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PLANNING PERMIT APPLICATION **ADVERTISED PLAN**

69 Carrington Road Box Hill

An architectural rendering of a modern, multi-story building with a glass facade and a curved top. The building is situated on a street corner. The ground floor features a large glass entrance with the words "TIMES SQUARE" above it. The upper floors are visible through the glass, showing interior spaces with people. A sign above the entrance reads "GOLDEN AGE". The surrounding area includes trees, a sidewalk with people, and a parking lot with cars. The sky is clear and blue.

Prepared for
CARRINGTON ROAD MANAGEMENT PTY LTD
19 December 2024

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We acknowledge, in each of our offices, the Traditional Owners on whose land we stand.

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1. EXECUTIVE SUMMARY

This report has been prepared by Urbis Ltd on behalf of the Carrington Road Management Pty Ltd in support of a planning permit application for a proposed multi-level, mixed-use development at No. 69 Carrington Road, Box Hill. The proposal comprises a fifteen (15) storey mixed-use (office and retail) building and associated services and facilities, including a two-level basement car park located on a site within the core of the Box Hill Metropolitan Activity Centre (MAC).

Particular attention has been paid to the urban development potential of the Carrington Road – Cambridge Street precinct and the existing residential precinct on the opposing side of Cambridge Street to the south. Specifically, the proposal has had regard to equitable development considerations relating to the adjacent development proposal currently under assessment at 30 Cambridge Street (WH/2023/553). As such, the proposed development results in an appropriate design and built form outcome that delivers upon the envisaged growth of the Box Hill MAC through higher building forms and a diversity of land uses.

The Victorian State Government has released planning reforms in September 2023, with the desire to increase housing provision throughout Victoria and stream-line significant economic developments. This permit application is submitted to the Department of Transport and Planning (DTP) pursuant to the 'Significant Economic Development' provisions set out in Clause 53.22 of the Whitehorse Planning Scheme.

This report outlines the proposal in detail, discusses its merits and provides an assessment against the relevant planning controls of the Whitehorse Planning Scheme. The site is located within the Commercial 1 Zone (C1Z) and is subject to the Parking Overlay – Schedule 1 (PO1).

The proposal seeks:

- To construct a fifteen (15) storey mixed-use development comprising:
 - A Gross Floor Area of 13,495 square metres.
 - Three (3) retail tenancies of 1,396 square metres of Net Floor Area.
 - Thirteen (13) office space levels of 6,646 square metres of Net Sellable Area.
 - A two-level basement carpark layout accommodating forty-one (41) car parking spaces, including:
 - Thirty-eight (38) car parking spaces for the office spaces, inclusive of one (1) DDA access bay.
 - Three (3) car parking spaces for the retail tenancies.

Overall, the proposal is an appropriate and acceptable outcome for the site with substantial public benefits.

The proposal is considered appropriate for the following reasons:

- The proposal demonstrates a high level of consistency with the Whitehorse Planning Policy Framework and planning controls, including the relevant local policy for office and retail uses.
- The proposal will positively contribute to the land use and development of the Box Hill MAC, particularly through the co-location of office and retail uses in a higher-density, tower form to support the Box Hill MAC's urban growth as the major focus for mixed-use development in Melbourne's eastern region.
- The built form design presents a balanced response to the existing conditions and will contribute to the revitalisation of the site and improved outcomes for Carrington Road and Cambridge Street. The architectural response successfully provides a visual and functional transition between the evolving commercial properties of Carrington Road and the residential areas to the south of Cambridge Street.
- The proposed design response has been carefully considered ensuring that a high level of internal amenity is afforded to the future occupants, whilst external amenity impacts to adjoining residential properties by way of visual bulk, overshadowing and equitable development are minimised.
- The proposal has appropriately considered its design response with regard to equitable development considerations of surrounding properties, and particularly to the proposed development at 30 Cambridge Street, ensuring a high quality urban design outcome.
- The proposed environmental and building services and performance considerations are well-resolved. The proposal provides for a high-quality delivery of landscape, sustainable management, waste management, wind, and traffic conditions.

- The proposed car parking provision seeks a reduction to statutory car parking requirements where a reduction in car parking already exists on the existing site, sufficient car parking exists in the Activity Centre area and where there is a diverse choice of public transport modes, which is consistent with the objectives of Clause 52.06.

1.1. RESPONSE TO OVGA

Since the original submission of the application on 13 December 2023, the proposal has implemented several design changes in response to the comments received by the OVGA upon the initial lodgement of the application.

These changes are summarised below:

- **Introduction of Eastern setback:** A tower setback of 4.5 metres has been introduced along the eastern boundary to respond to equitable development opportunities of any future development at No. 67 Carrington Road and to improve internal amenity outcomes for future occupants of the building.
- **Adjustment of podium street wall height:** The podium street wall has been reduced to 16.2 metres at the Carrington Road interface and 18.09 metres to the Cambridge Street interface commensurate the building height of the adjoining Telstra building at No. 75 Carrington Road.
- **Refined architectural expression to the podium:** The podium shroud has been refined whilst maintaining the original design intent and language of the building and achieving a perceived reduction in podium height without compromising visual expression.
- **New core and services location:** To maximise internal amenity provided by the 4.5 metre tower setback to the east, the lift core and services have been mirrored to the western side of the development.
- **Redesigned pedestrian link:** The pedestrian thoroughfare has been repositioned to the eastern side of the property boundary and redesigned to be step-free to ensure a highly accessible and barrier-free transition from Carrington Road to Cambridge Street.
- **Improved eastern elevation:** Leveraging the new setback, the eastern elevation has undergone a transformative detailed design enhancement, featuring expansive glazing and verdant terraces on the podium level, improving both visual appeal and integration with the streetscape.
- **Upgraded ground floor interface:** The ground-level interface facing Cambridge Street has been ameliorated using new materiality and access arrangements.

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2. INTRODUCTION

This report has been prepared by Urbis Ltd on behalf of the Carrington Road Management Pty Ltd in support of a planning permit application for a proposed multi-level, mixed-use development at No. 69 Carrington Road, Box Hill. The proposal comprises a fifteen (15) storey mixed-use (office and retail) building and associated services and facilities, including a two-level basement car park located on a highly prominent site in the Box Hill Metropolitan Activity Centre (MAC).

2.1. PERMIT APPLICATION

This town planning report addresses the planning merits of the proposal for No. 69 Carrington Road, Box Hill and its consistency with the relevant planning controls and policies of the Whitehorse Planning Scheme. It is supported by the accompanying Architectural Plans and Urban Context Report prepared by Gray Puksand which detail the proposed building program and architectural design.

This detailed assessment of the proposal should be read in conjunction with the following information:

- Architectural Plans and Urban Context Report prepared by Gray Puksand, Revision TP3 dated 19 December 2024
- Urban Context Report prepared by Urbis Ltd, dated 19 December 2024
- Feature Survey prepared by Reeds Consulting, dated 8 December 2023
- Landscape Design Response prepared by ASPECT Studios, dated 19 December 2024
- Environmental Wind Assessment prepared by MEL Consultants, dated 17 May 2024
- Traffic Impact Assessment prepared by Traffix Group, dated December 2024
- Waste Management Plan prepared by Traffix Group, dated December 2024
- Sustainable Management Plan prepared by WRAP, dated 19 December 2024
- MPL26498 issued 20 August 2024
- Certificate of Title

Collectively, the above documents and this report confirms that the proposed mixed-use development presents an appropriate land use and built form outcome from an urban design, equitable development, socio-economic, built form, environmental, landscape, and amenity perspective.

For the reasons discussed within this report, we submit that the proposed use and development presents as a well-considered design response for this site which will sit comfortably within the Box Hill MAC.

2.2. PLANNING CONTROLS

The subject site is affected by following Planning Zone and Planning Overlay controls:

- Commercial 1 Zone (C1Z)
- Parking Overlay – Schedule 1 (Box Hill Activity Centre) (PO1)

Further detail of these planning controls can be found at **Appendix A & B** of this report.

2.3. PLANNING PERMIT TRIGGERS

Subject to the provisions of the Whitehorse Planning Scheme a planning permit is required for the following:

- To construct a building and carry out works pursuant to the Commercial 1 Zone (**Clause 37.04-4**).
- To reduce the statutory car parking provision for the office and retail use (**Clause 52.06-3**).

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3. SUBJECT SITE AND SURROUNDS

3.1. SUBJECT SITE

The subject site is located on the southern boundary of Carrington Road and the northern boundary of Cambridge Street in Box Hill, approximately 250 metres west of Station Street and 80 metres east of Thurston Street. The site is located within the Box Hill MAC, forming part of the major development precinct spanning either side of Whitehorse Road. The site is designated within the Commercial 1 Zone (C1Z) as part of a large-scale commercial precinct spanning either side of Box Hill railway line.

The site is located 150 metres south-west of the Box Hill Railway Station, which services connections between Melbourne's CBD and Belgrave (Belgrave Line) and Melbourne's CBD and Lilydale (Lilydale Line). The site is also located 300 metres south-west of Tram Route 109 servicing connections between Box Hill and Port Melbourne, and 200 metres south-west of eighteen (18) bus routes at the Box Hill Bus Interchange. As such, the site is recognised as part of the Principal Public Transport Network (PPTN) and well-placed to optimise public transport connections between Box Hill and various locations across Victoria.

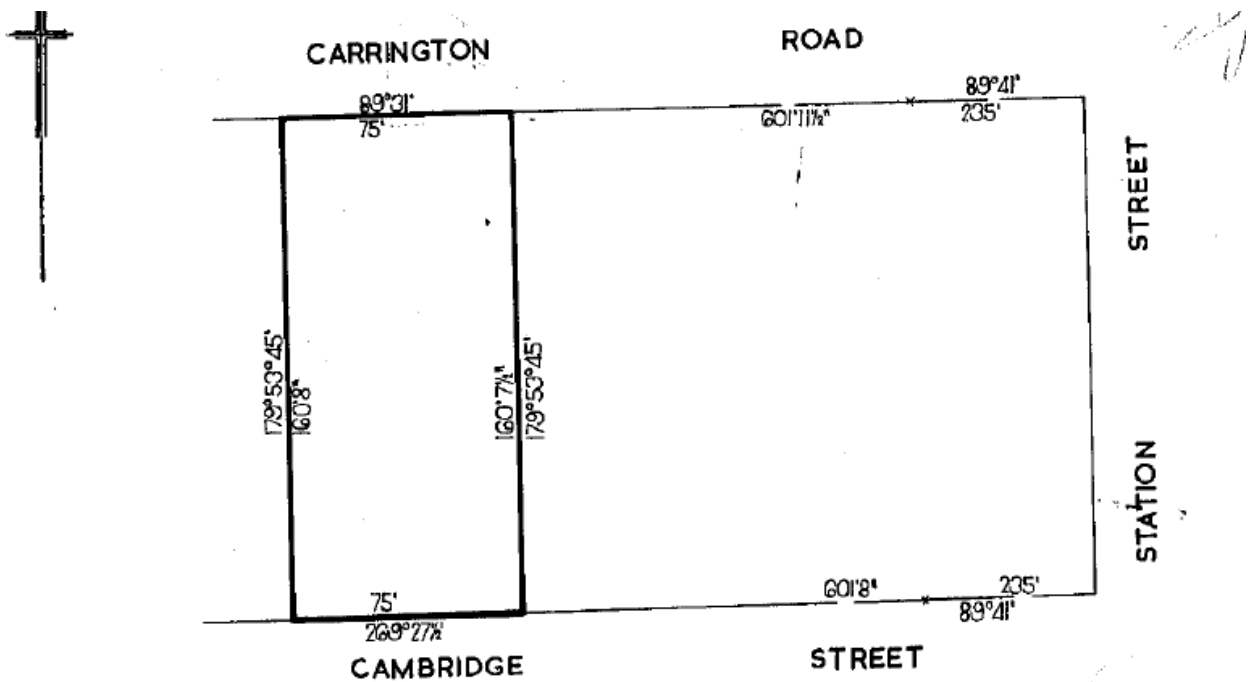
The site is rectangular in shape, with a primary frontage to Carrington Road of 22.8 metres, a secondary frontage to Cambridge Street of 22.8 metres and a depth of 48.9 metres. The total site area measures approximately 1,119 square metres.

The site is formally contained within one (1) Certificate of Title, being:

- Land in Plan of Consolidation 100283.

There are no encumbrances on the Title Plan that will affect the proposed use and development on the land.

Figure 1: Certificate of Title Plan



Source: State of Victoria, 2023

The site currently accommodates vacant land and is cleared of any vegetation. Historically, the site was occupied by a single-storey commercial building with a zero-level setback to the western boundary and an at-grade car parking area to the eastern boundary with thirteen (13) car parking spaces.

The site currently provides two (2) vehicle crossovers, including one (1) double width vehicle crossover to the north-west of the Carrington Road frontage and one (1) double width vehicle crossover to the south-east of the Cambridge Street frontage. There is an existing street tree to the Cambridge Street frontage.

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Figure 2: Aerial Imagery of the Subject Site and Surrounds



69 CARRINGTON ROAD, BOX HILL SITE LOCATION

Source: Urbis Pty Ltd, 2024

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3.2. SITE INTERFACES

The immediate site interfaces are described as follows:

3.2.1. Northern Interface – Carrington Road

The site's northern interface abuts Carrington Road, a collector road accommodating one-way traffic (eastbound) spanning between Thurston Street in the west and Station Street in the east. Carrington Road provides on-site car parking spaces either side of the central thoroughfare. Carrington Road is considered a primary pedestrian link pursuant to the *Box Hill Structure Plan (2007)*.

Adjoining the site to the north is the two-storey Box Hill Central Shopping Centre which provides a large-scale, multi-storey car park adjacent to the site. The commercial properties forming part of the Box Hill Shopping Centre precinct are designated within the Commercial 1 Zone (C1Z).

Figure 3: Northern Interface – Carrington Road



Source: Gray Puksand, 2024



Source: Gray Puksand, 2024

3.2.2. Southern Interface – Cambridge Street

The site's southern interface abuts Cambridge Street, a collector road accommodating one-way traffic (westbound) spanning between Station Street in the east and Thurston Street in the west. Cambridge Street provides on-site car parking spaces either side of the central thoroughfare.

Adjoining the site to the south are two-storey townhouses zoned within the Residential Growth Zone – Schedule 2 (RGZ2). The *Box Hill Structure Plan (2007)* seeks a four (4) storey preferred height limit for the residential precinct.

Figure 4: Southern Interface – Cambridge Street



Source: Gray Puksand, 2024



Source: Gray Puksand, 2024

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3.2.3. Western Interface – Telstra Exchange Building

The site's western interface abuts No. 75-77 Carrington Road, and No. 30 Cambridge Street, Box Hill. No. 75-77 Carrington Road, Box Hill, accommodates the Telstra Exchange Building, a four-storey commercial structure with a pitched roof form for the purposes of telecommunication services. No. 30 Cambridge Street, Box Hill, accommodates an at-grade car park fronting the Telstra Exchange Building. The land currently provides a building separation of 3 metres to the intermediate boundary.

The land at 30 Cambridge Street is currently subject to a planning permit application WH/2023/553 lodged for assessment by Whitehorse City Council. The proposal is characterised as an eleven (11) storey mixed use office building, with a traditional podium and tower formation. Prudently, the concept development plans nominate a 4.5 metre side tower setback to the subject site's common boundary.

The land abutting the western interface and the adjoining Box Hill Senior Citizens Centre at No. 79-81 Carrington Road are designated within the Commercial 1 Zone (C1Z). Adjoining the commercial strip to the west are residential properties zoned within the Residential Growth Zone – Schedule 1 (RGZ1).

Figure 5: Western Interface – Telstra Exchange Building & 30 Cambridge Street



Source: Gray Puksand, 2024



Source: Gray Puksand, 2024

Figure 6: Proposed Development WH/2023/553 – 30 Cambridge Street



Source: Cera Stribley Architects, 2024

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3.2.4. Eastern Interface – No. 65-67 Carrington Road, Box Hill

The site's eastern interface abuts No. 65-67 Carrington Road, Box Hill, a two-storey commercial development with pitched roof forms for the purposes of retail uses. No. 65-67 Carrington Road, Box Hill, is currently built to the intermediate site boundary.

The land abutting the eastern interface and the adjoining eastern properties are designated within the Commercial 1 Zone (C1Z). The eastern properties occupy similar narrow north-to-south lot sizes to the site.

Figure 7: Eastern Interface – No. 65-67 Carrington Road, Box Hill



Source: Gray Puksand, 2024



Source: Gray Puksand, 2024

3.3. SURROUNDING AREA

The subject site is located within the commercial core of the Box Hill MAC. The Box Hill MAC continues to experience substantial urban transformation into a regional centre in Melbourne's east with a focus on office, retail, medical, educational, higher-density residential, and hospitality land uses. The emerging urban form continues to be characterised by new development of up to 51 storeys, generally in podium-tower formats with curved facades and glazed finishes.

The surrounding residential area outside the Activity Centre's core is characterised by low-scale residential development in the form of detached dwellings and multi-unit developments, generally ranging between one and four storeys within landscaped settings. As such, the purpose of the Box Hill MAC is to equip the residential hinterlands with excellent access to a range of existing services and facilities. We note that recent approvals have been predominantly residential developments, necessitating the need for additional retail and office spaces to ensure local residents have employment opportunities within a 20-minute trip of their home.

The site is also well-supported with excellent access to the Principal Public Transport Network, noting its proximity to the Route 109 Tram traversing Whitehorse Road, and bus and rail services running from the Box Hill Railway Station and Box Hill Bus Interchange. The subject site will also be proximate to the future Suburban Rail Loop (SRL) Box Hill Station, enhancing access to and from the site.

The site's intensification offers efficient use of existing transport, infrastructure, and services, being located:

- Adjacent to the Primary Pedestrian Network along Carrington Road.
- Approximately 20 metres south of the Box Hill Central Shopping Centre.
- Approximately 150 metres south-west of the Box Hill Railway Station.
- Approximately 150 metres north-west of Brougham Park.
- Approximately 200 metres south-west of the Box Hill Station Bus Interchange.
- Approximately 275 metres north-west of Harrow Park.
- Approximately 300 metres south-west of Tram Route 109 along Whitehorse Road.

- Approximately 350 metres south-west of the Australian Taxation Office Building.
- Approximately 700 metres south-east of the Box Hill Hospital.

As such, several properties proximate to the site have achieved planning approval for high-density, infill development in tower forms, including along Carrington Road and Cambridge Street.

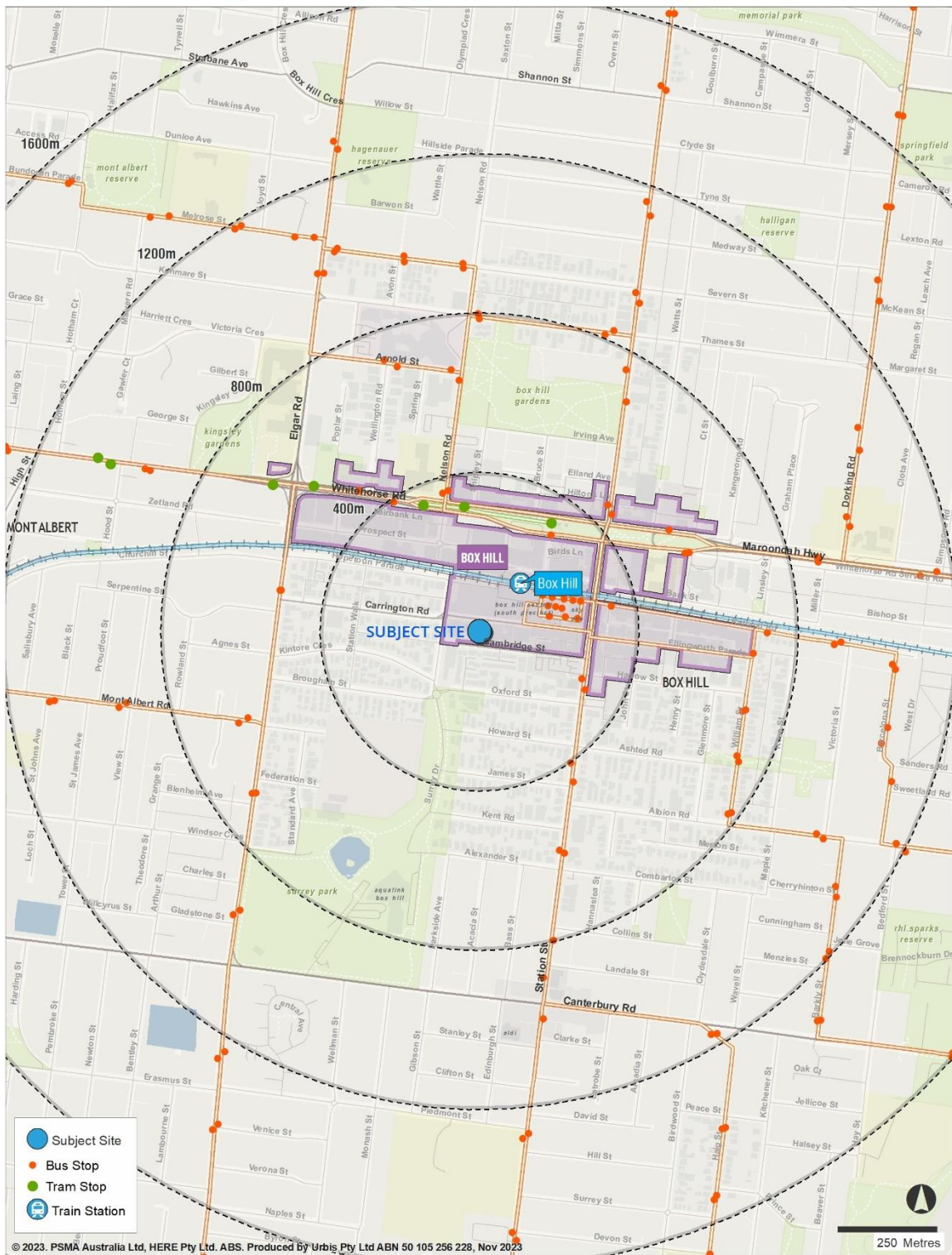
For instance, the following high-density, mixed-use developments have been approved near the site:

- Planning Permit No. WH/2016/1196/B was approved 9 March 2022 at 517-521 Station Street, Box Hill, allowing the construction of an eighteen (18) storey mixed-use development including retail uses and 431 dwellings. The approved development also allowed for a significant car parking reduction on the land.
- Planning Permit No. WH/2019/1332 was approved 9 March 2021 at 25-29 Prospect Street, Box Hill, allowing the construction of a twenty-three (23) storey building including 150 dwellings.
- Planning Permit No. WH/2019/200 was approved 5 April 2022 at 941-951 Whitehorse Road, Box Hill, allowing the construction of a fifteen (15) storey commercial building.
- Planning Permit No. WH/2020/1307 was approved 10 February 2022 at 6-10 Shipley Street, Box Hill, allowing the construction of a twenty-two (22) commercial development.
- Planning Permit No. WH/2020/597 was approved 23 May 2022 at 17-21 Market Street, Box Hill, allowing the construction of a fifty-one (51) storey mixed-use development including office and retail space.
- Planning Permit No. WH/2018/193 was approved 9 March 2021 at 2-4 Bruce Street, Box Hill, allowing the construction of a nineteen (19) storey building including office space and 235 apartments.
- Planning Permit No. WH/2011/986 was approved 6 July 2017 at 545 Station Street, Box Hill, allowing the construction of a thirty-four (34) storey building including office space and 434 dwellings.

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Figure 8: Public Transport and Activity Centre Map



69 CARRINGTON ROAD, BOX HILL ACTIVITY CENTRES & PUBLIC TRANSPORT

Source: Urbis Pty Ltd, 2024

4. PLANNING POLICY FRAMEWORK

The site is subject to the provisions of the Whitehorse Planning Scheme. The following summary outlines the key planning policies and controls that affect the proposed development.

A full list of these planning controls, as well as policies, objectives, requirements relevant to the proposed development have been included at **Appendix A & B** of this town planning report.

4.1. PLANNING CONTROLS

The subject site is affected by following Planning Zone and Planning Overlay controls:

- Commercial 1 Zone (C1Z)
- Parking Overlay – Schedule 1 (Box Hill Activity Centre) (PO1)

4.2. PARTICULAR & GENERAL PROVISIONS

The following particular provisions of the Whitehorse Planning Scheme apply to the site:

- Clause 52.06 – Car Parking
- Clause 53.22 – Significant Economic Development
- Clause 52.34 – Bicycle Facilities
- Clause 65 – Decision Guidelines
- Clause 72.01 – Responsible Authority

4.3. RELEVANT PLANNING POLICY

There are several relevant strategic documents, as well as state and local planning policies that have been considered when assessing the proposed development. These include:

Strategic Documents

- Plan Melbourne 2017
- Box Hill Transit City Activity Centre Structure Plan 2007
- Box Hill Metropolitan Activity Centre to 2036
- Suburban Rail Loop Precinct Vision for Box Hill, December 2024

Planning Policy Framework

- Clause 11 - Settlement
- Clause 11.03-1R – Activity Centres – Metropolitan Melbourne
- Clause 11.03-1L – Activity Centres
- Clause 11.03-1L-02 – Box Hill Metropolitan Activity Centre
- Clause 15 – Built Environment and Heritage
- Clause 15.01-1S – Urban design
- Clause 15.01-2L – Environmentally sustainable development
- Clause 17 - Economic Development
- Clause 18.02-3R - Principle Public Transport Network
- Clause 18.02-4L - Car Parking

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5. PROPOSED DEVELOPMENT

Figure 9: Proposed Mixed-use Development



Source: Gray Puksand, 2024

5.1. MIXED-USE DEVELOPMENT

The proposal seeks approval for the construction of a fifteen (15) storey, mixed-use building comprising a gross floor area of 13,495 square metres. At a finer-grain level, the development proposes approximately 1,396 square metres of retail space and 6,646 square metres of office space, with an associated two-level basement carpark accessed via a car lift.

Detailed plans of the proposal have been prepared by Gray Puksand and are submitted as part of this application. Gray Puksand seek to create a market leading and sustainable mixed-use development that responds to the urban potential of the Carrington Road – Cambridge Street precinct which forms part of the Box Hill Major Activity Centre (MAC). The proposed built form provides a contemporary response to office accommodation, delivering a high-quality architectural design that respects the significance of the existing buildings and the emerging character of the region.

The architectural response seeks to provide a visual and functional connection between the evolving commerce of Carrington Road and the residential areas to the south of Cambridge Street. At podium-level, the proposal prioritises street activation and connects pedestrian movement between the street interfaces along a public pedestrian link permeating through the ground plane. At tower-level, the proposal provides a transitional building height stepping down from the northern commercial buildings to the southern residential buildings.

Please refer to the Architectural Plans and Urban Context Report prepared by Gray Puksand dated 21 May 2024 for further details.

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5.2. DEVELOPMENT SUMMARY

The key aspects of the proposed development are summarised in the table below:

Table 1: Development Summary

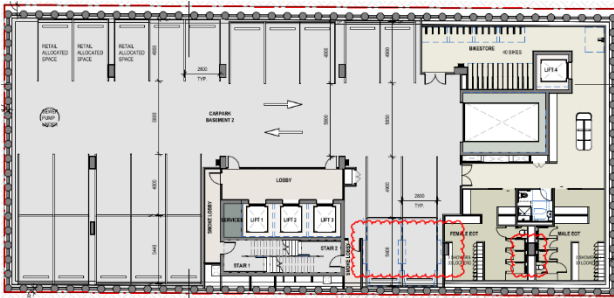
Design Component	Proposed
Proposed Uses	<ul style="list-style-type: none"> ▪ Retail: Three (3) retail tenancies (Ground Level – Level 1) ▪ Office: Thirteen (13) levels of office space (Level 2 – Level 14)
Site Area	<ul style="list-style-type: none"> ▪ 1,119 m²
Site Coverage	<ul style="list-style-type: none"> ▪ 100%
Development Area	<ul style="list-style-type: none"> ▪ Gross Floor Area (GFA): 13,495m² ▪ Basement Area (GFA): 2,238m² ▪ Net Sellable Area (NSA): 8,042m² <ul style="list-style-type: none"> – Office Net Sellable Area (NSA): 6,646m² – Retail Net Floor Area (NFA): 1,396m²
Building Height	<ul style="list-style-type: none"> ▪ 15 Levels ▪ 52.56 metres, measured to roof level. ▪ 56.7 metres, measured to roof services.
Street Wall Height	<ul style="list-style-type: none"> ▪ Carrington Road: Four Levels (16.20 metres) ▪ Cambridge Street: Four Levels (18.09 metres)
Terrace and Landscaped Areas	<ul style="list-style-type: none"> ▪ 644m²
Car Parking	<ul style="list-style-type: none"> ▪ 41 car parking spaces <ul style="list-style-type: none"> – Office: 37 car parking spaces – Retail: 3 car parking spaces – DDA: 1 car parking space
Bicycle Parking	<ul style="list-style-type: none"> ▪ 50 bicycle parking spaces <ul style="list-style-type: none"> – Staff: 40 bicycle parking spaces – Visitor: 10 bicycle parking spaces
End of Trip (EOT) Facilities	<ul style="list-style-type: none"> ▪ 7 shower facilities <ul style="list-style-type: none"> – 6 Male & Female Showers – 1 Unisex DDA shower ▪ 68 lockers <ul style="list-style-type: none"> – 60 male & female lockers – 8 unisex lockers

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5.3. BASEMENT LEVELS

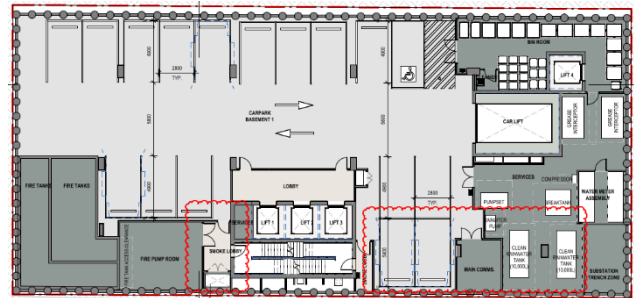
The proposal contains a two-level basement car park with forty-one (41) car parking spaces and fifty (50) staff bicycle spaces. The basement level 2 area accommodates End of Trip (EOT) facilities, including seven (7) shower facilities across three (3) changing room areas. The basement level 1 area accommodates a waste storage area of 64 square metres, and subsequent building services. Vehicle access to the basement car parking areas will be provided from a car lift from Cambridge Street, whilst pedestrian access is provided via lifts and stairwells from the central lobby and Cambridge Street frontage.

Figure 10: Basement Levels



Picture 1: Basement Level 2

Source: Gray Puksand, 2024



Picture 2: Basement Level 1

Source: Gray Puksand, 2024

5.4. STREET LEVEL

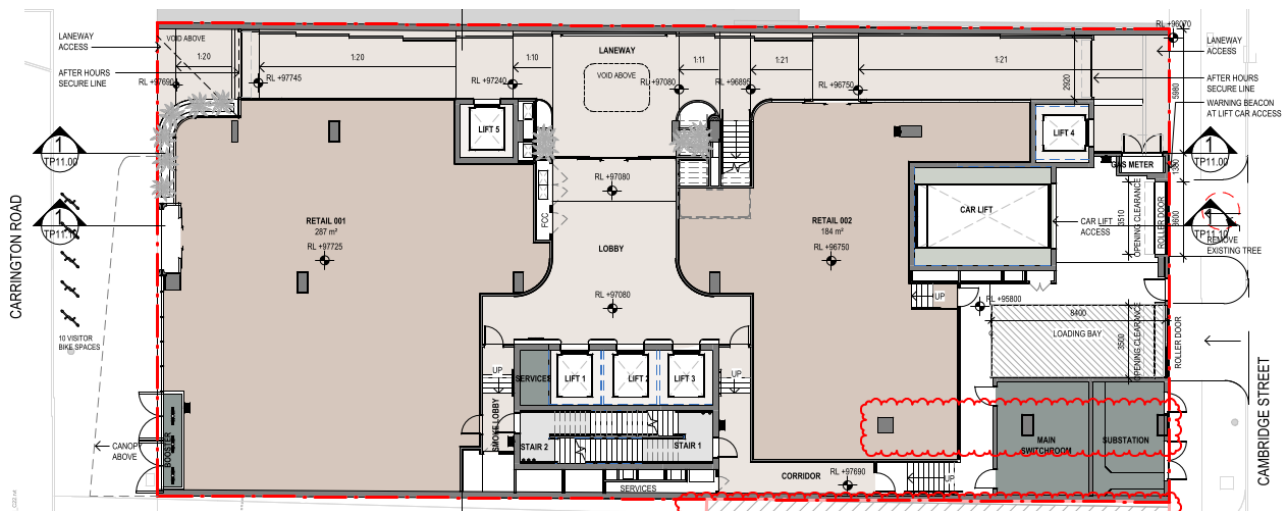
At the street level, the ground floor expands the current ground plane experience, inviting the public into the site with a central lobby along a new public pedestrian link between Cambridge Street and Carrington Road.

Two retail tenancies will be located either side of the central lobby, including a retail tenancy of 287 square metres fronting Carrington Road and a retail tenancy of 184 square metres to the centre of the site. Back of House (BOH) services will be located to the south-western corner of the site alongside the loading bay and car lift to Cambridge Street.

Ten (10) visitor bicycle parking spaces will be provided to the ground-level via on-street bicycle parking spaces on the Carrington Road frontage. Landscaping opportunities will be provided adjacent to the pedestrian entryways and central lobby entrance to clearly identify and direct wayfinding between the pedestrian access points.

The existing vehicular crossover located along Cambridge Street will be re-located to provide access to the car lift. The vehicle crossover will have a 3.5 metre opening clearance at the building entrance.

Figure 11: Street Level



Source: Gray Puksand, 2024

5.5. PODIUM LEVELS

At podium-level, a four-storey street wall will be provided to Carrington Road and Cambridge Street, with zero-level side setbacks to the adjacent properties. The podium elements contribute to the stepped-down built form approach, transitioning from the adjoining northern commercial properties to the southern residential areas. The upper-levels above the street-walls will be setback from the street edges to provide additional emphasis of this transition.

Within the podium element, a retail tenancy of 924 square metres will be provided to Level 1. The balance of the podium will accommodate several office tenancies containing 858 square metres of net lettable area per floor. The podium office tenancies provide future occupants with proximate access to outdoor terrace areas of 14 square metres with commensurate landscaping opportunities.

At the roof of the podium, a total outdoor terrace space of 370 square metres around all elevations will cap the podium level providing respective tenancies opportunity for outdoor spaces improved with substantial landscaping.

5.6. TOWER FORM

Within the tower form, office tenancies complete the balance of the development between Level 4 and Level 14. The upper-level office spaces continue to enjoy proximate access to outdoor terrace areas with landscaping opportunities, varying between 38 square metres and 51 square metres in size.

At roof-level, an overall crown feature is incorporated to the Carrington Road frontage to complete the development as a sculptural element from afar. To the Cambridge Street interface, the tower elements progressively step-back to channel light and air toward the adjoining residential properties to the south. The tower form adopts an expression of vertical and horizontal elements with an interplay of grey tone glazing elements and light-coloured concrete to deliver a unified, light-weight architectural composition.

Figure 12: Podium & Tower Forms



Picture 3: Cambridge Street

Source: Gray Puksand, 2024



Picture 4: Carrington Road, north-east perspective

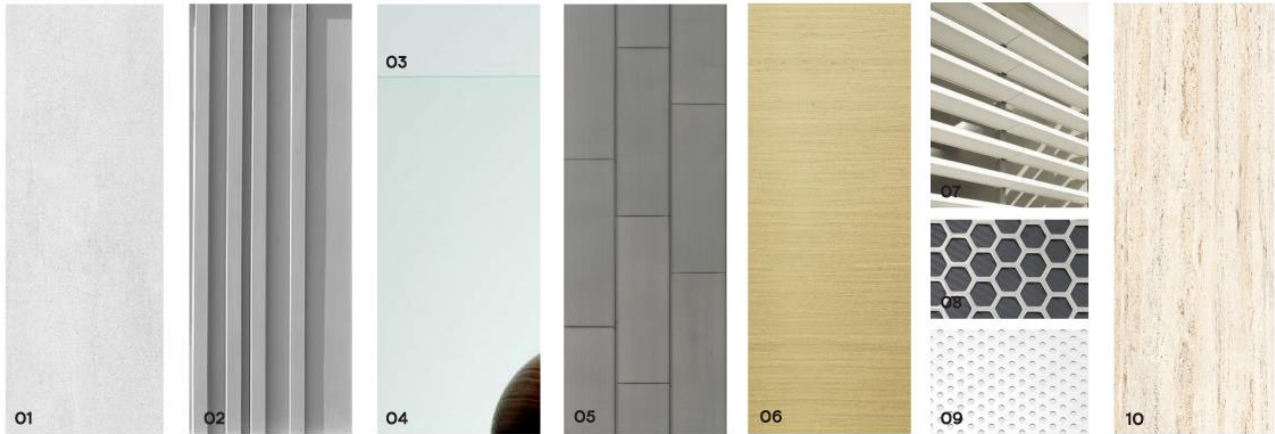
Source: Gray Puksand, 2024

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5.7. FAÇADE AND MATERIALITY

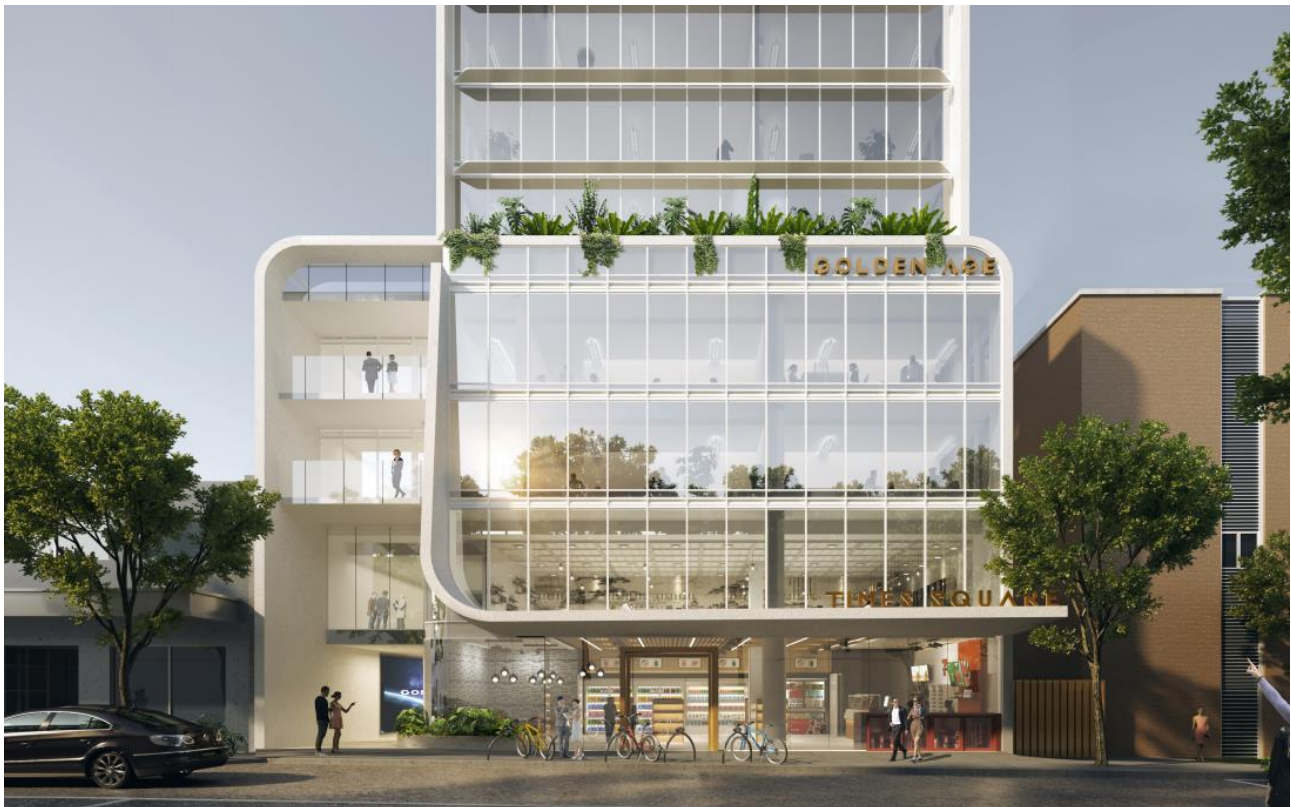
Proposed materiality incorporates light-coloured reinforced concrete and grey tone glazing elements, interspersed with light and dark tone louvre and metal elements. The palette expresses as smooth, contemporary style materials, providing a lightweight feel to provide visual interest from the street perspective whilst limiting the visual presence when viewed from the outlook of neighbouring properties.

Figure 13: Material Schedule



Source: Gray Puksand, 2024

Figure 14: Carrington Road Street Frontage



Source: Gray Puksand, 2024

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6. PLANNING ASSESSMENT

Based on the site's planning controls and urban context, the key considerations relevant to this proposal are:

- Planning policy support for the proposal (**Section 6.1**)
- Appropriateness of the Proposed Uses (**Section 6.2**)
- Built Form Considerations (**Section 6.3**)
- External Amenity Considerations (**Section 6.4**)
- Internal Amenity Considerations (**Section 6.5**)
- Environmental Considerations (**Section 6.6**)
- Building Services and Performance (**Section 6.7**)

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This following section includes a considered response to the policy directions and controls of the Whitehorse Planning Scheme. The assessment confirms that the proposal satisfies the principal aims and requirements of all the relevant policy frameworks and controls.

6.1. PLANNING POLICY

There is clear support for proposal as demonstrated in the Planning Policy Framework of the Whitehorse Planning Scheme, with the following demonstrating the consistency with the relevant Clauses:

- A key objective of Plan Melbourne is to facilitate a consolidated urban form, where new jobs are delivered in a targeted manner. As one of nine existing Metropolitan Activity Centres (the highest order of Activity Centre outside Melbourne's Central City) and one of two MAC's in the Eastern Subregion, Plan Melbourne clearly envisages Box Hill as a focus for substantial growth in terms of commercial activity and employment opportunities.
- At a local level, the *Box Hill Structure Plan (2007)* establishes strategic support for intensified built form and activity, which is enacted through the provisions of the Whitehorse Planning Scheme. Located within the Box Hill MAC, the proposal represents the redevelopment and intensification of an existing landholding resulting in a greater mix of uses that take advantage of the site's excellent connectivity and accessibility in accordance with Clause 11.
- The broad principle of a multi-level mixed use building in this location is consistent with the provisions of Clause 02.03-7 'Economic Development', which recognises the 'significant opportunities' afforded to Box Hill in terms of its community, commercial, cultural and residential focuses. The proposed development provides mixed use development through the inclusion of retail and office uses. This is consistent with Clause 02.03-1, which seeks to develop the Box Hill MAC for opportunities in retail, entertainment, higher density residential, commercial, health, and civic uses.
- The review site is well-located near higher density residential development owing to its proximity to commercial, community and recreational services and facilities within the Box Hill MAC, many of which are within walking distance of the site. This is responsive to the 'Objectives' of Clause 11.03-1L-02 which encourage walking as the primary means of transport within the Activity Centre.
- The ground and lower ground level retail precinct will further enhance the retail offering within the activity centre. The proposed public pedestrian link will introduce a new form of public realm to Box Hill and act as 'destination hub' to encourage pedestrian movement between Cambridge Street and Carrington Road, consistent with Clause 11.03-1L-02.
- The proposed built form is consistent with the site's location in the 'Built Form Precinct F: Major Development Precinct' which supports taller built form at 69 Carrington Road, Box Hill (Clause 11.03-1L-02 'Box Hill Activity Centre'). The built form also steps down to the adjoining residential properties.
- The proposed built form is consistent with the site's partial location in Activity Use Precinct A 'Box Hill Transport & Retail Precinct' to the north, which envisages 'retail sustained throughout the precinct complemented by entertainment, commercial and other uses' and 'intense mixtures of activity over extended hours'.

- The proposed built form is consistent with the site's partial location in Activity Use Precinct F 'Southern & Eastern Precincts' to the south, which seeks 'accommodation of growth in the local supply of office space' and 'continuation of the precinct's role in supporting some retail activity'.
- The proposed development provides a progressive transit response to minimise the use of private motor vehicles, opting to provide a car parking reduction in a development where sufficient car parking exists in the area and where there is a diverse choice of public transport modes (Clause 18.01-3R). As such, the proposed development is also supportive of active transport mode trips, particularly given the site's proximate location adjacent to a primary pedestrian link along Carrington Road. The proposed development affirms this position by exceeding the bicycle parking provision with a surplus of twenty-four (24) bicycle parking spaces (Clause 18.01-3R).

6.2. APPROPRIATENESS OF PROPOSED USES

The Victoria State Government have recently released planning reforms, with the desire to increase housing provision throughout Victoria and stream-line significant economic developments. The State Government identifies Metropolitan Activity Centres as a nominated area for higher-density residential and office developments. However, recent development approvals in the Box Hill MAC have focused on residential developments, necessitating the need for additional retail and office spaces to ensure local residents have employment opportunities within a 20-minute trip of their home.

The proposal is consistent with the purpose of the C1Z as it contributes to vibrant mixed use commercial centres for retail, office, business, entertainment, and community uses. Specifically, the proposal incorporates three (3) new retail tenancies and thirteen (13) levels of office floor space. It is noted that each use proposed by the development is as-of-right under the controls of the Commercial 1 Zone (C1Z), which demonstrates that such uses are encouraged in this location.

The uses have been arranged to ensure the office uses benefit from views and amenity afforded at the upper levels. While the more accessible retail use is proposed within the podium levels, providing street activation at the lower levels. The policy frameworks provide strong support for a sustained increase in the provision of retail and office uses to complement the mixed-use function of the Box Hill MAC and increase the diversity of workers and visitors within the area.

6.3. BUILT FORM CONSIDERATIONS

The site's location within the centre of the Box Hill MAC presents an excellent opportunity to provide greater intensity of development on a site that is currently underutilised. The proposal will result in a greater level of interest, activity and vibrancy in Box Hill and contribute to the growth of the Metropolitan Activity Centre.

The built form design presents a balanced response to the existing conditions and will contribute to the revitalisation of the site and improved amenity of Carrington Road and Cambridge Street. The architectural response seeks to provide a visual and functional connection between the evolving commercial properties of Carrington Road and the residential areas to the south of Cambridge Street.

6.3.1. Street Frontages and Ground Floor Plane

The proposed development is designed to continue the retail experience along Carrington Road and presents an appropriate portal to the activity centre from the residential properties to the south by providing a pedestrian link between Cambridge Street and Carrington Road.

To Carrington Road, the proposal adopts a wide expanse of clear glazing which provides a robust contribution to street level activation along Carrington Road and enables a visual connection to the ground floor retail tenancy. The architectural language ensures that the ground floor retail use retains visual prominence when viewing the development from the public realm whilst the location of the pedestrian link adjacent to the eastern boundary will provide an appropriate visual break between the adjoining retail use at 67 Carrington Road.

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Figure 15: Carrington Road Ground Plane



Source: [Gray Puksand, 2024]

A large portion of the Cambridge Street frontage is occupied by building services and vehicle entries to the site, with the balance of the frontage at the ground plane containing the pedestrian link which facilitates through block pedestrian movement along with activation. Where possible, the services and vehicle entries have been treated with materiality and landscaping to soften their appearance and provide activation to the facade. In addition, the enclosing walls which flank the pedestrian link will be constructed using off-white pre-cast concrete which will contrast well to the darker metal cladding proposed to the remainder of the Cambridge Street frontage, placing visual emphasis on the entrance to the pedestrian link from the public realm.

The pedestrian link itself remains at-grade and includes a gradient ranging from 1:10 to 1:20 ensuring a high level of accessibility. Internally, the pedestrian link will provide public access to a luxury retail experience that contributes to the diversity of retail offerings within the local context. The two ground floor retail uses will be constructed with a high extent of glazing to the pedestrian link to promote passive surveillance, continue the street level activation along Carrington Road and to create a three dimensional retail experience.

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Figure 16: Cambridge Street Ground Plane



Source: [Gray Puksand, 2024]

6.3.2. Building Height

6.3.2.1. Maximum Building Height

The proposed development involves a fifteen (15) storey maximum building height of 52.56 metres to roof level and 56.7 metres to the top of the roof services. The *Box Hill Structure Plan (2007)* seeks major development on the site where taller buildings are permitted to increase density. It is acknowledged that *the Structure Plan* merits building heights on their capacity to minimise overshadowing to public spaces and residential areas, and the level of transition between higher-order and lower-order precincts at the edges of the activity centre.

The maximum building height has been carefully considered to address the primary frontage to Carrington Road with a setback distance of 4.5 metres. Alternatively, upper levels are progressively stepped back from the Cambridge Street interface, with the maximum building height set back at a distance of 24.5 metres. Therefore, the proposed development provides for a transition in the overall building height from the northern commercial properties to the southern residential areas.

The proposed maximum building height of 52.56 metres will fit comfortably in its context when compared to other buildings in the immediate surrounds. Other recent tower development approvals within close vicinity, such as 517-521 Station Street, Box Hill, have allowed for building heights up to eighteen (18) storeys or 64.3 metres that address Cambridge Street within the 'Built Form Precinct F'. As such, the proposed height of the development reinforces the robust built form character sought for the area, allowing the proposed building to sit comfortably at the southern end of the Box Hill MAC.

The proposed height has been shaped by the built form policy direction of the Box Hill MAC, particularly overshadowing, and the emerging and anticipated surrounding context of high-rise buildings. Responding to the size of the site, while being carefully articulated, ensures the building height develops a contextual relationship with its surroundings, alleviating visual impacts to neighbouring properties. Moreover, it is considered that the proposed design has the ability to accommodate scale while delivering a transition in scale to the south, while also minimising off-site amenity impacts such as overshadowing.

We note that in October 2021, a revised Box Hill Metropolitan Activity Centre to 2036 Draft Structure Plan was prepared for the Box Hill MeAC by MGS which indicates a preferred maximum building height of 8 storeys (or 28 metres). The plan was supported by the Box Hill Metropolitan Activity Centre to 2036 Urban Design Framework (UDF) which contains recommendations for a new built form framework. Due to the Suburban Rail Loop precinct planning, both have remained in draft form and have not progressed to a planning scheme amendment. Therefore, though the UDFs objectives are considered relevant, the work has not been tested nor does it have any statutory weight.

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6.3.2.2. Podium and Street Wall Height

The proposed development involves a four (4) storey street wall height of 16.2 metres to Carrington Road and 18.09 metres to Cambridge Street in conjunction with the slope of the land. The architectural design carefully considers the emerging character of higher-density built forms along Carrington Road, whilst recognising the lower-order developments to Cambridge Street. Therefore, the proposed development provides for a transition in the street wall height from the northern commercial properties to the southern residential areas.

The relevant planning policy within the Whitehorse Planning Scheme does not specify targeted requirements for podium heights of new buildings. Clause 11.03-1L-02 of the Planning Scheme generally seeks to ensure new built form is appropriate in the context of the area and present visual interest in the streetscape with pedestrian scale design at street level. The *Box Hill Structure Plan (2007)* seeks a four-storey street wall height for both Carrington Road and Cambridge Street, whilst the *Draft Urban Design Framework (2021)* seeks a preferred street wall height of 20 metres to Carrington Road and 13.5 metres to Cambridge Street.

It is considered that the street wall to Carrington Road responds to the emerging built form character to the north, while providing a comfortable street width to street wall height ratio. Whilst the street wall to Cambridge Street complements the low-rise character on the opposite side of the street, with levels above designed to transition in scale to the south to avoid overwhelming the public realm. The four (4) storey street wall to both interfaces contextually responds to the approx. 15.4 m overall building height of the Telstra Exchange Building which is earmarked for long-term retention given its role as critical infrastructure. Moreover, the rounded parapet and decorative semi-looped canopy assist with the articulation of the street wall without adding unnecessary perceived height to the streetscape. Therefore, the proposal will contribute to an appropriate precedent for street wall heights along Carrington Road for future development. It is noted that the *Box Hill Structure Plan (2007)* seeks four-storey developments to the residential properties to the south.

Overall, it is considered that this outcome to the street wall maintains a human scale, responds to the emerging street wall character, and provides a transition in street wall height between the northern commercial properties and the southern residential properties. The height of the proposed podiums respects the scale of the adjoining buildings and sets an appropriate precedent for podium development along the Carrington Road – Cambridge Street precinct.

6.3.3. Massing and Scale

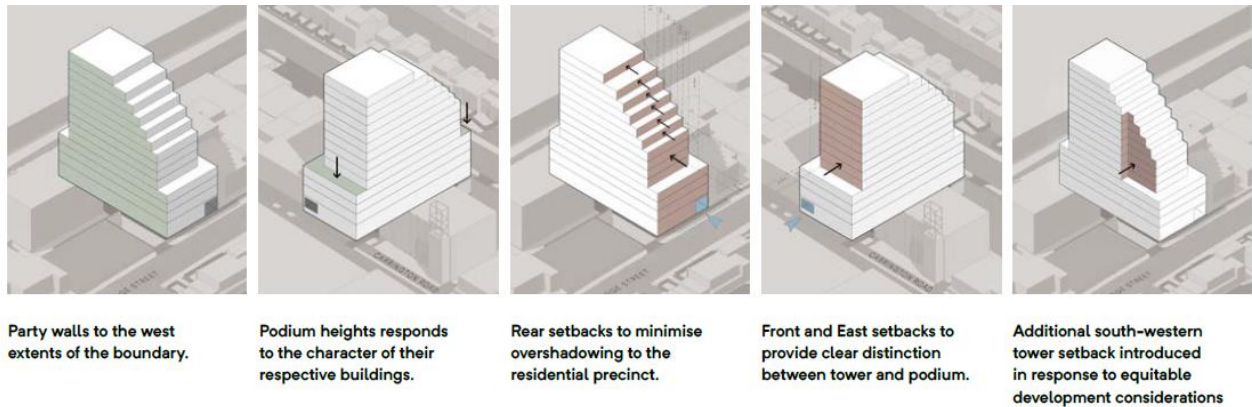
The surrounding area is a Metropolitan Activity Centre with an emerging character of higher-density, mixed-use developments. It is considered the proposed massing and scale of the development is appropriate within this context, noting:

- The proposal provides upper levels to the development that are recessed to each street interface, reducing the perceived height of the development and visual bulk. A 4.5 metre setback is provided to the primary frontage to Carrington Road, providing clear distinction between the tower and podium forms.
- The proposal adjusts the floor plates of the development towards the south, resulting in increased setbacks to the adjoining sensitive residential interfaces. The proposed development results in an appropriate transition from the northern commercial areas to the southern residential areas.
- The proposal strategically locates balcony terraces to upper levels on the outer building edges facing street interfaces, resulting in greater visual recession of the structural walls to the upper floors.
- The proposal varies the structural language across upper levels, expressing both vertical and horizontal elements and curved designed features that complement the overall architectural composition.
- The proposal includes a 4.5 metre upper level setback to the east, allowing for equitable development opportunities and sky views through the site when viewed from the street level.
- A 3 metre upper level setback is proposed to the south-west, directly adjacent the proposed development at 30 Cambridge Street Box Hill, in order to ensure equitable development rights are maintained to the future building to be constructed.
- The proposal maintains carefully considers the interplay between setbacks and the material and finishes schedule, ensuring equitable development opportunities to the eastern properties through the employment of curtain glazing to the eastern wall.

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- The proposal adopts the use of different material types and colours to provide visual interest within the façades whilst reducing the appearance of the built form.
- The proposal provides landscaping opportunities to the upper levels, softening the building edges when viewed from the street and neighbouring properties. The predominant landscaping features are provided in tandem with the stepped-back upper-levels to Cambridge Street to prioritise the visual impacts to sensitive areas.
- The proposal integrates landscaping into all site interfaces and the public realm area at the building entry points and within the walkways to soften the appearance of the building.
- The proposal adopts a two-level basement car park layout to reduce the visual impacts of car parking facilities from the street perspective.

Figure 17: Massing and Form Evolution



Source: Gray Puksand, 2024

6.4. EXTERNAL AMENITY CONSIDERATIONS

The proposed mixed-use development has been designed to respond to the site and surrounds, future anticipated growth and limit the external amenity impacts for surrounding properties. The proposal has been designed to limit three key measures of external amenity impact – visual bulk, equitable development, and overshadowing.

The design response has adequately addressed these areas as outlined in detail below:

6.4.1. Visual Bulk

In regard to the overall presentation to the streetscape and surrounding properties, it is considered that the scale and design of the development provides a sensitive response to the site's context and is consistent with the preferred development outcomes in the area.

As noted in Table 2 and the descriptions below, the proposal provides an appropriate interface-related design response consistent with the *Box Hill Structure Plan (2007)* and Whitehorse Planning Scheme.

Table 2: Interface and Dimension Summary

Dimensions / Interface	Northern	Eastern	Southern	Western
Building Height	52.56 metres	52.56 metres	52.56 metres	52.56 metres
Street Wall Height	16.2 metres	-	18.09 metres	-
Ground Floor Setback	0 metres	0 metres	0 metres	0 metres
Level 1 – 3 Setback	0 metres	0 metres	0 metres	0 metres

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Dimensions / Interface	Northern	Eastern	Southern	Western
Level 4 Setback	4.5 metres	4.5 metres	3.8 metres	0m to north-west 3 metres south of building core
Level 5-8 Setback	4.5 metres	4.5 metres	3.8 metres	0m to north-west 3 metres south of building core
Level 9 Setback	4.5 metres	4.5 metres	6.3 metres	0m to north-west 3 metres south of building core
Level 10 Setback	4.5 metres	4.5 metres	8.8 metres	0m to north-west 3 metres south of building core
Level 11 Setback	4.5 metres	4.5 metres	11.8 metres	0m to north-west 3 metres south of building core
Level 12 Setback	4.5 metres	4.5 metres	14.8 metres	0m to north-west 3 metres south of building core
Level 13 Setback	4.5 metres	4.5 metres	17.8 metres	0m to north-west 3 metres south of building core
Level 14 Setback	4.5 metres	4.5 metres	20.8 metres	0m to north-west 3 metres south of building core

6.4.1.1. Northern Interface – Carrington Road

To the non-sensitive northern street interface, the proposed development has a building height ranging between 16.2 metres at street level and 52.56 metres at maximum building height.

At ground level, the northern interface provides a zero-level setback to Carrington Road, a priority pedestrian corridor that is earmarked for public realm upgrades. The ground level provides street activation through the provision of a retail tenancy with clear glazing and a pedestrian thoroughfare that is clearly identifiable with landscaping features. At first-floor level, the zero-level setback is maintained, save for a void above the pedestrian entryway providing further framing of the pedestrian access point. The first-floor retail tenancy will provide passive surveillance opportunities to Carrington Road in addition to the ground-level retail tenancy.

At first and second-floor level, the zero-level setback is maintained, save for outdoor terrace areas of 14 square metres that are setback 3.4 metres to the north-western corner of each level. The third-floor level completes the podium element to Carrington Road, with a communal terrace area between two office tenancies of 106 square metres with landscaping opportunities.

At fourth-floor level to the roof level, an additional recession of 4.5 metres is provided with a consistent setback of 4.5 metres provided to the northern boundary, with architectural elements minorly encroaching 1.5 metres with a consistent setback of 3.65 metres.

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6.4.1.2. Southern Interface – Cambridge Street

To the non-sensitive southern street interface, the proposed development has a building height ranging between 18.09 metres at street level and 52.56 metres at maximum building height.

At ground level, the southern interface provides a zero-level setback to Cambridge Street. The ground level provides street activation through the provision of public pedestrian link connecting Cambridge Street to the internal retail tenancies and the priority pedestrian corridor along Carrington Road. At first-floor level, the zero-level setback is maintained, save for a void above the pedestrian entryway to provide framing of the pedestrian access point. The first-floor retail tenancy will provide passive surveillance opportunities to Cambridge Street in addition to the ground-level retail tenancy.

At second and third-floor level, the zero-level setback is maintained. The fourth-floor level completes the podium element to Cambridge Street, with a communal terrace area between three office tenancies of 97 square metres with landscaping opportunities. At fifth-floor level to the roof level, the southern interface is progressively stepped back from an initial setback of 3.8 metres at Level 5 to a final setback of 21 metres at rooftop level. Communal outdoor terrace areas are provided to each progressive setback with landscaping opportunities to soften the appearance of the building edges.

Figure 18: Northern and Southern Interfaces



Source: Gray Puksand, 2024

6.4.1.3. Western Interface

To the non-sensitive western commercial interface, the proposed development has a maximum building height of 52.56 metres. The proposed development maintains zero-level setbacks to the western boundary for the entire extent of the building where the site is adjacent the Telstra exchange building. In the south-western corner of the building, the development maintains zero-level setbacks from Levels 1 to 3. Levels 4 to 14 incorporate a 3 metre side setback to respond to the proposed development at 30 Cambridge Street, to maintain equitable development rights noting their proposed setback to the common boundary. It is considered that the Telstra Exchange building site to the west has limited development potential and is unlikely to change in the future and accordingly is appropriate for a proposed nil setback. In addition, grey vision glazing has been incorporated into the western façade to uplift internal amenity to the office floors which simultaneously ensuring the building reads in the round more generally from oblique views in the public realm.

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6.4.1.4. Eastern Interface

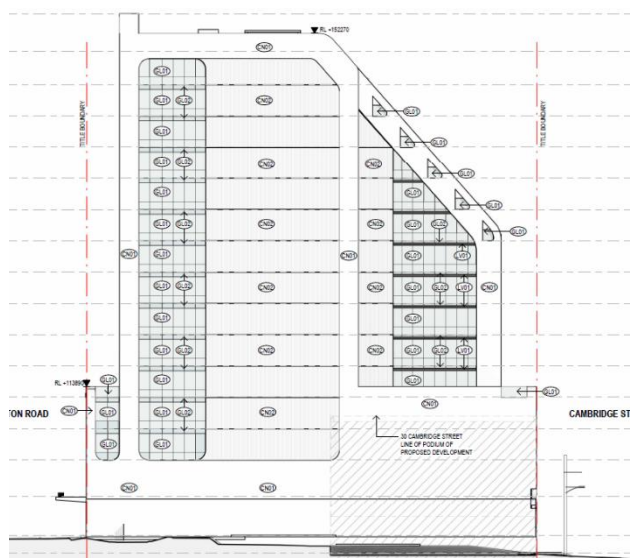
To the east, the proposal provides a zero-level setback to the height of the podium. Above the podium, the building is setback 4.5 metres with a roof top terrace provided at Level 4. The tower setback will reduce visual bulk impacts and facilitate clear views to the sky above the podium from both Carrington Road and Cambridge Street, as well as providing daylight and outlook to future building occupants.

By providing a 4.5m setback above the podium this allows any future development at 67 Carrington Road to mirror the outcome, providing separation between the tower elements. This would facilitate an efficient mixed use or commercial development floorplate that also utilises its dual street frontages for primary orientation.

Until such development occurs to the east, the proposal maximises glazing opportunities along the majority of the eastern boundary to ensure a well-articulated boundary interface which allows the development to be read in the round when viewing the site from the east.

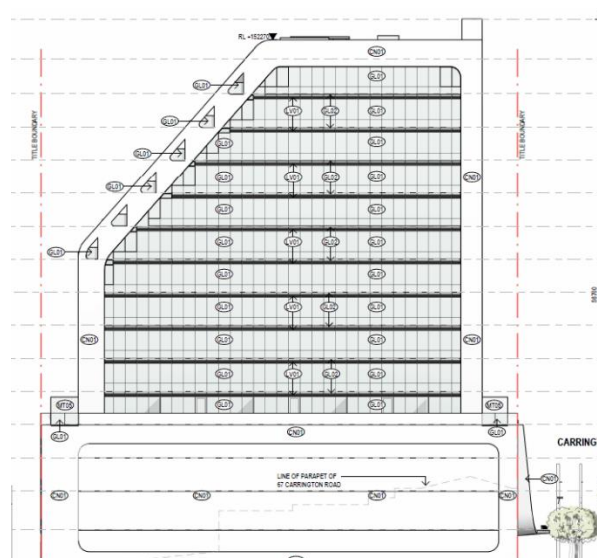
Equitable development considerations are discussed in Section 6.4.2 below.

Figure 19: Eastern & Western Interfaces



Picture 5: Western Interface

Source: [Gray Puksand, 2024]



Picture 6: Eastern Interfaces

Source: [Gray Puksand, 2024]

6.4.2. Equitable Development

Urbis Ltd has prepared an Urban Context Report supporting the design rationale behind the proposed building separation and equitable development response of the development.

The Whitehorse Planning Scheme does not provide specific requirements ensuring equitable development to adjacent properties. Nevertheless, the *Box Hill Structure Plan (2007)* aims to maintain building separation between upper levels of high-rise buildings to maximise private amenity access to sunlight and views. The *Draft Urban Design Framework (2021)* also emphasises building separation for sky views from the street, enhancing amenity and outlook within the buildings, while ensuring fairness in the development of neighbouring sites. The *Draft Urban Design Framework (2021)* seeks side setbacks above the street wall, advocating for a 4.5 metre setback for heights below 28 metres and a 6 metre setback for heights exceeding 28 metres.

Adhering to the draft urban design controls to both side interfaces would significantly impact the viability of floorplates above the podium, requiring a centralised lift core which would compromise the functionality and size of the basement carpark and office floorplates. As such, the proposed development seeks to implement a 4.5 metre tower setback to the east. The development will be constructed with a nil setback to the full height of the building where the subject site is adjacent the Telstra Exchange building. Where the site adjoins 30 Cambridge Street, a 3 metre tower setback will be employed. The design response is considered appropriate on the premise that the Telstra Exchange building site to the west has limited development

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potential, whilst the response to the east and to 30 Cambridge Street will provide the opportunity for appropriate and equitable developments to be constructed to each site.

To the north-western interface, the Telstra Exchange Building is considered a critical piece of infrastructure servicing the area and is unlikely to change in the future. The existing building already provides a 3 metre building separation to the intermediate boundary.

The subject site adjoins 30 Cambridge Street along the southern portion of the site's western boundary, which is currently subject to a planning permit application under assessment by Whitehorse City Council. The proposal seeks approval for an 11-storey building (42.3m) with a 4-storey podium and 7-storey tower. The proposal seeks a 3.8 metre tower setback to Cambridge Street consistent with the proposed development. In addition, a 4.5 metre tower setback is proposed to its eastern façade, and the common boundary with the subject site.

The proposal's 3 metre western setback where it interfaces with 30 Cambridge Street is an appropriate design response and will assist in maintaining equitable development rights to the adjacent development, ultimately facilitating a higher standard of internal amenity for both developments. A 7.5 metre separation between the two towers is a warranted outcome noting both developments will be for office land uses, which are subject to less intense internal amenity requirements and are not subject to overlooking considerations. Holistically, the proposal is considered to achieve an appropriate equitable development outcome to the western adjoining properties noting the above.

To the eastern interface, the adjoining properties occupy similar lot sizes and widths along Carrington Road including the adjacent site at No. 67 Carrington Road, Box Hill. The adjoining properties are also designated within the same precinct with the same policy intent for higher-density, mixed-use developments providing a transition between the northern commercial properties and southern residential areas. Therefore, it is considered that the adjoining properties will likely seek a similar development outcome to the proposed development.

Taking this into consideration, the 4.5 metre tower setback to the eastern boundary will provide the opportunity for development at 67 Carrington Road to mirror the built form outcome of the proposal and achieve a total 9 metre tower separation. As that the proposal would be the first major redevelopment along Carrington Road, this would set an appropriate precedent for preferred building siting and separation outcomes for new development along the street.

Notwithstanding, the proposed development acknowledges that in the short-term, the side interfaces will provide exposed sheer walls. The western wall is proposed to have glazing elements attached to the precast concrete wall, with a ribbed concrete pattern and triangular puncture detail. However, the western wall will provide clear glazing in response to the low development potential of the western properties, whilst the eastern wall will provide a combination of vision and spandrel glazing via an in window wall system, in response to the high development potential of the eastern properties. Therefore, the architectural design provides visual interest when viewed from a western and eastern perspective, whilst not prejudicing the neighbouring properties from the opportunity to develop.

In summary, it is considered that the 4.5 metre setback above the podium to the east, and 3 metre setback to 30 Cambridge Street is a positive outcome in facilitating equitable development opportunities that affords efficient floorplates for both office and/or residential development outcomes.

Please refer to the Urban Context Report prepared by Urbis Ltd dated 21 May 2024 for further details.

6.4.3. Overshadowing

The proposed development has been carefully designed to minimise any additional overshadowing impacts on neighbouring properties and public spaces. The proposal strategically adopts a stepped back response to ensure that overshadowing is minimised to the sensitive interfaces. We note that the two-storey townhouses to the south of Cambridge Street are the only sensitive residential properties that could be affected by overshadowing impacts. We also note that the southern residential properties fronting Cambridge Street are located within the Mid-rise Commercial and Mixed-use Precincts.

It is noted that the current Box Hill Structure Plan seeks a four-storey preferred height limit for the southern residential properties, and therefore, the townhouses may be subject to increased density. It is also acknowledged that the Box Hill Structure Plan also seeks development that avoids overshadowing of key public spaces, peripheral residential precincts, or residential areas '*outside the activity centre between 11am and 2pm on 22 June, beyond what would result from an 11-metre building over the full extent of the site*'.

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The proposed development does not overshadow the residential areas outside the activity centre beyond Thurston Street to the west.

However, a specific overshadowing control does not apply to residential areas within the activity centre. Therefore, Standard B21 of the Whitehorse Planning Scheme has been applied to determine the acceptability of the overshadowing to the southern residential properties. In line with the recent State Government Planning Reforms, and the changes to the interpretation of ResCode, compliance with Standard B21 now means that the relevant overshadowing objective is met with no further consideration. Therefore, we consider the application of Standard B21 in a commercial setting where it is not required, should elicit the position that the overshadowing impacts have been appropriately resolved without further consideration.

As shown below, the proposal will cast additional shadows to the SPOS beyond the existing fence of 37-41 Cambridge Street until 10am and will result in 5 hours of sunlight for the balance of the day. The SPOS of the town houses at 35 Cambridge Street are overshadowed at 10am, with partial overshadowing at 11am, and minimal impact beyond the shadows cast by the existing fences from 12pm onwards. We note that these SPOSs are provided within the front setback of this lower-order, residential precinct. Please refer to the Urban Context and Architectural Design Package prepared by Gray Puksand for further details.

Figure 20: Overshadowing Diagrams



Source: Gray Puksand, 2024

6.5. INTERNAL AMENITY CONSIDERATIONS

The proposed development has been designed to ensure the future occupants of the building will be provided with a high level of internal amenity.

The following is a summary of the key aspects relating to the internal amenity provision:

- The proposed fifteen (15) storey building maximises the floor area for employees and visitors, whilst remaining in line with the surrounding Box Hill MAC context.
- The proposed ceiling-to-floor heights vary between 4.2 metres at ground-level and 3.4 metres at upper levels, providing ample space for efficient and comfortable day-to-day operations.
- The proposal has been designed to be fit-for-purpose, considering the day-to-day requirements of the office and retail uses and providing the amenity and internal service required to operate effectively.

- The ventilation modelling results demonstrate that 83% of the nominated floor area will achieve effective natural ventilation.
- The proposal provides a new publicly accessible pedestrian link along the ground plane, providing internal street activation and direct access between the northern commercial properties and southern residential areas.
- The proposal maximises landscaping opportunities on the site, creating a softened development and increased 'green space' outcome for staff and visitors of the development.
- The proposal provides EOT facilities, including seven (7) showers across three (3) changing room areas and sixty-eight (68) storage lockers.
- The proposed development considers accessible design to ensure the development is available to people of all ages and abilities, providing accessible EOT facilities, lift services and car parking.
- The proposed retail uses to the ground and first floor level will ensure that an appropriate level of passive surveillance is provided to the building entryways and streetscapes.
- The proposed carpark provides safe and efficient use of the site, facilitating external access to the built form. The carpark is separated from the operations of the proposed development, providing a safe environment for staff and visitors.

6.6. ENVIRONMENTAL CONSIDERATIONS

6.6.1. Landscaping

The Landscape Plan prepared by ASPECT studios details the concept landscaping proposed for the proposed development. The concept landscaping involves the provision of several uncovered terrace areas between Level 4 and Level 13 in addition to the publicly accessible pedestrian link that provides planters to the street interfaces and within the site. The proposed development will provide a total landscaped and terraced area of 644 square metres.

Landscaping and 'green space' treatment is provided across the proposed development to improve the sites permeability, environmental response, and sense of place. Each landscaped space seeks to provide 'green' amenity and refuge, blurring internal and external boundaries and integration of deep soil planting treatments to the street interfaces wherever structurally viable.

On the ground level, new landscaping treatment is proposed to the Carrington Road and Cambridge Street entryways to clearly identify the public pedestrian link that leads to landscaped spaces framing the central lobby. At the rooftop of the podium, landscaping is incorporated to the north, south and east setbacks to visually integrate landscape the building form ascends. At upper-levels, further landscaping opportunities are provided to the outdoor terrace areas, softening the outlook of the building edges, and enhancing the communal outdoor spaces for the building occupants.

Whilst the removal of an existing street tree to Cambridge Street is sought to enable the re-location of the vehicle accessway, the proposal makes a commitment to improve the nature strips to the street interfaces following construction. This will result in a key public benefit by enhancing the landscaping quality of the public realm.

Please refer to the Landscape Plans prepared by Aspect Studios for further details.

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Figure 21: Landscape Imagery (Carrington Road)



Source: Gray Puksand, 2024

Figure 22: Landscape Imagery (Cambridge Street)



Source: Gray Puksand, 2024

Figure 23: Landscape Imagery (Carrington Road podium)



Source: [Gray Puksand, 2024]

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6.6.2. Environmentally Sustainable Design

WRAP Engineering Pty Ltd has prepared a Sustainability Management Plan to demonstrate that the proposed development achieves 'best practice' in accordance with environmental design standards.

The development will incorporate the following key sustainable design initiatives:

- BESS score of 54%, including Stormwater (100%), Water (57%), Energy (54%) and IEQ (56%).
- A STORM score of 100%, involving two (2) underground rainwater tank of 20,000L in the basement area.
- A 4.5-kW solar photovoltaic (PV) renewable energy system located on the rooftop.
- Daylight modelling results, which demonstrates the following nominated floor areas will achieve a daylight factor of 2% or greater:
 - 30% of the nominated retail floor area will achieve the daylight factor.
 - 50% of the nominated office floor area will achieve the daylight factor.
- Ventilation modelling results, demonstrating that 83% of the nominated floor area will achieve effective natural ventilation.

Please refer to the Sustainability Management Plan prepared by WRAP Engineering Pty Ltd for further details.

6.6.3. Wind Impacts

MEL Consultants have prepared an Environmental Wind Assessment detailing the likely environmental wind conditions and demonstrating that the proposed development will be able to achieve wind safety criterions.

The Environmental Wind Assessment recommends the following wind comfort criterion for the development:

- Streetscapes – Walking
- Building Entrances – Standing
- External Lift Entrances (Lifts 4 & 5) – Standing
- Office Levels Outdoor Terraces – Walking

The Environment Wind Assessment concludes that wind conditions would satisfy the pedestrian safety criterion with minor changes to the built form. It is recommended that wind tunnel studies are employed to provide further design detail through the quantification of the wind conditions around the development.

Please refer to the Environmental Wind Assessment prepared by MEL Consultants for further details.

6.7. BUILDING SERVICES AND PERFORMANCE

6.7.1. Traffic

The Traffic Impact Assessment prepared by Traffix Group details the traffic-related aspects of the proposed development.

6.7.1.1. Car Parking

Pursuant to Clause 52.06-3, a permit is required to reduce the number of car parking spaces required under Clause 52.06-5. The site is located within the Principal Public Transport Network (PPTN); therefore, the car parking rates specified in Column B of Table 1 to Clause 52.06-5 applies to the proposed development.

Retail

- Column B of Table 1 to Clause 52.06 sets out a rate of 3.5 parking spaces to each 100 square metres of leasable floor area for a retail (shop) use. The proposed development seeks a total leasable retail floor area of 1,396 square metres which results in a statutory requirement of forty-eight (48) car parking spaces.

Office

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- Clause 3.0 of the Parking Overlay – Schedule 1 (PO1) sets out a rate of 2 parking spaces to each 100 square metres of net floor area for an office use. The proposed development seeks a total net office floor area of 6,646 square metres which results in a statutory requirement of one-hundred and thirty-two (132) car parking spaces.

Based on the above, the proposed development has a statutory requirement to provide one-hundred and eighty (180) car parking spaces. The applicant proposes to provide forty-one (41) car parking space on-site, resulting in a shortfall of one-hundred and thirty-nine (139) car parking spaces when compared to the statutory requirements.

The proposed car parking reduction to the statutory car parking requirements is deemed appropriate provided it will occur where sufficient car parking exists in the Activity Centre area and where there is a diverse choice of public transport modes.

Please refer to the Traffic Impact Assessment prepared by Traffix Group for further details.

6.7.1.2. Access Arrangements

Vehicle access to the site will be from a re-located vehicle crossover to Cambridge Street, adjacent to a loading bay and leading to a two-level basement carpark area via a car lift. The proposed car lift, whilst single use, is appropriate having regard to the expected arrival and departure times of vehicles to the development. The development incorporates forty-one (41) car parking spaces, as well as fifty (50) bicycle spaces with EOT facilities for all peoples.

Pedestrian access to the site will be provided to Carrington Road and Cambridge Street. The pedestrian entryways will be connected via a public pedestrian walkway along the ground plane and adjacent to the eastern boundary, linking the primary pedestrian network (Carrington Road) to the southern residential areas through the site. The retail tenancies fronting the Carrington Road and Cambridge Street interfaces will provide a high level of passive surveillance to the street entryways and the public pedestrian link on-site.

Please refer to the Traffic Impact Assessment prepared by Traffix Group dated 20 May 2024 for further details.

6.7.1.3. Bicycle Parking

Clause 52.34 sets out that a new use must not commence until the required bicycle facilities and associated signage has been provided on the land. Clause 52.34 specifies bicycle parking rates for retail and office uses based on the leasable floor area and net floor area respectively.

Retail

- Table 1 to Clause 52.34 sets out a rate of 1 parking space to each 300 square metres of leasable floor area for staff of a retail (shop) use. The proposed development seeks a total leasable retail floor area of 1,396 square metres which results in a statutory requirement of five (5) staff bicycle parking spaces.
- Table 1 to Clause 52.34 sets out a rate of 1 parking space to each 500 square metres of leasable floor area for customers of a retail (shop) use. The proposed development seeks a total leasable retail floor area of 1,396 square metres which results in a statutory requirement of three (3) customer bicycle parking spaces.

Office

- Table 1 to Clause 52.34 sets out a rate of 1 parking space to each 300 square metres of net floor area for staff of an office use. The proposed development seeks a total net office floor area of 6,646 square metres which results in a statutory requirement of twenty-two (22) staff bicycle parking spaces.
- Table 1 to Clause 52.34 sets out a rate of 1 parking space to each 1,000 square metres of net floor area for visitors of an office use. The proposed development seeks a total net office floor area of 6,646 square metres which results in a statutory requirement of seven (7) visitor bicycle parking spaces.

Based on the above, the proposed development has a statutory requirement to provide thirty-seven (37) bicycle parking spaces – twenty-seven (27) staff bicycle parking spaces and ten (10) visitor bicycle parking spaces. The applicant proposes to provide fifty (50) bicycle parking spaces on-site, resulting in a surplus of thirteen (13) bicycle parking spaces when compared to the statutory requirements.

Please refer to the Traffic Impact Assessment prepared by Traffix Group for further details.

6.7.2. Waste Management

Traffix Group has prepared a Waste Management Plan that outlines the waste strategies proposed for the development.

In summary, the report proposes the following waste collection measures:

- One (1) dedicated waste storage area of 64 square metres to the south-eastern boundary on the basement 1 floor level, including:
 - Thirty-two (32) bins – seventeen (17) 240L FOGO bins, nine (9) 1,100L garbage bins, one (1) 660L and two (2) 1,100L recycling bins, one (1) 660L glass bin, one (1) 1,100L and one (1) 360L paper & cardboard bins.
- All waste bins are to be collected by private contractors on-site within a loading area at ground level that is accessible from Cambridge Street.
- All bins will be collected five times per week.

Please refer to the Waste Management Plan prepared by Traffix Group for further details.

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7. CONCLUSION

In summary, it is considered that the proposed use and development for a fifteen (15) storey, mixed-use development at No. 69 Carrington Road, is considered worthy of approval for the following reasons:

- The proposal demonstrates a high level of consistency with the Whitehorse Planning Policy Framework and planning controls, including the relevant local policy for office and retail uses.
- The proposal will positively contribute to the land use and development of the Box Hill MAC, particularly through the co-location of office and retail uses in a higher-density, tower form to support the Box Hill MAC's urban growth as the major focus for mixed-use development in Melbourne's eastern region.
- The built form design presents a balanced response to the existing conditions and will contribute to the revitalisation of the site and improved outcomes for Carrington Road and Cambridge Street. The architectural response successfully provides a visual and functional transition between the evolving commercial properties of Carrington Road and the residential areas to the south of Cambridge Street.
- The proposed design response has been carefully considered ensuring that a high level of internal amenity is afforded to the future occupants, whilst external amenity impacts to adjoining residential properties by way of visual bulk, overshadowing and equitable development are minimised.
- The proposed environmental and building services and performance considerations are well-resolved. The proposal provides for a high-quality delivery of landscape, sustainable management, waste management, wind, and traffic conditions.
- The proposed car parking provision seeks a reduction to statutory car parking requirements where a reduction in car parking already exists on the existing site, sufficient car parking exists in the Activity Centre area and where there is a diverse choice of public transport modes, which is consistent with the objectives of Clause 52.06.

For the reasons outlined within this report and the accompanying documentation, it is considered that this planning permit application reflects a well resolved mixed-use development and warrants favourable determination to facilitate this high-quality development.

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8. DISCLAIMER

This report is dated 19 December 2024 and incorporates information and events up to that date only and excludes any information arising, or event occurring, after that date which may affect the validity of Urbis Ltd (**Urbis**) opinion in this report. Urbis prepared this report on the instructions, and for the benefit only, of Carrington Road Management Pty Ltd (**Instructing Party**) for the purpose of Planning Report (**Purpose**) and not for any other purpose or use. To the extent permitted by applicable law, Urbis expressly disclaims all liability, whether direct or indirect, to the Instructing Party which relies or purports to rely on this report for any purpose other than the Purpose, and to any other person which relies or purports to rely on this report for any purpose whatsoever (including the Purpose).

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All surveys, forecasts, projections and recommendations contained in or associated with this report are made in good faith and on the basis of information supplied to Urbis at the date of this report, and upon which Urbis relied. Achievement of the projections and budgets set out in this report will depend, among other things, on the actions of others over which Urbis has no control.

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Whilst Urbis has made all reasonable inquiries it believes necessary in preparing this report, it is not responsible for determining the completeness or accuracy of information provided to it. Urbis (including its officers and personnel) is not liable for any errors or omissions, including in information provided by the Instructing Party or another person or upon which Urbis relies, provided that such errors or omissions are not made by Urbis recklessly or in bad faith.

This report has been prepared with due care and diligence by Urbis and the statements and opinions given by Urbis in this report are given in good faith and in the reasonable belief that they are correct and not misleading, subject to the limitations above.

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APPENDIX A PLANNING POLICY FRAMEWORK

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PLANNING POLICY FRAMEWORK

The site is subject to the provisions of the Whitehorse Planning Scheme. The following summary outlines the key planning policies and controls that affect the proposal. We note that there have been no substantive changes to planning policy since the permit was originally issued.

PLANNING ZONE

Commercial 1 Zone (C1Z)

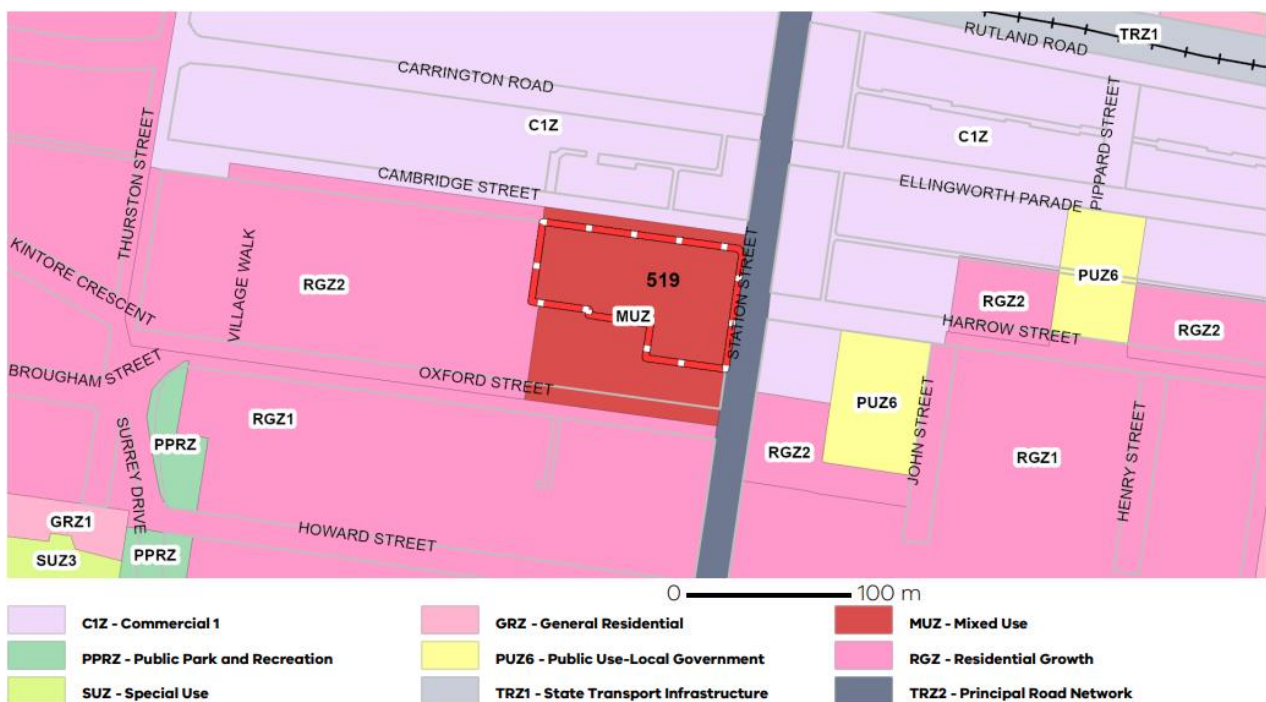
The subject site is located within the Commercial 1 Zone (C1Z). The purpose(s) of the C1Z are as follows:

- To implement the Municipal Planning Strategy and the Planning Policy Framework.
- To create vibrant mixed use commercial centres for retail, office, business, entertainment, and community uses.
- To provide for residential uses at densities complementary to the role and scale of the commercial centre.

Pursuant to the Commercial 1 Zone (C1Z):

- A planning permit is not required for the use of the land for office or retail uses (Clause 34.01-1)
- A planning permit is required to construct a building or construct or carry out works for a use in Section 2 (Clause 34.01-4)

Figure 24: Commercial 1 Zone (C1Z)



Source: VICPLAN, 2023

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PLANNING OVERLAYS

Parking Overlay – Schedule 1 (PO1)

The site is affected by Parking Overlay – Schedule 1 (PO1). The relevant purpose(s) of the PO1 are:

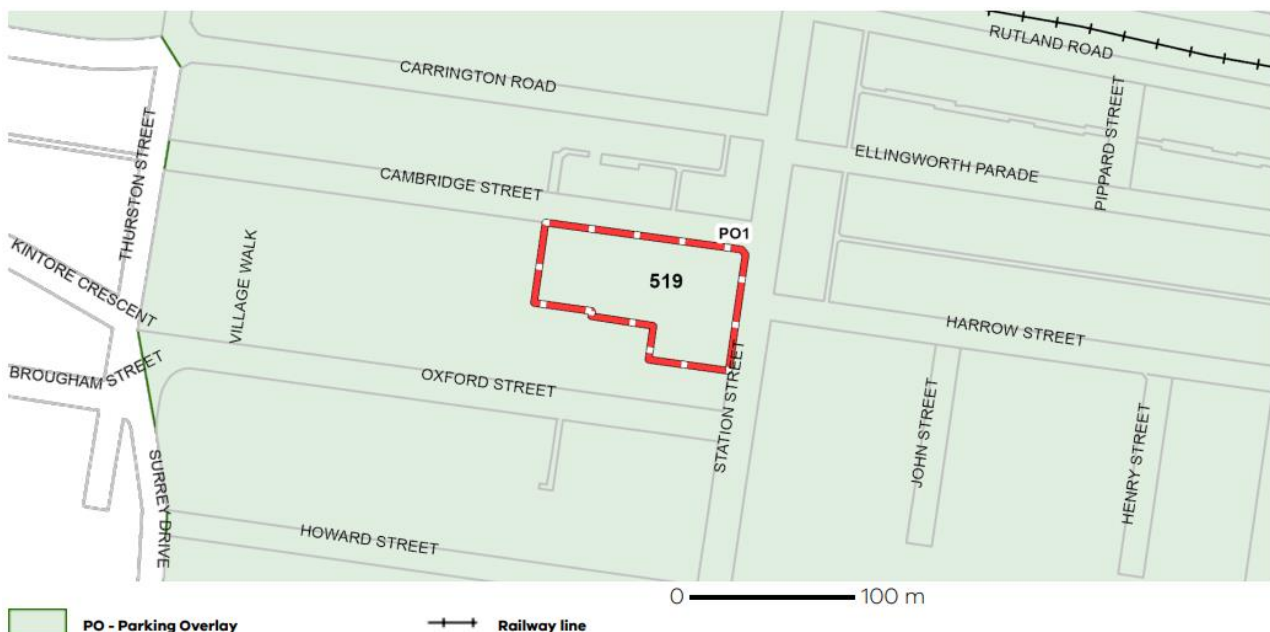
- To implement the Municipal Planning Strategy and the Planning Policy Framework.
- To facilitate an appropriate provision of car parking spaces in an area.
- To identify areas and uses where local car parking rates apply.
- To identify areas where financial contributions are to be made for the provision of shared car parking.

Pursuant to Clause 1.0 of the PO1, the parking objectives to be achieved include:

- Manage car parking demand and supply to satisfy user needs (with a focus on maintaining/increasing the viability of Box Hill).
- Locate and manage car parking so as to minimise traffic generated by the search for a parking space.
- Reduce vehicle trips through minimising parking provision where appropriate.
- Encourage the use of active and sustainable travel modes rather than increased private vehicle travel.
- Improve general amenity for pedestrians within Box Hill to increase the willingness for visitors and staff to walk to and within the Centre to their destination.

The Parking Overlay allocates minimum car parking rates for dwelling and office uses at a lower rate than the statutory requirements prescribed by Clause 52.06-5 of the planning scheme. All other uses should accord with Column B of Table 1 of Clause 52.06-5.

Figure 25: Parking Overlay – Schedule 1 (PO1)



Source: VICPLAN, 2023

PARTICULAR PROVISIONS

The site is affected by the following Particular Provisions:

Clause 52.06 – Car Parking

Clause 52.06 seeks to ensure an appropriate level of car parking is provided for a new use or an increase in the floor area of an existing use, as well as ensuring the design of access and parking areas is considered to ensure efficient and safe manoeuvrability throughout a site.

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Pursuant to Clause 52.06-5, the car parking requirement specified in Table 1 does not apply as a schedule to the Parking Overlay specifies the number of car parking spaces required for the proposed uses.

The proposed development has a statutory requirement to provide two-hundred and one (201) car parking spaces – forty-eight (48) retail car parking spaces and one-hundred and thirty two (32) office car parking spaces. The applicant proposes to provide forty-one (41) car parking space on-site, resulting in a shortfall of one-hundred and thirty-nine (139) car parking spaces when compared to the statutory requirements.

Therefore, the proposed development requires a permit for the reduction of the statutory car parking requirement. The proposed car parking reduction to statutory car parking requirements is deemed appropriate provided it will occur where sufficient car parking exists in the Activity Centre area and where there is a diverse choice of public transport modes.

Please refer to the Traffic Impact Assessment prepared by Traffix Group for further details.

Clause 52.34 – Bicycle facilities

Clause 52.34 specifies bicycle parking requirements new developments. The proposed development has a statutory requirement to provide thirty-seven (37) bicycle parking spaces – twenty-seven (27) staff bicycle parking spaces and ten (10) visitor bicycle parking spaces.

The applicant proposes to provide fifty (50) bicycle parking spaces on-site, resulting in a surplus of thirteen (13) bicycle parking spaces when compared to the statutory requirements.

Please refer to the Traffic Impact Assessment prepared by Traffix Group dated for further details.

Clause 53.22 – Significant Economic Development

Clause 53.22 seeks to prioritise and facilitate the planning, assessment and delivery of projects that will make a significant contribution to Victoria's economy and provide substantial public benefit, including jobs for Victorians.

Pursuant to Clause 53.22-1, a planning application can be lodged under the 'Category 1' pathway where the following conditions are met:

- *The use must be specified in Table 2 and the condition corresponding to that use must be met.*
- *If the application includes more than one use in Table 2, only one use must meet the corresponding condition. Must have written advice from the Chief Executive Officer, Invest Victoria confirming the likely financial feasibility of the proposal.*

Table 2 to Clause 53.22 provides the following requirements for office developments, where the proposed use must meet one of the following:

- *The combined gross floor area of all buildings associated with the proposed use or development must be at least 10,000 square metres if any part of the land is in metropolitan Melbourne; or*
- *The combined gross floor area of all buildings associated with the proposed use or development must be at least 5,000 square metres if the land is not in metropolitan Melbourne.*

The proposed development is considered to meet the minimum provision with a total Gross Floor Area (GFA) of 13,495 square metres, including at least 9,048 square metres attributed to the proposed office use on the land.

GENERAL REQUIREMENTS AND PERFORMANCE STANDARDS

Clause 53.18 – Stormwater Management in Urban Development

Clause 53.18 seeks to ensure that stormwater in urban development, including retention and reuse, is managed to mitigate the impacts of stormwater on the environment, property, and public safety, and to provide cooling, local habitat, and amenity benefits.

Clause 65 – Decision Guidelines

The Clause contains a range of decision guidelines that the responsible authority must consider before deciding on an application.

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Clause 72.01 – Responsible Authority

This Planning Scheme identifies the Minister of Planning as the responsible authority for this application as the proposal is one where the use or development applies to Clause 53.22

PLANNING POLICIES

The Planning Policy Framework seeks to develop the objectives for planning in Victoria (as set out in the Planning and Environment 1987) to foster appropriate land-use and development planning policies and practices that encompass relevant environmental, social and economic factors.

The main thrust of these policies can be summarised as follows:

- **Clause 11 Settlement** seeks to anticipate and respond to the needs of existing and future communities through provision of zoned and serviced land for housing, employment, recreation and open space, commercial and community facilities and infrastructure.
- **Clause 11.03-1R Activity Centres – Metropolitan Melbourne** supports the development and growth of Metropolitan Activity Centres by ensuring they can accommodate significant growth for a broad range of land uses.
- **Clause 11.03-1L Activity Centres** seeks to ensure that land use and development in activity centres is consistent with, and reinforces, the role and category of the centre.
- **Clause 11.03-1L-02 Box Hill Metropolitan Activity Centre** outlines a series of objectives and strategies for the Box Hill Metropolitan Activity centre. Within this clause, Map 1 notes that the site is situated within the Box Hill Transport and Retail Precinct, as well as the Southern & Eastern Precinct. Map 2 - Box Hill Metropolitan Activity Centre Built Form Precincts Map, identifies the site to be in precinct F, which is the Major Development Precinct, where *taller buildings permitted, enabling increased density. Heights must not cause overshadowing of Key Open Spaces, Residential Precincts A or B or residential areas beyond the study area. Transitional heights to be provided at edges of the precinct to respect the scale of neighbouring precincts.* Map 3 - Box Hill Metropolitan Activity Centre Built Form Precincts Map and Map 4 - Box Hill Metropolitan Activity Centre Access Framework Plan, demonstrates the site to be within the Priority Pedestrian Corridor.

Objectives

- *To facilitate the continued expansion of the Box Hill Metropolitan Activity Centre consistent with market demand.*
- *To support walking as the primary means of access in and around the activity centre.*
- *To encourage cycling as a sustainable and healthy means of travel to and within the activity centre.*
- *To provide accessible, vibrant and comfortable public spaces that offer diverse opportunities for recreation and social engagement.*

Land use strategies

- *Accommodate a wide range of uses, including residential and entertainment, that increase choices and opportunities, and help the activity centre become a focus of activity.*
- *Maximise employment growth for Whitehorse.*
- *Support the continued development of key activity clusters.*
- *Create more and diverse opportunities for housing, including affordable housing.*
- *Encourage complementary mixed uses and links between activities.*
- *Support development and uses in the Box Hill Transport and Retail Precinct that are consistent with its role and function as a regional transport interchange for rail, bus, tram and taxi services.*

Built form strategies

- *Transition building heights around the core of the activity centre to protect the amenity of surrounding residential areas.*

- *Protect key open spaces from overshadowing.*
- *Create street-orientated development.*
- *Integrate new development with heritage buildings.*
- *Facilitate change in each built form precinct while protecting areas of stability, as described on Map 2.*

Movement strategies

- *Encourage walking as the primary means of access with most trips of 1 kilometre or less within the activity centre to be taken on foot.*
- *Encourage cycling as a mode of transport for trips within the activity centre and for up to 5 kilometres between the activity centre and surrounding areas.*
- *Encourage use and development that improves the connectivity, safety and amenity of the pedestrian and cycling networks, consistent with Maps 3 and 4, including by:*
 - *Widening footpaths wherever possible.*
 - *Improving pedestrian crossings of busy streets.*
 - *Supporting way-finding.*
 - *Creating a local network of bicycle routes.*
 - *Improving the connectivity of streets while deterring through traffic.*

Public spaces strategies

- *Protect and improve access to existing public open spaces.*
- *Encourage development that creates new public spaces and facilities.*
- *Develop synergies between public spaces and uses at their edges.*
- *Enhance streets as public spaces.*

Vehicle parking strategies

- *Encourage use and development that would support reduced parking provision rates.*
- *Support the sharing of parking spaces between multiple uses.*
- *Provide car parking under new buildings instead of in the open air.*
- *Provide on-site motorcycle parking for occupiers and visitors.*
- *Provide service vehicle parking and access for new development in a manner that supports local uses, while minimising conflicts with other uses.*
- **Clause 15 Built Environment and Heritage** states that planning should ensure all new land use and development appropriately responds to its landscape, valued built form and cultural context, and protect places and sites with significant heritage, architectural, aesthetic, scientific and cultural value. The clause recognises that creating quality built environments supports the social, cultural, economic, and environmental wellbeing of communities, cities and towns.
- **Clause 15.01-2L Environmentally sustainable development** seeks to achieve best practice in environmentally sustainable development from the design stage through to construction and operation.
- **Clause 17 Economic Development** states planning aims to provide for a strong and innovative economy, where all sectors are critical to economic prosperity. Overall contributing to the economic wellbeing of the state and fostering economic growth by providing land, facilitating decisions, and resolving land use conflicts, enabling each region to build on its strengths and achieve its economic potential.
- **Clause 18.02-3R Principle Public Transport Network** encourages the increase the diversity and density of development along the Principal Public Transport Network, particularly in activity centres.

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- **Clause 18.02-4L Car parking** encourages the provision and usage of off-street parking areas to minimise on-street parking.

STRATEGIC DOCUMENTS

Plan Melbourne 2017

Plan Melbourne, the Victorian Government's metropolitan planning strategy that will guide the city's growth to 2050 was released in May 2014 (updated 2017) and is currently referenced in the State Planning Policy framework. It is a strategy focussed on providing adequate employment, housing, and transport around the central city and beyond.

The Strategy focuses on the following 9 key concepts:

- *Protecting the Suburbs by Delivering Density in Defined Locations*
- *A State of Cities*
- *Delivering a Pipeline of Investment Opportunity*
- *Better Use of Existing Assets*
- *20 Minute Neighbourhoods*
- *Housing Choice and Affordability*
- *Transitioning to a More Sustainable City*
- *Good Governance and Strong Partnerships*

The report recognises Melbourne's population will increase to more than 6.5 million people by 2050. This additional population will require housing, employment and ancillary services and facilities.

Part of the direction is to ensure this increased population can be catered for without unreasonably impacting on the existing qualities of Melbourne's suburbs. The Strategy establishes five metropolitan subregions including the Box Hill (MAC) Metropolitan Activity Centre.

Box Hill Transit City Activity Centre Structure Plan 2007

The *Box Hill Transit City Activity Centre Structure Plan 2007* (referred to as the Structure Plan 2007) is a reference document of the Whitehorse Planning Scheme at Clause 11.03-1L-02.

The Structure Plan's broad framework encompasses a 'network of public spaces' within a 'fine grain network of safe and attractive streets and laneway' where walking and public transport use is encouraged. 'Land uses that complement the qualities of their immediate area' should be provided within 'buildings contribute to the quality of the public environment, protect the amenity of neighbouring users, and provide appropriate transitions between areas of significant change and areas of relative stability'.

The Structure Plan encourages the activity centre's substantial development to occur within the core of the centre, with built form transitioning down to the surrounding residential areas. The subject land is located within Precinct F 'Major Development'. The guiding principles and urban design outcomes are discussed within the Clause 11.03-1L-02 section of this Appendix.

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Figure 26: Box Hill Precinct Plan (2024)

Figure 5: BUILT FORM PRECINCTS



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- A** Peripheral Residential Precincts: New development supporting higher densities but consistent with the areas' existing built form character.
- B** Low-rise Higher-density Residential Precincts: 3-storey height limit with reduced setback requirements enabling increased residential densities while maintaining a transitional building scale.
- C** Traditional Town Centre: The existing built form character and 2 to 3-storey scale of the precinct retained including listed heritage buildings as well as complementary buildings.
- D** Mid-rise Commercial and Mixed Use Precincts: 4 storey height limit supporting increased density, with no (or minimal) front and side setbacks to create active frontages onto streets.
- E** Town Hall Precinct: Civic buildings given visual emphasis and the significance of heritage buildings and related spaces protected. Heights to be determined on case-by-case basis and may vary across each site.
- F** Major Development Precinct: Taller buildings permitted, enabling increased density. Heights must not cause overshadowing of Key Open Spaces, Residential Precincts A or B or residential areas beyond the study area. Transitional heights to be provided at edges of the precinct to respect the scale of neighbouring precincts.
- Key Open Spaces:** Protect and enhance existing public and major private open spaces' character and provide an effective increase in useable open space through removal of encumbrances and design improvements.

Source: Victoria State Government, 2024

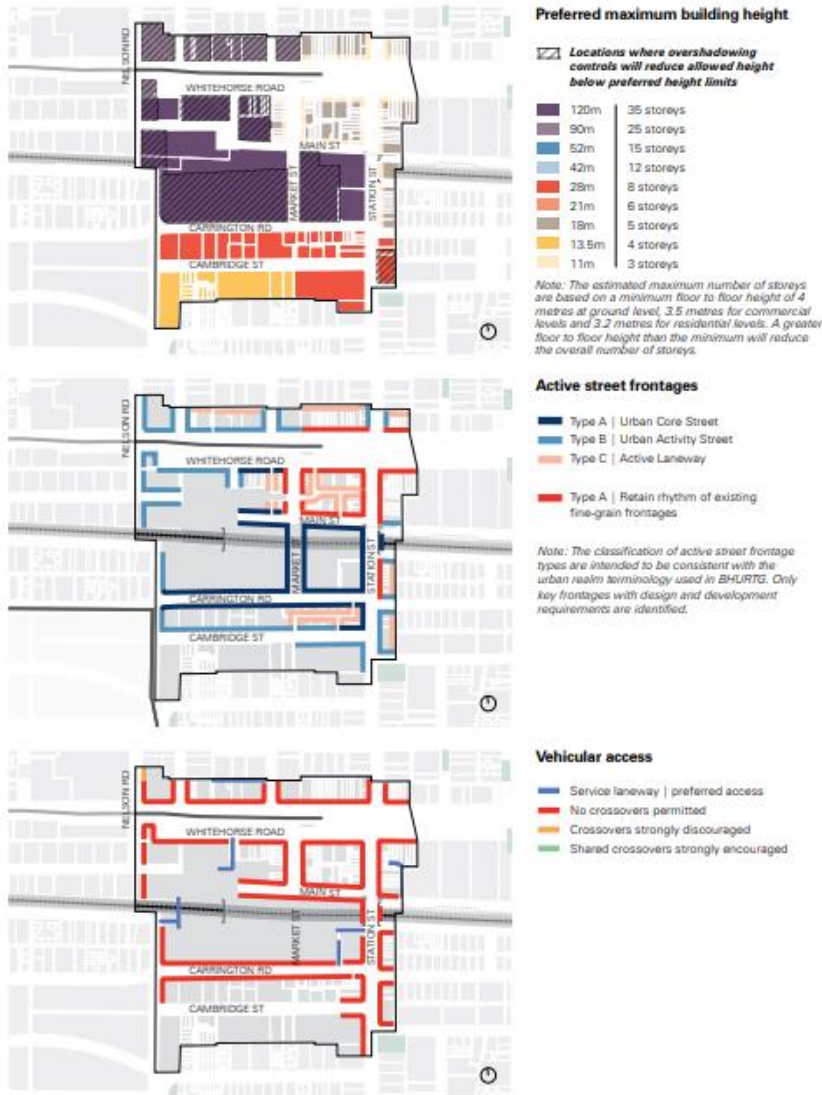
Box Hill Metropolitan Activity Centre to 2036

In October 2021 a revised *Box Hill Metropolitan Activity Centre to 2036 Draft Structure Plan* was prepared for the Box Hill MAC. The plan was supported by the *Box Hill Metropolitan Activity Centre to 2036 Urban Design Framework* which contains recommendations for a new built form framework. While it is noted the work remains in draft form and has not progressed to a planning scheme amendment, it's objectives and built form guidance are considered relevant. The urban design framework cannot be 'seriously entertained' within the assessment.

The 'Central Neighbourhood' Precinct Guidelines apply to the subject land. The Site is identified within the 'diversified transition' area with a preferred maximum height of 28 metres. North of Carrington Road is classified as 'intensified urban core' while south of Cambridge Street is classified as 'residential transition'. The role and character directed for Carrington Road is quite different to Cambridge Street. Carrington Road is identified as a primary pedestrian link where increased public realm upgrades and tree canopy cover increases are directed. The southern footpath is also protected from overshadowing. The Draft UDF distinguishes it as an 'urban core street' where vehicle crossovers should be avoided. A preferred street wall height of 20 metres is directed.

Cambridge Street is not identified within the primary pedestrian network nor are crossovers discouraged. A preferred street wall height of 13.5m is directed. Of note, a preferred future link is identified in the Plan between Carrington Road and Cambridge Street east of the Site.

Figure 27: Built Form Framework (Central Neighbourhood)



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Source: MGS Architects, 2021

Suburban Rail Loop Precinct Vision (Box Hill) 2024

The Suburban Rail Loop (SRL) will deliver a 90km rail line linking every major train service from the Frankston Line to the Werribee Line via Melbourne Airport, better connecting Victorians to jobs, retail, education, health services and each other. The SRL project involves the SRL East station at Box Hill which will be one of the busiest and most well-connected stations on the metropolitan network.

The vision for Box Hill states the goal of creating a thriving, culturally dynamic and cosmopolitan place where global and local communities connect. Its exceptional transport accessibility will elevate its strategic importance as one of Melbourne’s economic and lifestyle centres, and it will play a key role in the city’s visitor economy. Further stating that through building on existing strengths, new economic opportunities in Box Hill will leverage established health, education, cultural and recreational services, as well as the vibrant local business community.

The key directions for the SRL East (Box Hill) precinct include:

- *Enriching Community*
- *Boosting the Economy*
- *Enhancing Place*
- *Better Connections*

- *Empowering Sustainability*

These five (5) precinct themes will guide and direct strategic planning to realise the Vision for Box Hill.

The planning process for Box Hill began in late 2023, where a Box Hill Draft Precinct Plan was delivered. The Draft Precinct Plan identified where change is likely to occur as Box Hill grows over time. Following this in early to mid 2024, feedback was sought regarding the key directions which further refined the understanding of opportunities and challenges, and explore place-shaping initiatives that could help realise the Vision. Currently in late 2024 and early 2025 the final Precinct Visions have been released along with draft structure plans and planning scheme amendments.

The precinct plan identifies the site and neighbouring properties on Carrington Road as 'significant change areas', while the properties on the southern side of Cambridge Street are within a 'higher change area' precinct. The Significant Change areas will continue to deliver urban density in line with the emerging character already developed in Box Hill, with mixed-use neighbourhoods providing space for jobs growth and local services.

Therefore, the state-significant policy supports further growth within the Box Hill MAC. The Draft Precinct Plan does not limit height on the land but rather it encourages taller buildings and increased density, while ensuring transitional heights are provided to respect the scale of neighbouring properties.

Figure 28: Box Hill Conceptual Precinct Plan (2024)

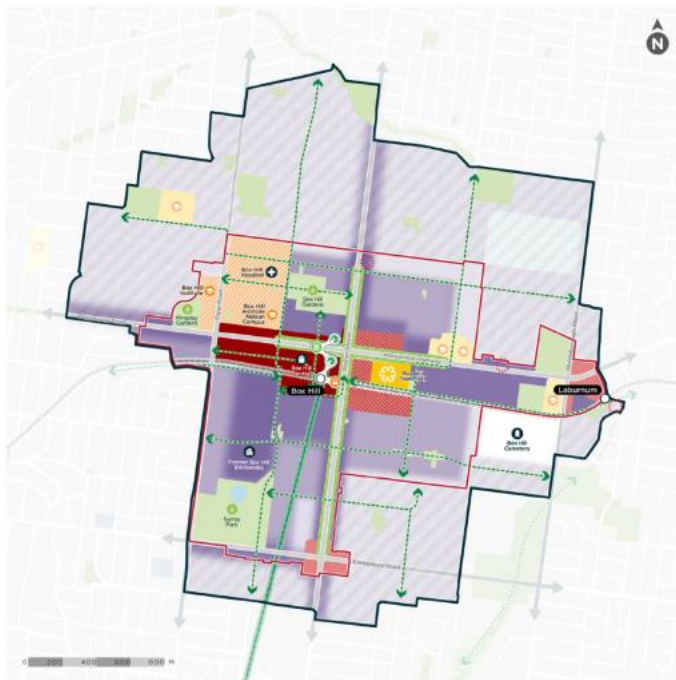
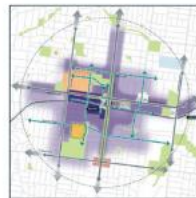


Figure 4: Box Hill Conceptual Precinct Plan



The Conceptual Precinct Plan has evolved from the Draft Precinct Plan released in the *Box Hill Draft Precinct Vision* in December 2023, in response to community feedback and stakeholder submissions.



Box Hill Draft Precinct Plan, December 2023

ADVERTISED PLAN

Source: Victoria State Government, 2024

ADVERTISED PLAN

APPENDIX B PLANNING CONTROL MAPS

**ADVERTISED
PLAN**

Figure 29: Commercial 1 Zone (C1Z) Map



69 CARRINGTON ROAD, BOX HILL
PLANNING ZONES

Source: Urbis Pty Ltd, 2023

**ADVERTISED
 PLAN**

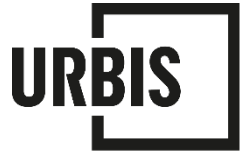
ADVERTISED PLAN

Figure 30: Parking Overlay – Schedule 1 (PO1) Map



69 CARRINGTON ROAD, BOX HILL PARKING OVERLAY (PO1)

Source: Urbis Pty Ltd, 2023



ADVERTISED PLAN