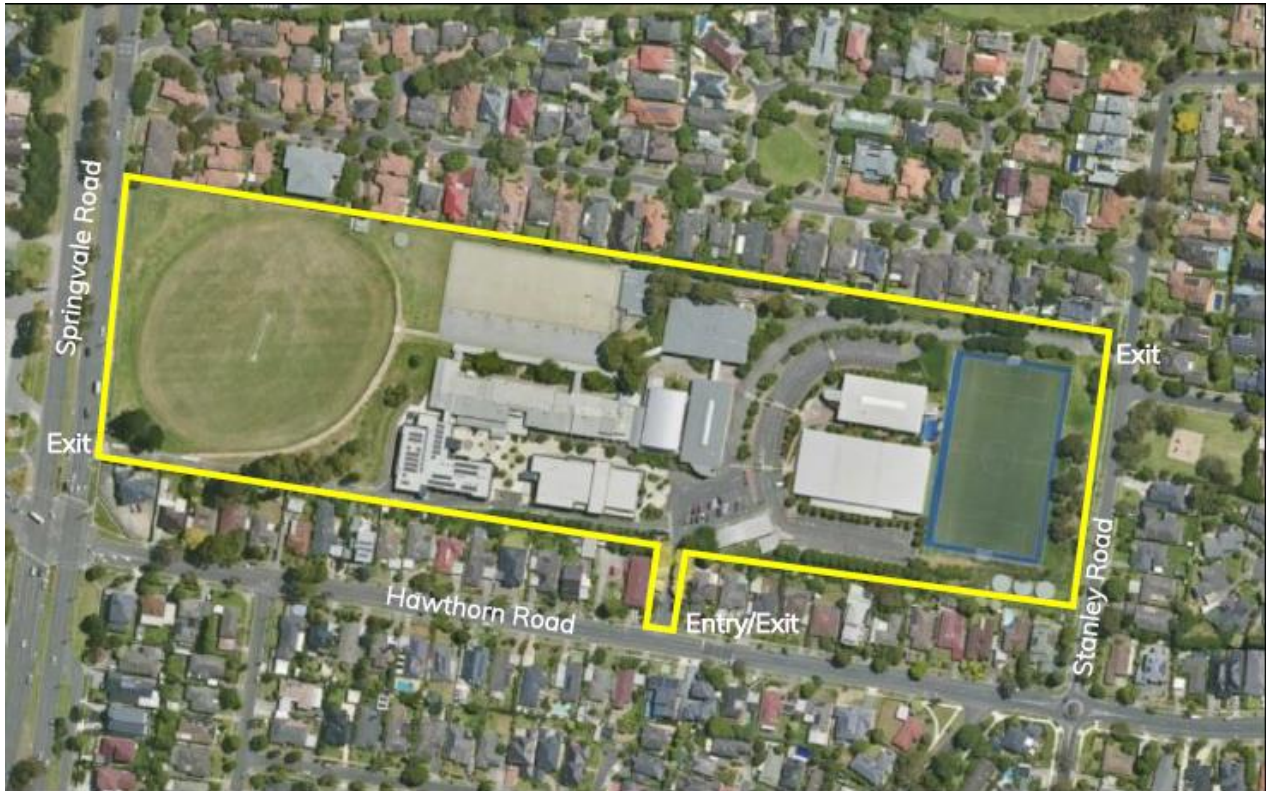


Image x: Subject site (Source: Application)

8. The site is located at 487-503 Springvale Road Vermont South. The site is located to the east of Springvale Road and is generally rectangular in shape. The primary portion of the site extends from Springvale Road to Stanley Road and has a length of approximately 529.3 metres. The width of the site is 151 metres, and the total site area is approximately 79,800 square metres. An additional parcel is located to the south of the site and provides access to Hawthorn Road.
9. The existing site is used for a secondary school, which currently contains the following:
  - A mixture of single and double storey school buildings.
  - A variety of sports courts, outdoor areas, and gathering spaces.
  - Large carparking areas and internal accessways.
  - Landscaping including internal pathways, garden beds and planted trees around the school grounds.
10. The site is formally described as comprising the following land parcels:
  - Lot 235 PS88921
  - Lot 1 TP333893K
11. A drainage and sewerage easement is located on north boundary of Lot 235, at the Hawthorn Road entry to the site.
12. Covenant E432033 is also registered on title for Lot 235, which prohibits construction of buildings other than a dwelling. As the proposal includes only driveway and landscaping works on that lot, there is no conflict with the covenant.



13. There are three current vehicle ingress/egress points to the site:
  - Hawthorn Road: 16.5m wide crossover. Facilitates ingress/egress. Hawthorn Road operates as the main entrance and has the highest number of traffic movements
  - Stanley Road: 7m wide crossover. Facilitates ingress/egress
  - Springvale Road: 5m wide crossover. Facilitates only egress and is infrequently used.
14. There are currently 163 onsite parking spaces.
15. The topography of the subject site is generally flat in the centre; however, there is a notable slope between the hardstand and southern boundary.
16. The subject site is located within the Special Use Zone – Schedule 1 and is within a Development Contributions Overlay – Schedule 1. The southern half of Stanley Road in front of the site is affected by a Special Building Overlay, although this overlay does not affect the subject site itself.

### Site Surrounds

17. Adjoining land to the north, east and south is in a Neighbourhood Residential Zone – Schedule 5, as well as Transport Zone 2. Surrounding land is also affected by a Significant Landscape Overlay – Schedule 9, although the subject site itself and notably the road reserve directly in front of the subject site to the east and west (Stanley Road and Springvale Road) is not affected by the overlay.
18. The site is surrounded by residential properties to the north, south, and east. The dwellings to the north and south directly adjoin the subject site.

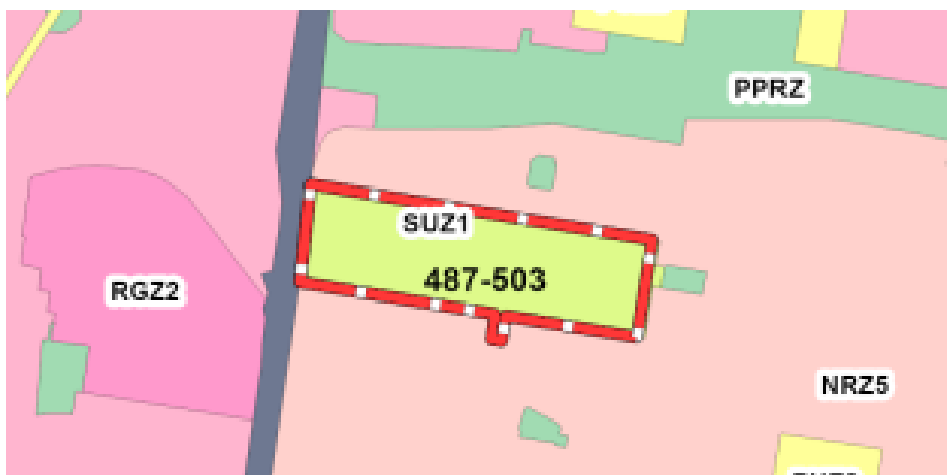


Image x: Zoning of site and surrounds (Source: Planning Property Report)



## Municipal Planning Strategy

19. The following objectives and strategies of the Municipal Strategic Statement of the scheme are relevant to the proposal:

Clause	Description
02.01	Context
02.02	Vision
02.03-2	Environmental and landscape values
02.03-3	Environmental risks and amenity
02.03-5	Built environment and heritage

## Planning Policy Framework

20. The following objectives and strategies of the Planning Policy Framework of the scheme are relevant to the proposal:

Clause	Description
13.07-1S:	Land use compatibility
13.07-1L	Non-residential uses in residential areas
15.01-1S	Urban design
15.01-2S	Building design
15.01-4S	Healthy neighbourhoods
15.01-2L	Environmentally sustainable development
15.01-5L-01	Tree conservation
18.01-1S	Land use and transport integration
18.02-4S	Roads
18.02-4L	Car parking
19.02-2S	Education facilities
19.02-2L	Education facilities
19.03-3S	Integrated water management

## Zoning and Overlays

### Applicable Zone

21. A planning permit is required to construct a building or construct or carry out works in accordance with Clause 37.01 – Special Use Zone.

22. The purpose of Schedule 1 to Clause 37.01 is:

- To provide for areas in private ownership to be used for an education centre or place of worship.
- To ensure that the use and development of these facilities takes place in an orderly and proper manner and does not cause loss of amenity to the neighbourhood.

23. The following sections include a discussion of how the proposal responds to these requirements.



## **Applicable Overlay**

24. The site is affected by the Development Contributions Plan Overlay, but is exempt as a non-government school from the requirements under the schedule to the overlay.

## **Particular and General Provisions**

### **Provisions that Require, Enable or Exempt a Permit**

25. Clause 52.06 sets out the requirements of car parking provision and design. No additional car parking is required for the proposal as no additional staff are proposed however, there will be an increase in carparking of 20 spaces. Car parking design is required to be to the satisfaction of the responsible authority.
26. Clause 52.34 sets out the requirements for provision of bicycle parking. No bicycle facilities are required for the proposal as there will be no increase in staff or students however, 25 additional bike parking racks are provided.

### **General Requirements and Performance Standards**

27. Clause 53.18 applies to this application for buildings and works. The clause includes standards and objectives relating to stormwater.



## Referrals

28. No referrals are required under Section 55 of the *Planning and Environment Act 1987*.

## Municipal Council Comments

29. Whitehorse City Council (the council) officers objected to the application, raising the following issues:

- Overlooking from the proposed administration building to the south.
- Visual bulk, overlooking, noise, light spill and air pollution from the bus shelter to the south.
- Impact and lack of clarity of level changes along south and east boundaries.
- Increased vehicle movements to Stanley Road. The council noted the proposed changes to the Stanley Road access/egress would be a change from permit (WH/2020/26/A), which was issued by the council for buildings and works to an education facility.
- Vegetation removal and proposed carparking at Stanley Road interface.
- Impacts on street trees in Stanley Road.

30. The council requested the following changes:

- The use of the access point of Stanley Road should be reconsidered to lessen the amount of vehicles entering/exiting this site to lessen the impact of traffic flow along this residential street.
- Priority should be given towards increasing boundary setbacks to address (a) significant level change (minimising earthworks) and (b), to provide a thick landscape screen (tall shrubs and canopy trees to north, south and east boundary interfaces).
- Address significant land slope such that floor levels are not generally elevated beyond 400mm above natural ground level, and all external amenity impacts can be mitigated.
- The upper floor of the new administration building repositioned to the northern half of the development footprint (not southern half), and north-south aligned (not east-west aligned) so that views and light spill project away from the southern residential boundary interface.
- The administration building and south abutting staff carpark, relocated 400mm further north, to take all earthworks off the south boundary, and to provide thick landscape screening along the fence-line.
- All car spaces on the east side of the accessway aisle, east of the sports courts or beside the east boundary, deleted, replaced with landscape screening.
- A 2 to 3 metre wide multi row of landscaping (tall shrubs and canopy trees) along the south boundary, south of the 3 water tanks.
- The vertical screen for the pedestrian cantilevered walkway associated with the bus drop off / pick up area, provided with a green wall (vines etc) on its south side.
- Any trees proposed in the landscape plan should be at least 1.5 metres in height when planted.

31. All matters raised by the council are discussed in the assessment section of this report. The council also provided a list of standard council permit conditions, should a permit be granted.

## Advice sought from other agencies

32. Head, Transport for Victoria provided conditional support for the proposal. The conditions required by Transport for Victoria have been included as conditions on the permit.





## Notice

33. The applicant was directed to give notice in accordance with s52 of the *Planning and Environment Act 1987*, by way of erecting signs on the site and notifying adjoining owners and occupiers.
34. Two objections were received, raising the following issues:
  - Increased vehicle and pedestrian traffic in Stanley Road.
  - Impacts during construction.
  - Overlooking.
  - Noise.

## Applicant response

35. The applicant provided a written response to concerns raised by council and other objectors. The applicant has advised they would agree to the following requirements:
  - Proposed trees to be either juvenile or 1.5m in height when planted.
  - A tree management plan as a condition of permit to manage impacts on trees on adjoining land, including street trees.
  - Additional street trees in Stanley Road.



## Key Considerations

36. The following are deemed the key considerations in assessing the acceptability of the proposal:

- Strategic Direction and Land Use
- Buildings and works
- Car Parking and Traffic
- Vegetation impact
- Council concerns
- Concerns raised in objections.

## Strategic Direction and Land Use

37. The *Planning Policy Framework* encourage appropriate land use and development which enhances the built environment, supports economic growth, meets the community expectations on retail and commercial provision, delivers diversity in housing supply to meet existing and future needs, and integrates transport and infrastructure planning.

38. The relevant MPS and PPF policies have been considered in assessing the application, as described earlier in the report.

39. The proposed land use is consistent with the strategic directions of the Whitehorse Planning Scheme and will deliver enhanced facilities for the existing school, which is in an established urban area of Vermont. No change in the use of the school is proposed by this application.

## Buildings and Works

40. The zoning provisions have been considered in the application.

41. The purpose and decision guidelines of the zoning have been considered and it is noted the proposal includes a masterplan showing the proposed buildings and works in relation to the whole site.

42. The proposal is appropriate, having regard to the built-form decision guidelines of the Special Use Zone.

## Car and Bicycle Parking, Loading, and Other Services

### Car Parking

43. No additional parking is required as noted earlier. The proposal does, however, include an increase of 20 additional onsite parking spaces.

44. The transport impact assessment report (TIAR) has provided an estimate that the additional onsite parking could generate an additional 40 vehicle movements per hour during peak hours, which it is considered could be accommodated by the existing road network.

### Design Standards for Car Parking

45. The TIAR has assessed the proposal as compliant with Clause 52.06-9, except for the northeast parking area, which has an accessway width of 5.5m rather than 6.4m. The TIAR regarded this as acceptable based on swept path diagrams showing vehicles can satisfactorily enter and exit the spaces.



## Access, Traffic Movement and Circulation

46. The proposal includes the following changes to site access/egress:

- Hawthorn Road – local collector road:
  - Existing: Primary access. Also allows for egress, though internal accessway linemarkings direct most traffic to the Stanley Road egress.
  - Proposed: Access only for school traffic, buses, maintenance staff, and waste collection vehicles. Access from the left lane will only allow access to buses, maintenance staff and waste collection via a boom gate.
- Stanley Road:
  - Existing: Main egress for all vehicles, including buses.
  - Proposed: Access for staff including a boomgate, main egress for all vehicles, excluding buses, staff maintenance and waste vehicles.
- Springvale Road:
  - Existing: Infrequently used egress only.
  - Proposed: Egress only for all buses, staff maintenance and waste vehicles.

47. Council and one of the objectors raise concerns with the impact of additional traffic on Stanley Road should the proposed access be approved.

48. Additional traffic from staff using the Stanley Road access is unlikely to result in an unreasonable impact, given the vehicle movements will be limited to staff.

49. Furthermore, the proposal will divert buses away from Stanley Road to the Springvale Road exit. 20 additional onsite parking spaces are proposed, which will also divert these 'visitor' vehicles from onstreet parking.

50. The removal of parking in the centre of the site will also provide a more pedestrianised environment within the school.

51. It is therefore considered there will be a net benefit from the proposed changes.

## Bicycle Facilities

52. No bicycle facilities are required for the proposed buildings and works as there is no increase in staff or students.

53. The 25 bicycle space proposed will add to the existing bicycle parking supply and are considered appropriately located, noting no concerns were raised by the council.

## Waste

54. A waste management plan has not been provided. Council's objection did not raise this as a concern. The proposal includes moving the waste area to the west side of the site and for waste vehicles to exit onto Springvale Road, which should improve the operation of the site. It is not considered that a waste management plan is required as there is ample space onsite for waste storage and collection. A condition of permit will, however, require all waste vehicles to enter via the Hawthorn Road access and exit via Springvale Road.

## Sustainability

### Native Vegetation and Landscaping

55. All vegetation within the site is planted and therefore a planning permit is not required for its removal. Vegetation onsite, however, provides landscape amenity value and requires consideration.



56. The arborist report specifies a number of trees (Tree numbers 23, 24, 148 & 164) will be subject to major encroachment by the proposed buildings and works and requires supervision by an arborist, as well as the retention of two trees (78 and 79) being subject to management techniques.
57. The council has raised concerns regarding the extent of tree removal, and in particular, regarding the potential impact on two street trees (23 and 24). Council has requested works in the vicinity of these trees be supervised by an arborist.
58. A condition of permit will require a tree protection management plan be prepared to ensure all on-site and street trees are protected during construction as well as any ongoing measures implemented.
59. The concerns of council regarding tree removal are acknowledged. The tree removal is considered acceptable, however, as it will facilitate significant improvements to the school and has been avoided where possible. Substantial replanting is proposed, which will assist in retaining a landscaped character to the site.
60. As noted earlier, the applicant has offered additional planting in response to the concerns raised by the council, which will be required as a condition in any permit issued.

### **Environmentally Sustainable Design (ESD) and Water Sensitive Urban Design (WSUD)**

61. A Sustainable Design Assessment (SDA) has been provided which includes sustainability initiatives, including WSUD. The council has not raised any concerns and accordingly a condition of permit will require the measures in the SDA to be implemented.

### **Stormwater Management**

62. The SDA, while including WDUD initiatives, notes the proposal cannot meet best practice requirements within the site. A contribution to the local drainage scheme is instead proposed to offset the additional stormwater runoff resulting from the proposed buildings and works.
63. Council did not comment on the proposed civil plans but provided some draft conditions including the requirement for a stormwater management plan, which will be included as a condition on any permit issued.
64. Subject to the provision of a stormwater management plan that includes the WSUD initiatives included in the SDA, the proposal will result in appropriate stormwater outcomes.

### **Council concerns**

65. Concerns not discussed earlier are addressed as follows:
  - Overlooking from the proposed administration building to the south: The proposed administration building is setback 12.5m from the south boundary and will be screened at least in part by trees and fencing proposed on the south boundary. This setback exceeds the 9m setback that would be required for a residential development in a residential zone – which does not apply to the proposal but is a useful benchmark. Therefore, there are no unreasonable overlooking impacts.
  - Visual bulk, overlooking, noise, light spill and air pollution from bus shelter to the south: The bus shelter structure is considered acceptable, as the plans specify it will include cladding that will obscure overlooking to the south. The trees proposed to be planted along the south boundary will also soften the appearance of the structure over time. It is further noted the 3.3m maximum height of the structure above the existing ground level at a setback of 2.5m, while in an elevated position, would be similar to a dwelling in a residential area.

It is also noted, although not specifically raised as a concern by council, the shadow diagrams provided with the application demonstrate the structure would also not unreasonably overshadow the adjoining land to the south – the overshadowing would be compliant if the Rescode standard was to be applied. In the same respect it is not considered the proposal would unreasonably reduce light to the adjoining land.



The buses will operate only during school hours. While this should mitigate impacts on adjoining properties from vehicle noise, an acoustic report will be required as a condition of any permit issued to demonstrate compliance with the relevant EPA guidelines and noise attenuation measures applied, if necessary.

- Impact and lack of clarity of level changes along south and east boundaries.

While it is considered the plans provide sufficient detail for assessing the proposed buildings and works, further details could be provided in response to this concern showing the proposed earthworks for the accessway and carpark in the southeastern corner of the site adjacent to the sports courts, and the interface with Stanley Road. This will be required as conditions on any permit issued.

The proposed carparking along the south and east boundaries is regarded as acceptable, noting a landscape buffer will be maintained along these boundaries and the applicant has offered to plant additional trees in the Stanley Road nature strip which will be required as a condition on any permit issued.

## Objector concerns

66. Concerns not discussed earlier are addressed as follows:

- Impacts during construction: The council has provided a condition for a construction management plan, which will be included on any permit issued.
- Overlooking: Both proposed buildings will be adequately setback from property boundaries and will be screened in part by boundary fencing and landscaping.
- Noise: It is not considered the proposal will result in unreasonable ongoing noise to the objectors. As discussed earlier, a condition of permit will, however, require an acoustic report be provided to address any additional noise generated from the proposed new bus dropoff area.

## Other Matters

### Staging

67. The proposal shows the proposed buildings and works will be constructed in a staged manner. There does not, however, appear any need to 'tie' permit conditions to specific stages and it has not been requested by the applicant.



# Recommendation



68. The proposal is generally consistent with the relevant planning provisions of the Whitehorse Planning Scheme and will contribute to the provision of secondary school facilities within the area.
69. The proposal is supported by Head, Transport for Victoria and, while not supported by the council, it is considered some concerns can be addressed by permit conditions.
70. It is **recommended** that Planning Permit No. PA2402910 for Buildings and works in association with an existing secondary school at 487-503 Springvale Road Vermont South, be issued subject to conditions.
71. It is **recommended** that the applicant, the council and objectors be notified of the above in writing.

